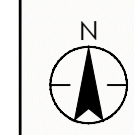


INDICATIVE JUNCTION LAYOUT



LOCATION PLAN

what3words: broad.folk.ribcage



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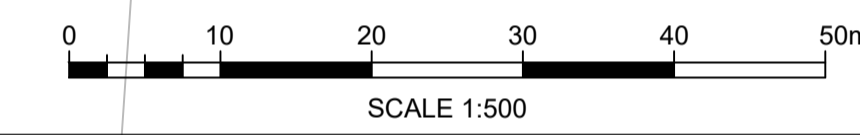
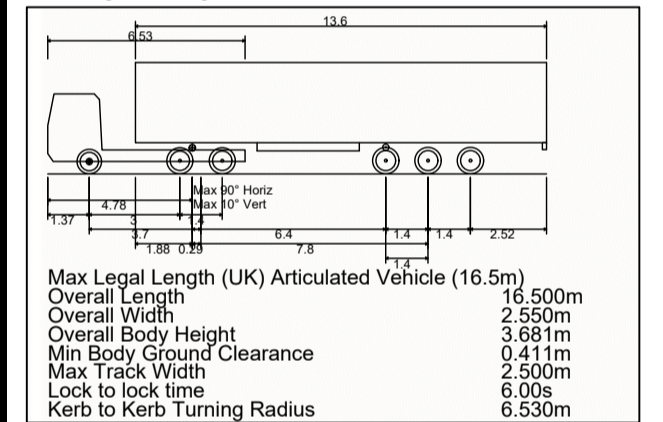
Notes

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- PHASE USE OF CROSSING A33 - CONSTRUCTION.

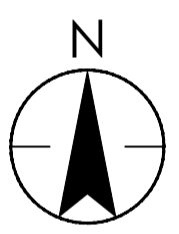
- LEGEND**
- PUBLIC RIGHT OF WAY (PROW)
 - ORDER LIMIT
 - 2.4 x 90m VISIBILITY SPLAY
 - VISIBILITY ADDITIONAL AREA

VEHICLE PROFILE

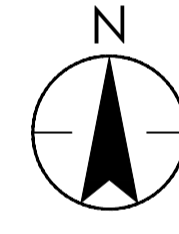


D

C



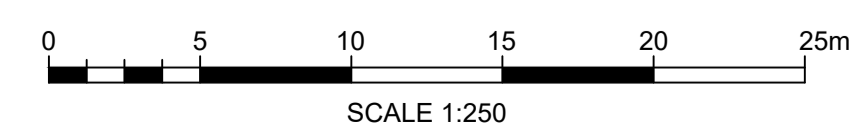
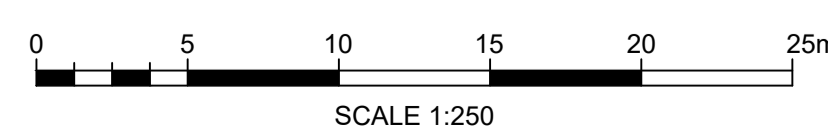
16.5m HGV VEHICLE SWEEP PATH - NORTHBOUND



16.5m HGV VEHICLE SWEEP PATH - SOUTHBOUND

B

A



P:\01678\333101678\10020\DWG\10020.DWG (I:\0020\DWG\10020.DWG) - 11/10/2024 10:02:00 AM - User: j.greene

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		KWM	KWM	JO
		Dwn.	Dsgn.	Chkd.
				2025.12.09
				YYYY.MM.DD

Issue Status
S2 - FOR INFORMATION

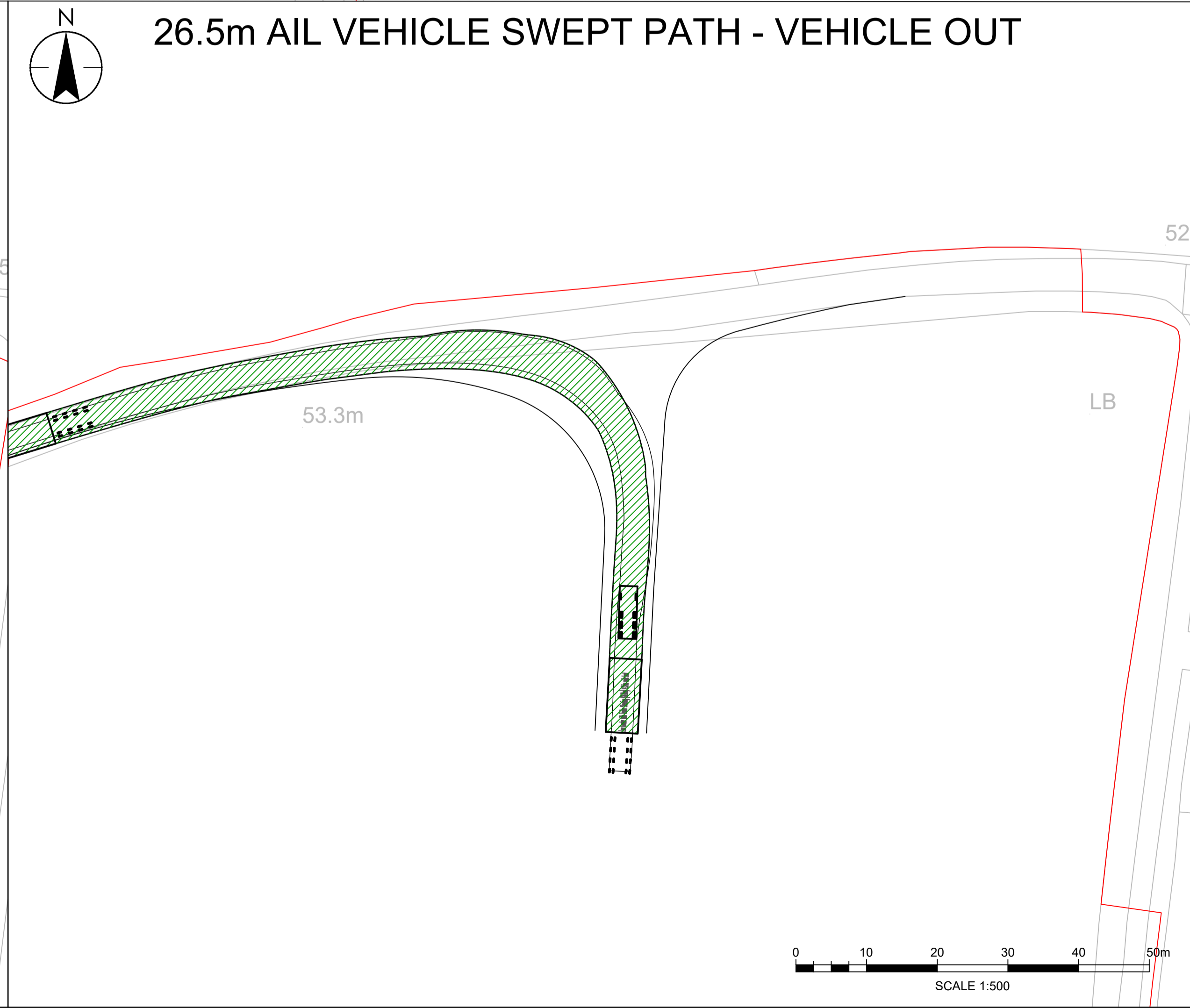
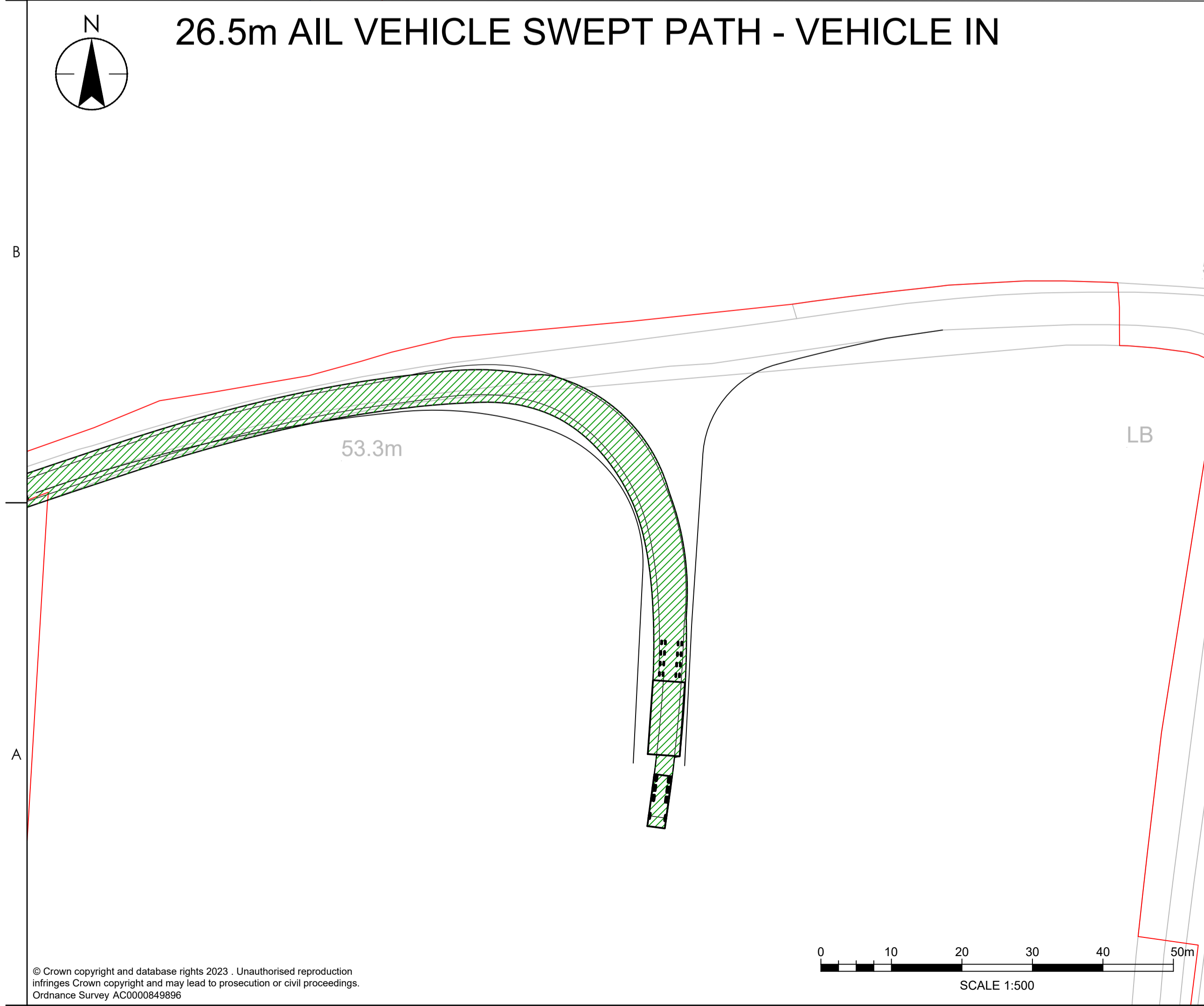
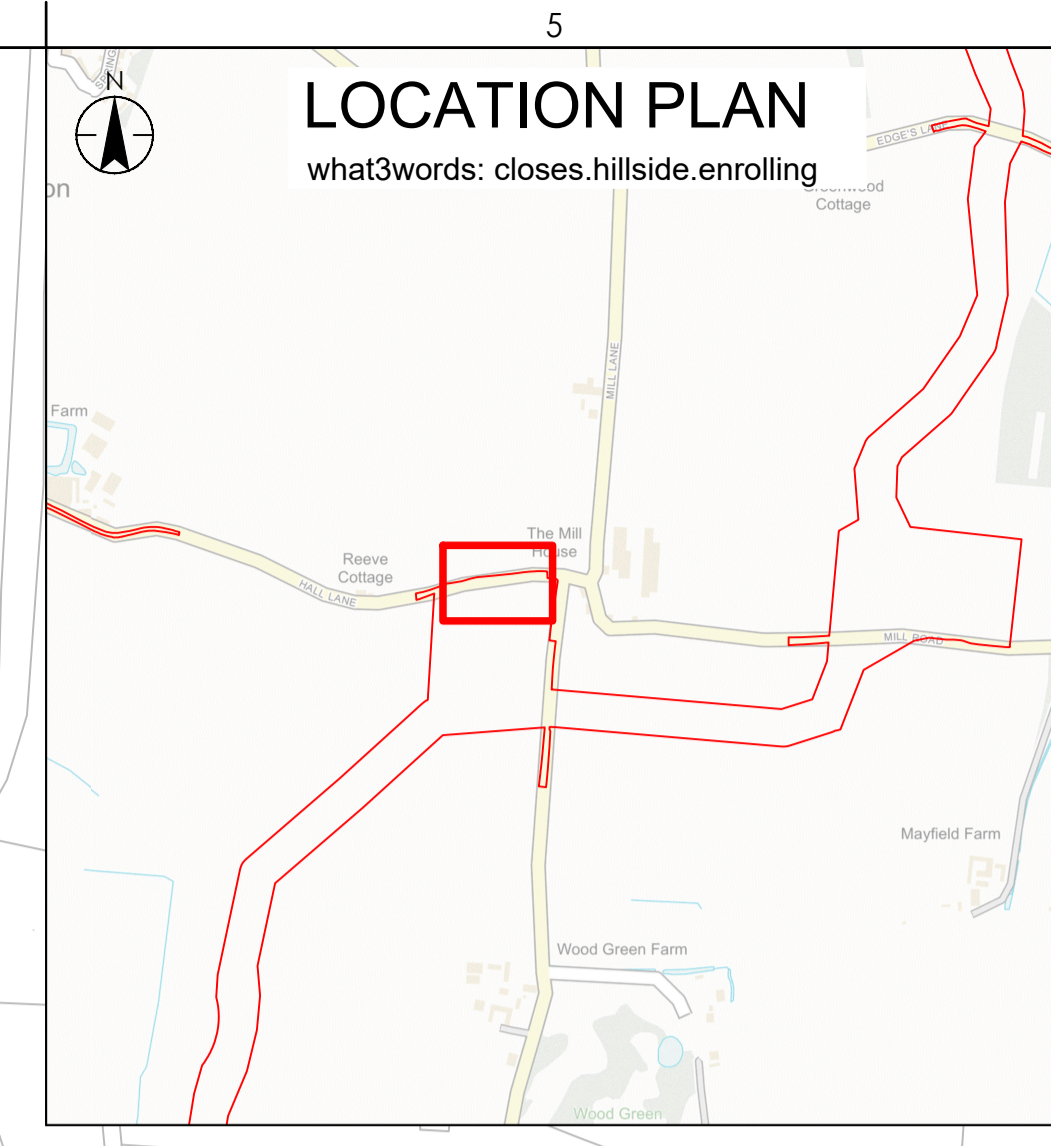
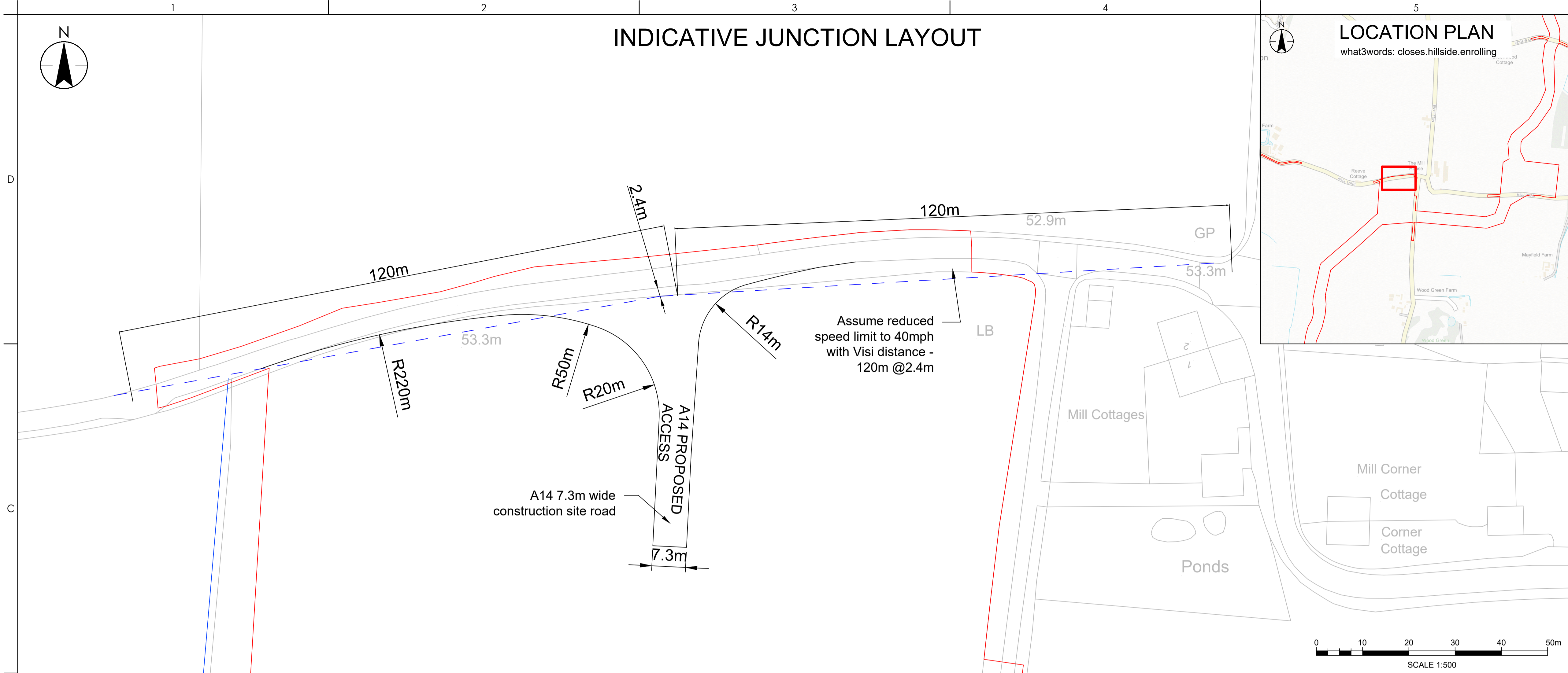
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Client/Project
ISLAND GREEN POWER
EAST PYE SOLAR FARM
DEVELOPMENT CONSENT ORDER

Title
INDICATIVE SITE CROSSING ACCESS A33
LUNDY GREEN
16.5m HGV
SHEET 48

Project No.	333101678	A1 Scale	AS SHOWN
Revision	PO1	Drawing No.	333101678-STN-HGN-XX-DR-TR-5500



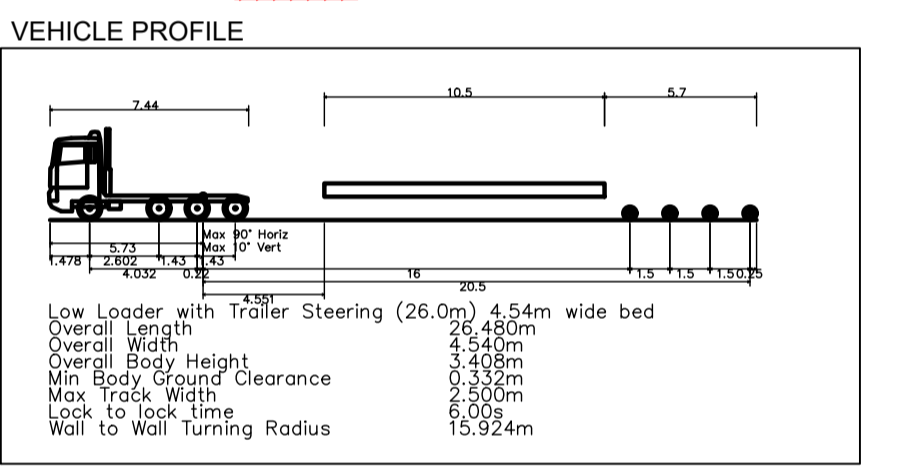
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 - STATION ROAD IS SUBJECT TO THE NATIONAL SPEED LIMIT
 - VEHICLE TRACKING AND VISIBILITY IS BASED ON MAXIMUM LEGAL VEHICLE SIZE, THROUGH DETAILED DESIGN AND VEHICLE SELECTION THE REQUIREMENTS FOR HEDGE REMOVAL AND EXTENT OF HARD SURFACING WHERE PRACTICAL MAY BE REDUCED.
 - VEHICLES EXPECTED TO TRAVEL TO / FROM ACCESS A14 VIA A140 IPSWICH ROAD / HALL LANE OR LONG STRATTON BYPASS.
 - PHASE USE OF ACCESS A14 - CONSTRUCTION & OPERATION. A14 HIGHWAY ACCESS TO PROVIDE ACCESS TO OHL WORKS.

LEGEND

	PUBLIC RIGHT OF WAY (PROW)
	ORDER LIMIT
	2.4 x 120m VISIBILITY SPLAY
	VISIBILITY ADDITIONAL AREA



PO1	FIRST ISSUE	JO	TH	2024.02.11
	Issued/Revision	By	Appd	YYYY.MM.DD
		KWM	KWM	JO
		Dwn.	Dsgn.	Chkd.
				2025.12.09
				YYYY.MM.DD

Issue Status

S2 - FOR INFORMATION

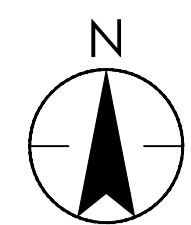
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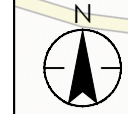
Client/Project
 ISLAND GREEN POWER
 EAST PYE SOLAR FARM
 DEVELOPMENT CONSENT ORDER

Title
 INDICATIVE SITE ACCESS A14
 HALL LANE
 26.5m AIL
 SHEET 50

Project No. 333101678 A1 Scale AS SHOWN
 Revision P01 Drawing No. 333101678-STN-HGN-XX-DR-TR-5500



INDICATIVE JUNCTION LAYOUT



LOCATION PLAN

what3words: equity.relishing.canal



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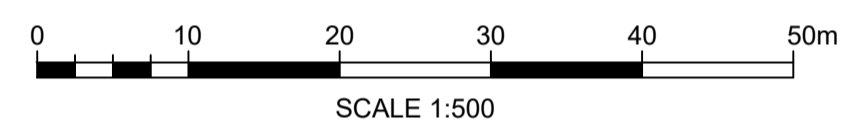
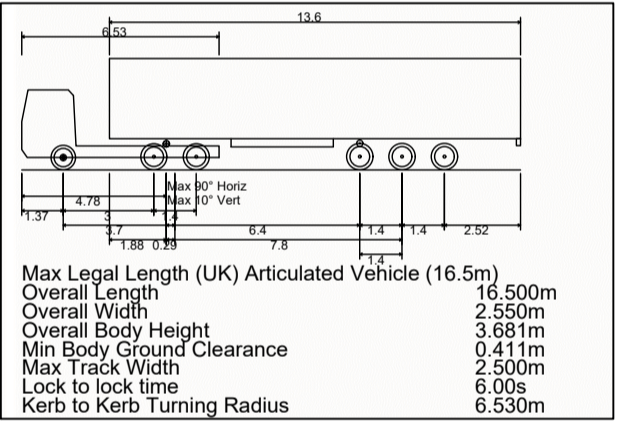
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- VEHICLES EXPECTED TO TRAVEL TO / FROM ACCESS A16 VIA CABLE ROUTE CORRIDOR ONLY
- PHASE USE OF ACCESS A16 - CONSTRUCTION & OPERATION. A16 HIGHWAY ACCESS TO PROVIDE ACCESS TO OHL WORKS.

LEGEND

- PUBLIC RIGHT OF WAY (PROW)
- ORDER LIMIT
- 2.4 x 90m VISIBILITY SPLAY
- VISIBILITY ADDITIONAL AREA

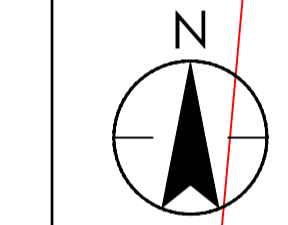
VEHICLE PROFILE



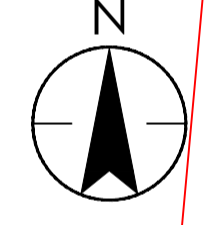
D

4.7m

C



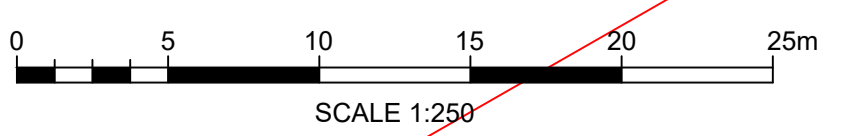
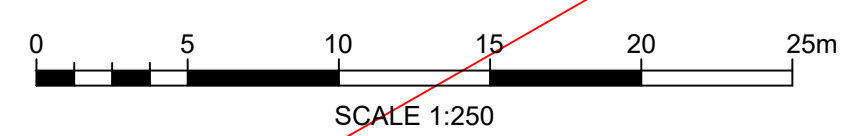
16.5m HGV VEHICLE SWEEP PATH - SOUTHBOUND



16.5m HGV VEHICLE SWEEP PATH - NORTHBOUND

B

A



P:\016\333101678\333101678-001\Drawings\16.5m HGV Swept Path\16.5m HGV Swept Path - Southbound.dwg (16.5m HGV Swept Path - Southbound.dwg) - 11/11/2024 10:00:00 AM - 11/11/2024 10:00:00 AM - 11/11/2024 10:00:00 AM

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		Dwn.	Dsgn.	Chkd.
				2025.12.09
				YYYY.MM.DD

Issue Status
S2 - FOR INFORMATION

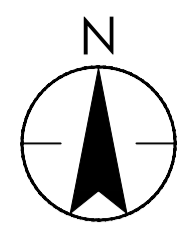
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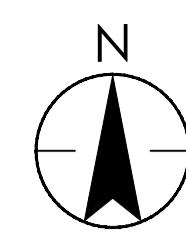
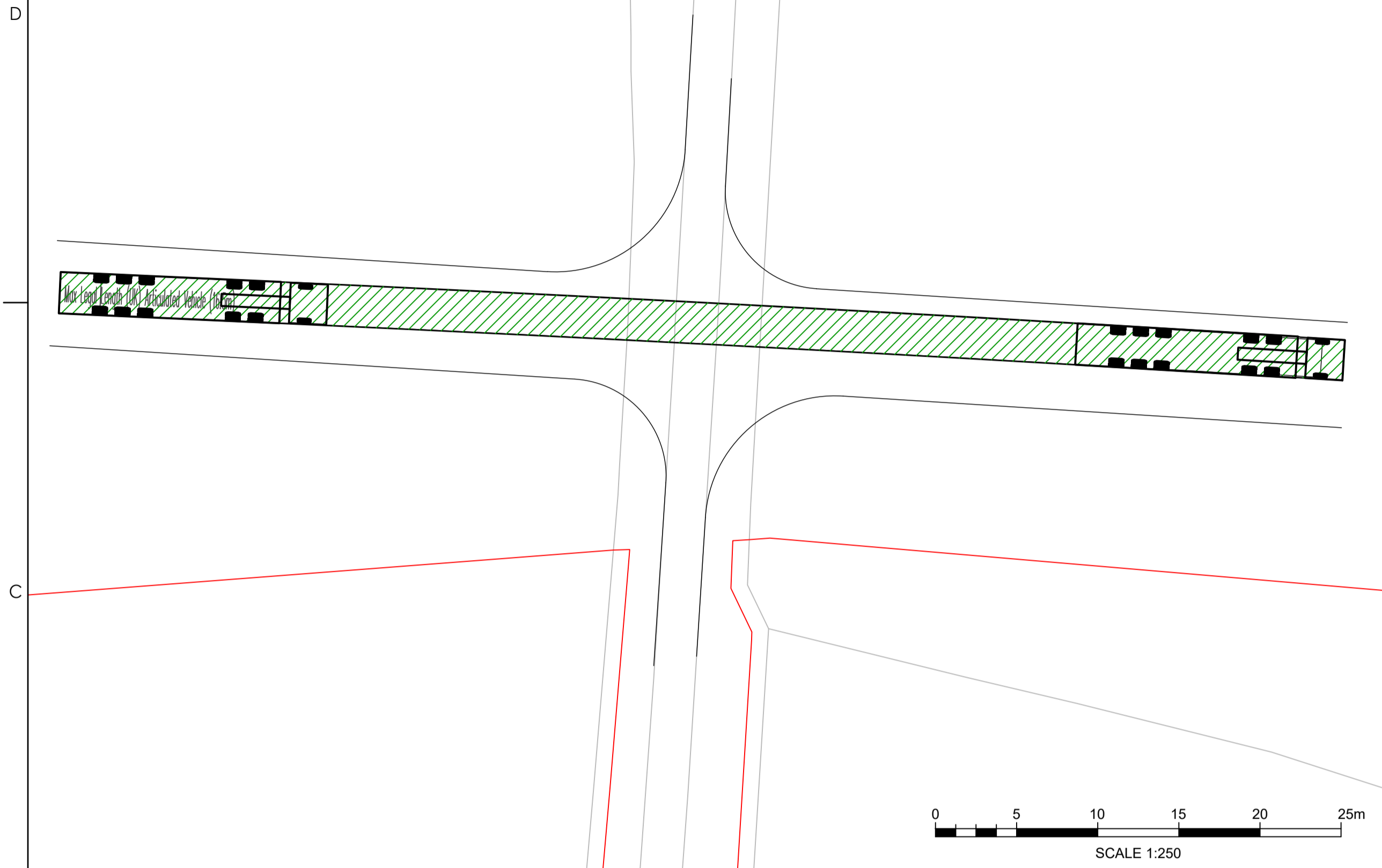
Client/Project
ISLAND GREEN POWER
EAST PYE SOLAR FARM
DEVELOPMENT CONSENT ORDER

Title
INDICATIVE SITE ACCESS A16
MILL ROAD
16.5m HGV
SHEET 51

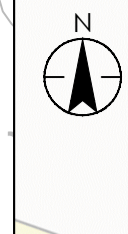
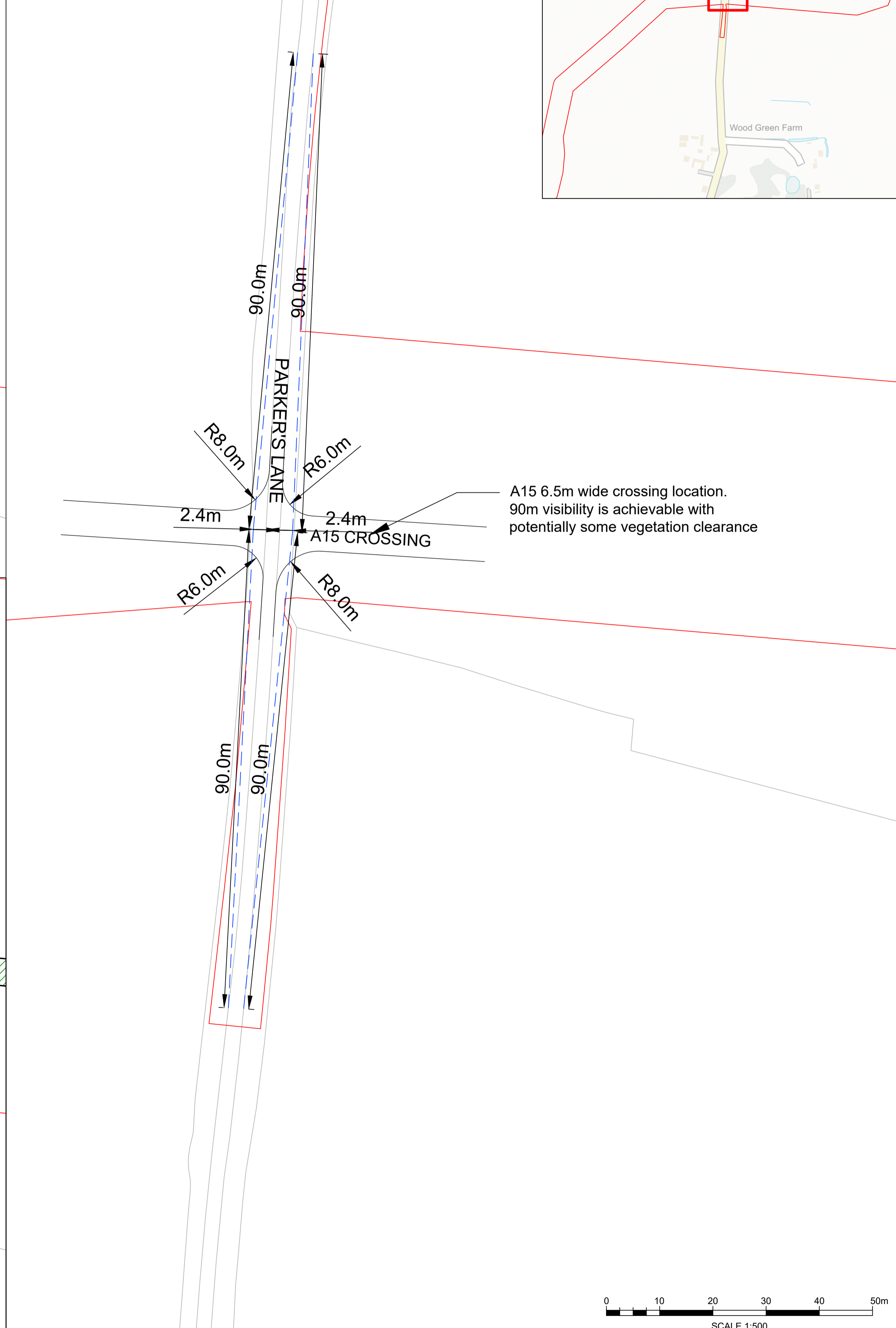
Project No. 333101678	A1 Scale AS SHOWN
Revision PO1	Drawing No. 333101678-STN-HGN-XX-DR-TR-5500



16.5m HGV VEHICLE SWEEP PATH - EASTBOUND

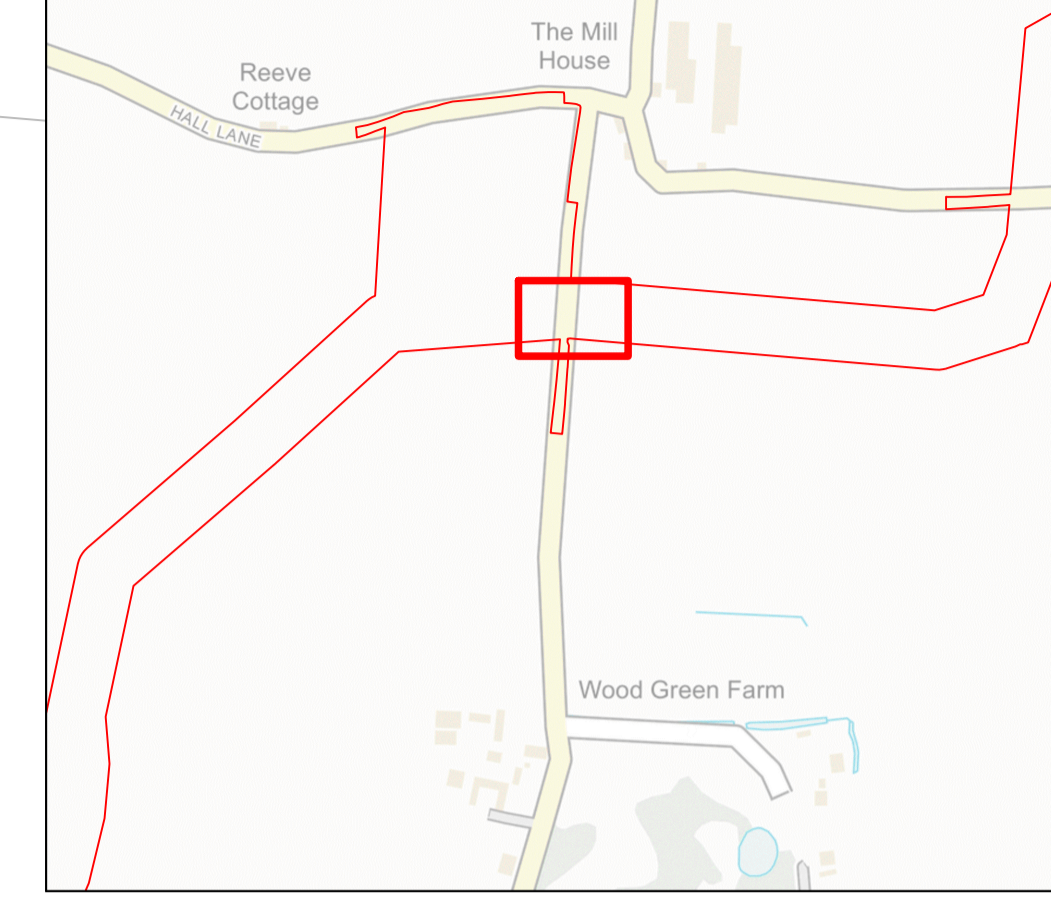


INDICATIVE JUNCTION LAYOUT



LOCATION PLAN

what3words: parked.wished.producing



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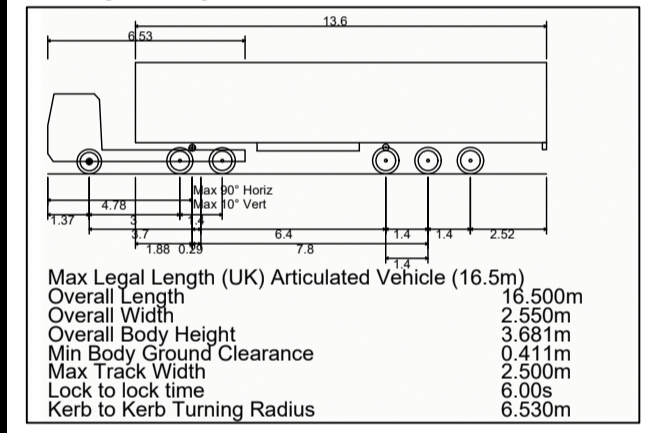
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- VEHICLES EXPECTED TO TRAVEL TO / FROM ACCESS A15 VIA CABLE ROUTE CORRIDOR ONLY
- PHASE USE OF ACCESS A15 - CONSTRUCTION & OPERATION. A15 HIGHWAY ACCESS TO PROVIDE ACCESS TO OHL WORKS.

LEGEND

- PUBLIC RIGHT OF WAY (PROW)
- ORDER LIMIT
- 2.4 x 90m VISIBILITY SPLAY
- VISIBILITY ADDITIONAL AREA

VEHICLE PROFILE



FIRST ISSUE	JO	TH	2024.02.11
Issued/Revision	By	Appd	YYYY.MM.DD
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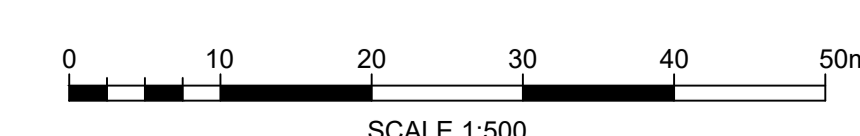
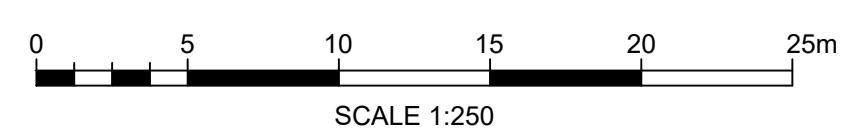


Client/Project
ISLAND GREEN POWER

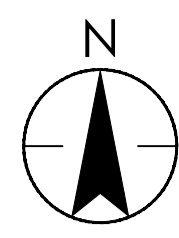
EAST PYE SOLAR FARM
DEVELOPMENT CONSENT ORDER

Title
INDICATIVE SITE ACCESS A15
PARKER'S LANE
16.5m HGV
SHEET 52

Project No.	A1 Scale
333101678	AS SHOWN
Revision	Drawing No.
P01	333101678-STN-HGN-XX-DR-TR-5500



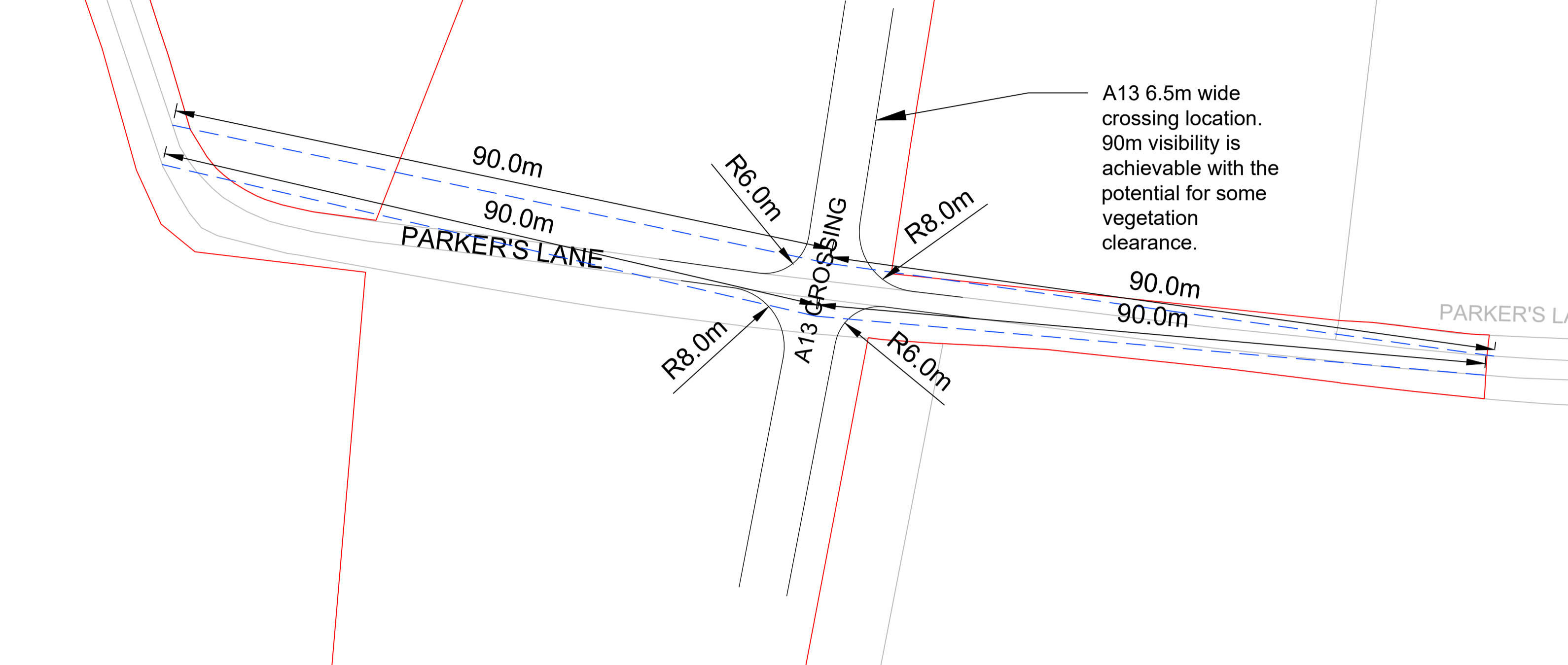
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Collects

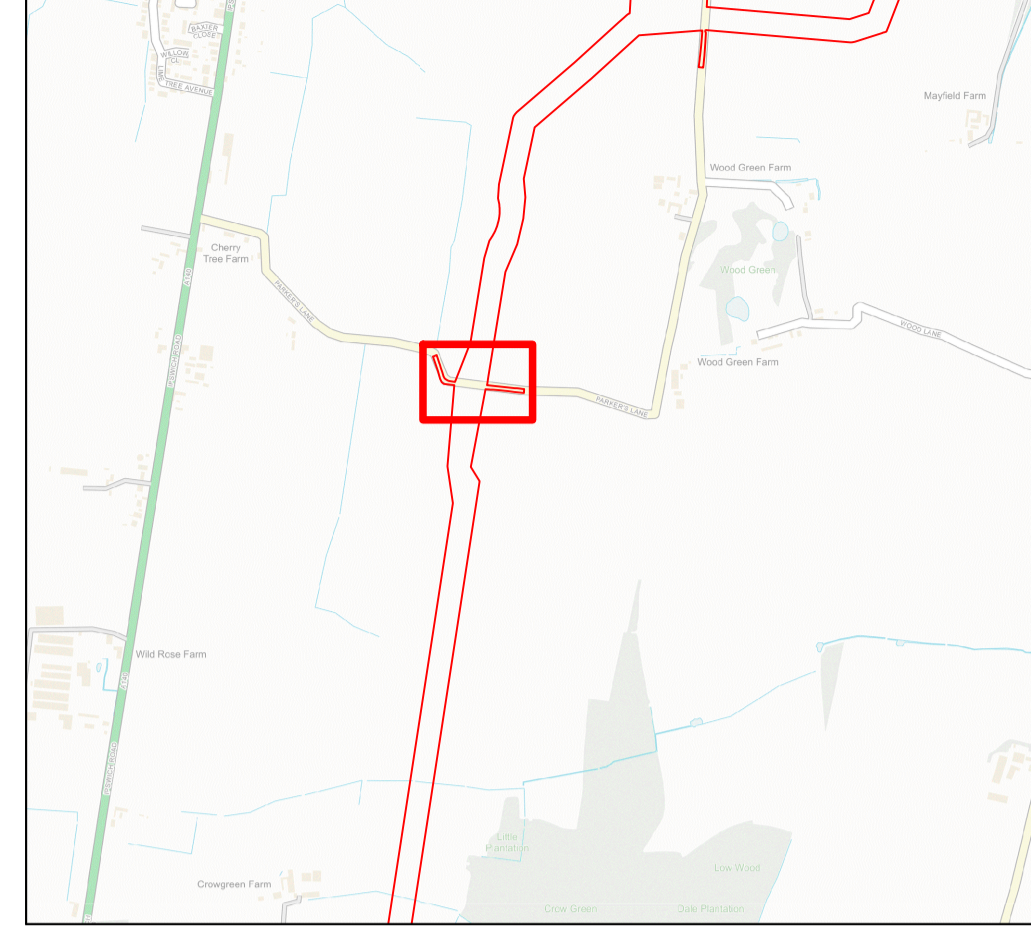
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INDICATIVE JUNCTION LAYOUT



LOCATION PLAN

what3words: trailer.simmer.centuries

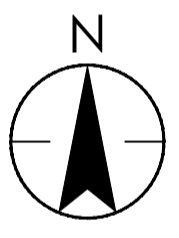
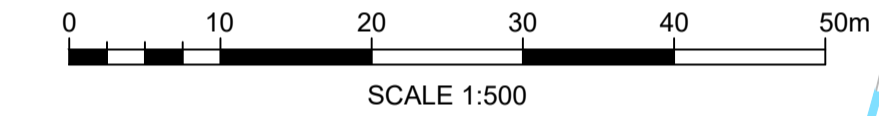
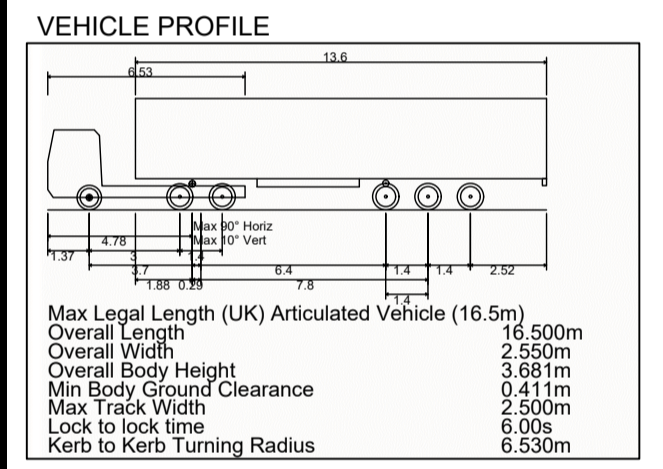


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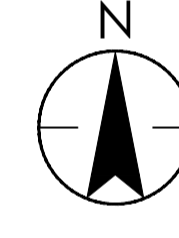
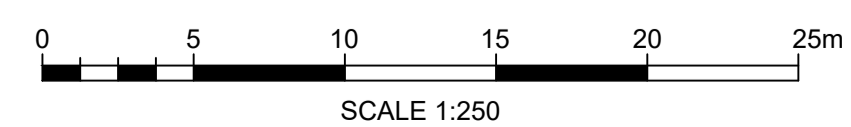
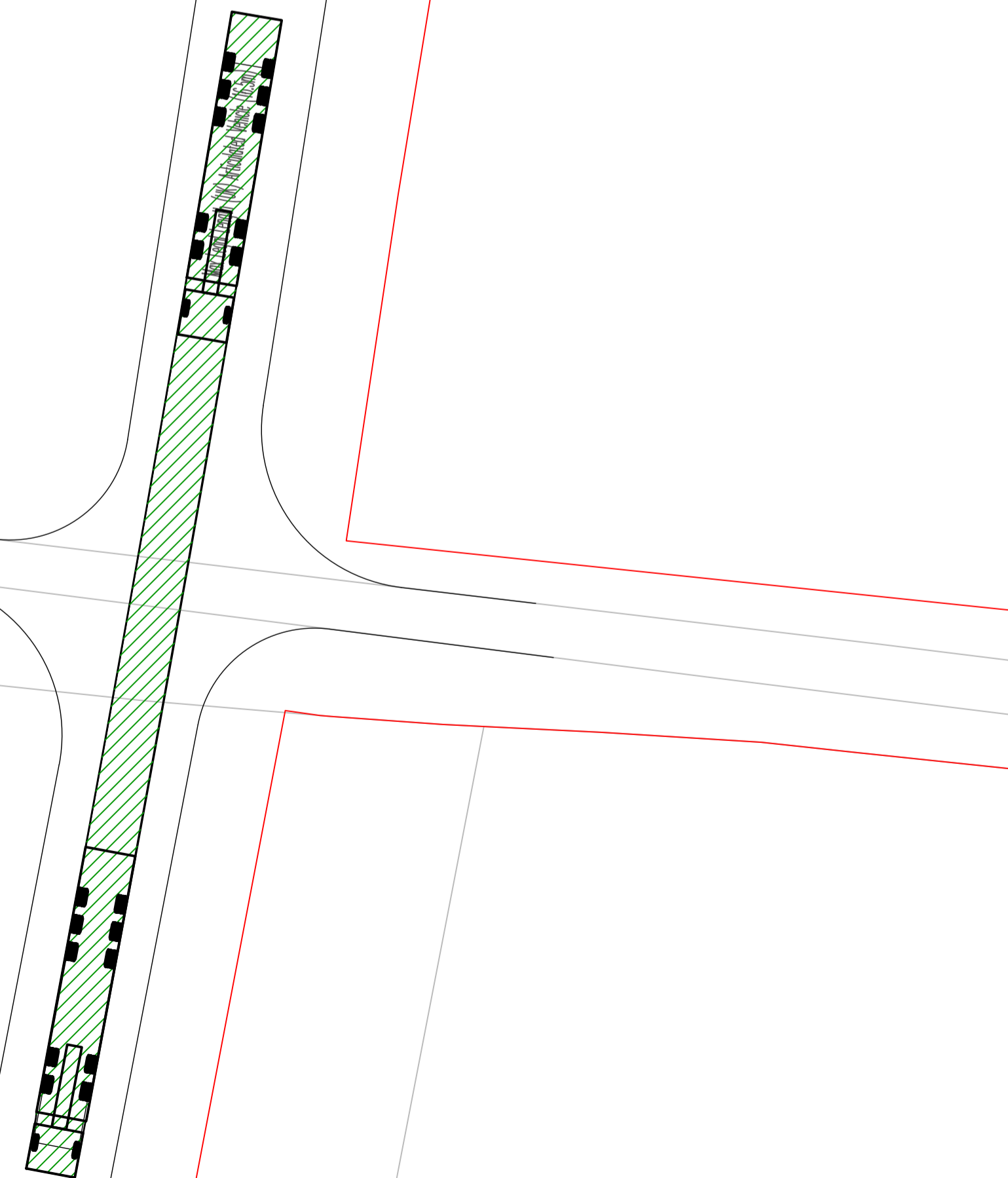
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 - PHASE USE OF ACCESS A13 - CONSTRUCTION & OPERATION. A13 HIGHWAY ACCESS TO PROVIDE ACCESS TO OHL WORKS.

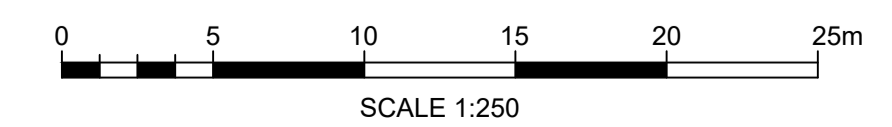
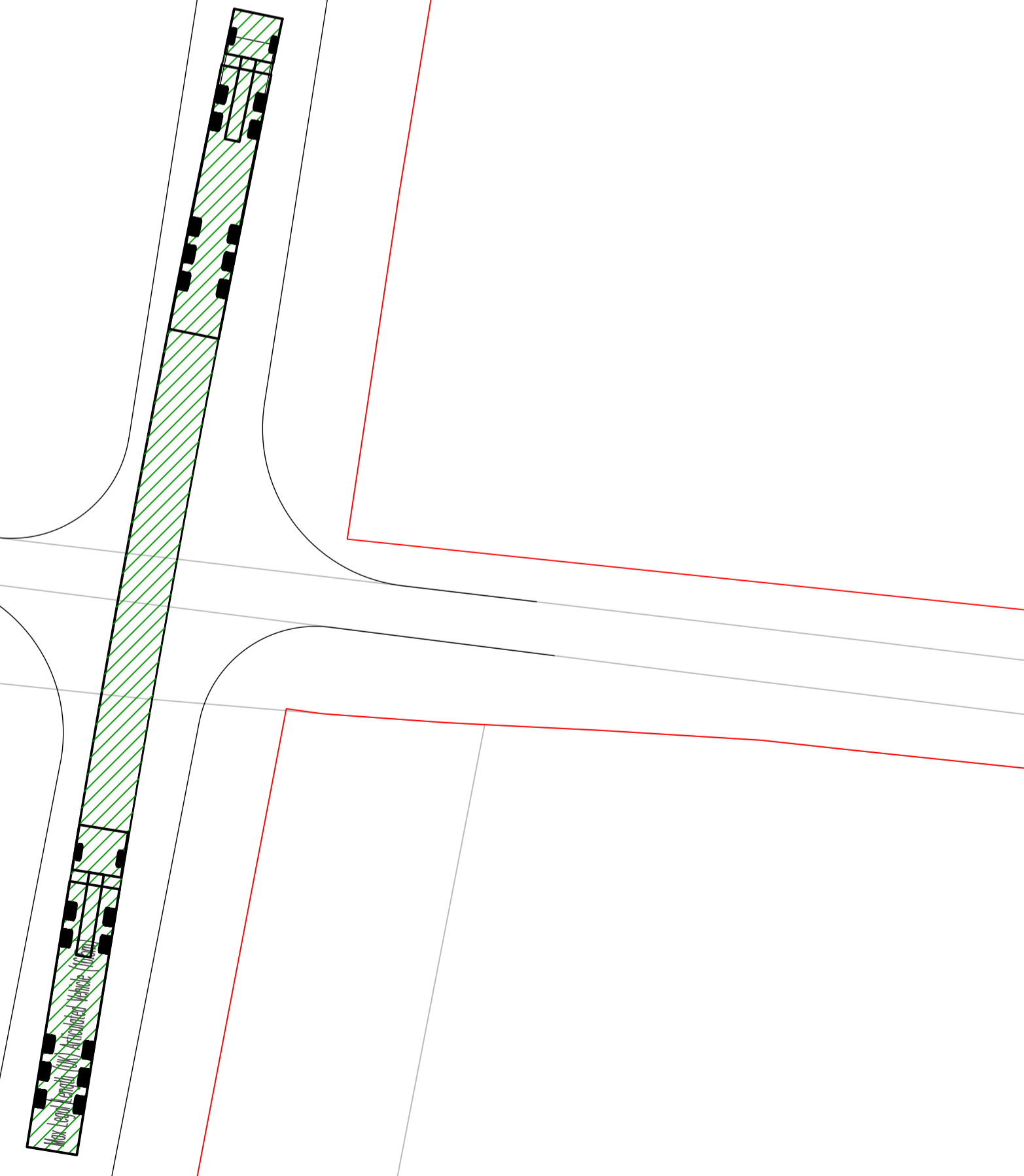
- LEGEND
- PUBLIC RIGHT OF WAY (PROW)
 - ORDER LIMIT
 - 2.4 x 90m VISIBILITY SPLAY
 - VISIBILITY ADDITIONAL AREA



16.5m HGV VEHICLE SWEEP PATH - SOUTHBOUND



16.5m HGV VEHICLE SWEEP PATH - NORTHBOUND



P01	FIRST ISSUE	JO	TH	2024.02.11
	Issued/Revision	By	Appd	YYYY.MM.DD
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		Dwn.	Dsgn.	Chkd.
				2025.12.09
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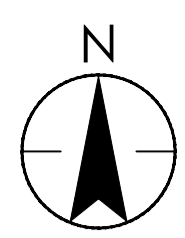
Client/Project
 ISLAND GREEN POWER
 EAST PYE SOLAR FARM
 DEVELOPMENT CONSENT ORDER

Title
 INDICATIVE SITE ACCESS A13
 PARKER'S LANE
 16.5m HGV
 SHEET 53

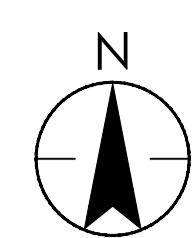
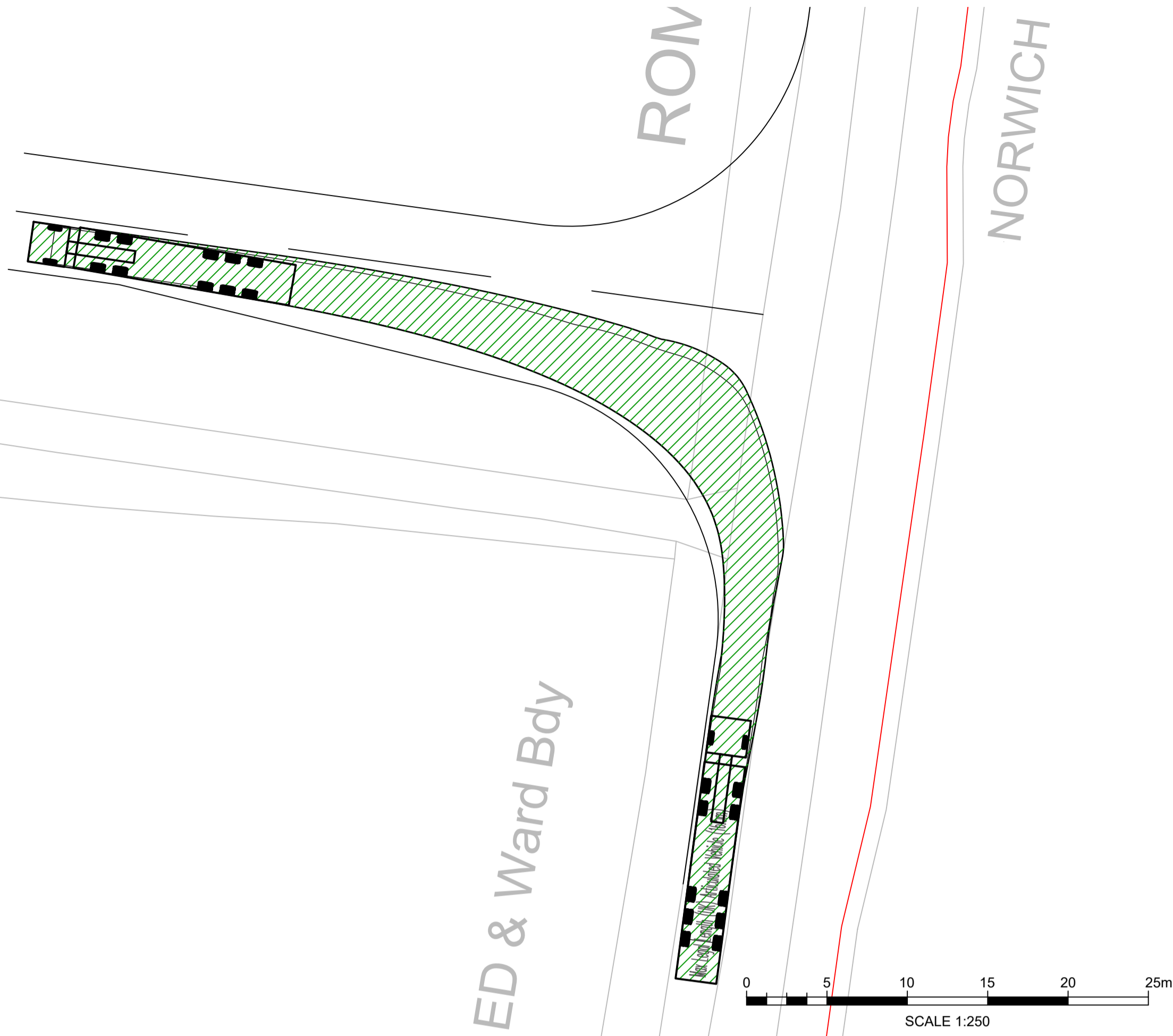
Project No. 333101678 A1 Scale AS SHOWN

Revision P01 Drawing No. 333101678-STN-HGN-XX-DR-TR-5500

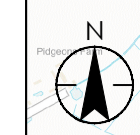
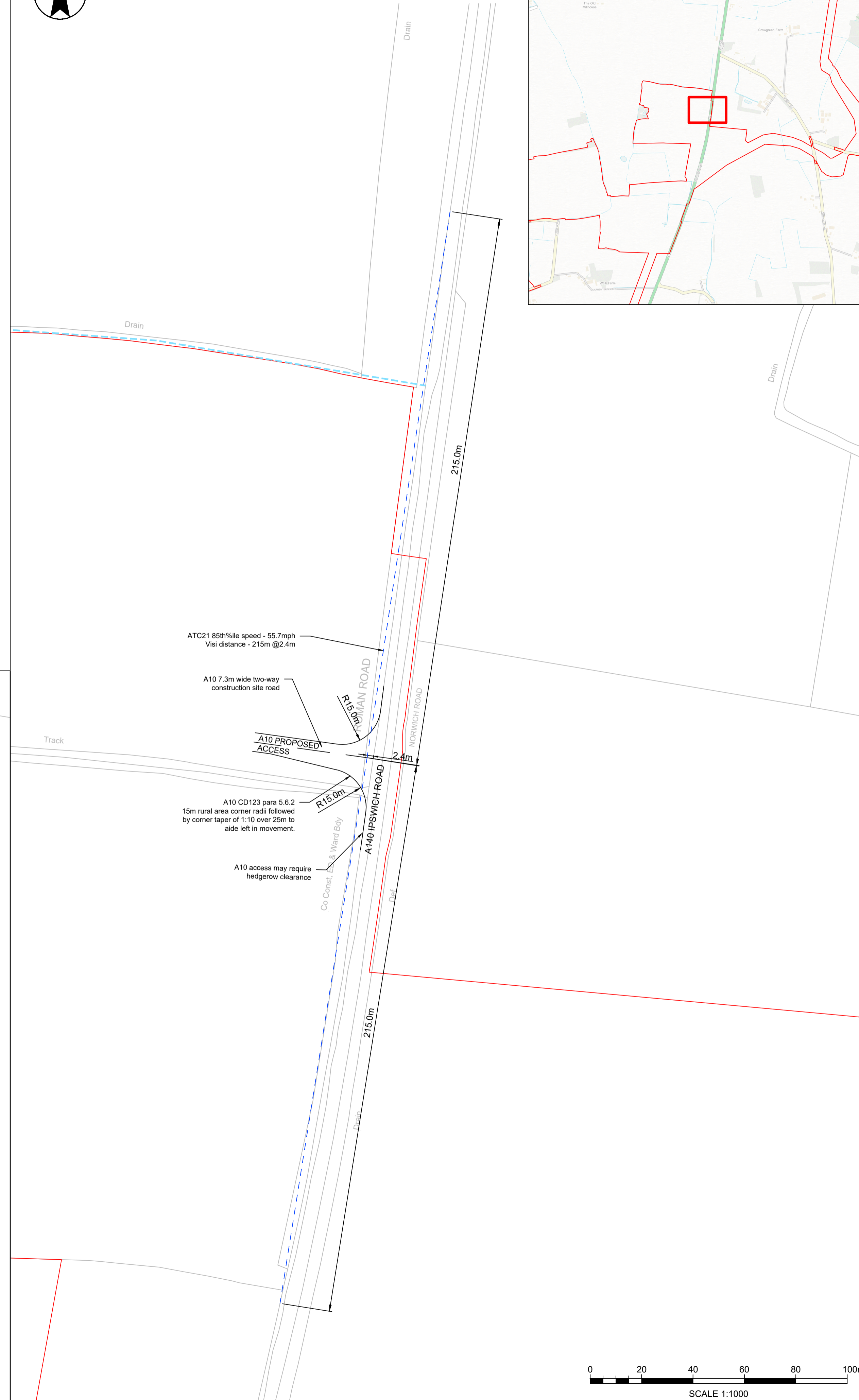
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 Ordnance Survey AC0000849896



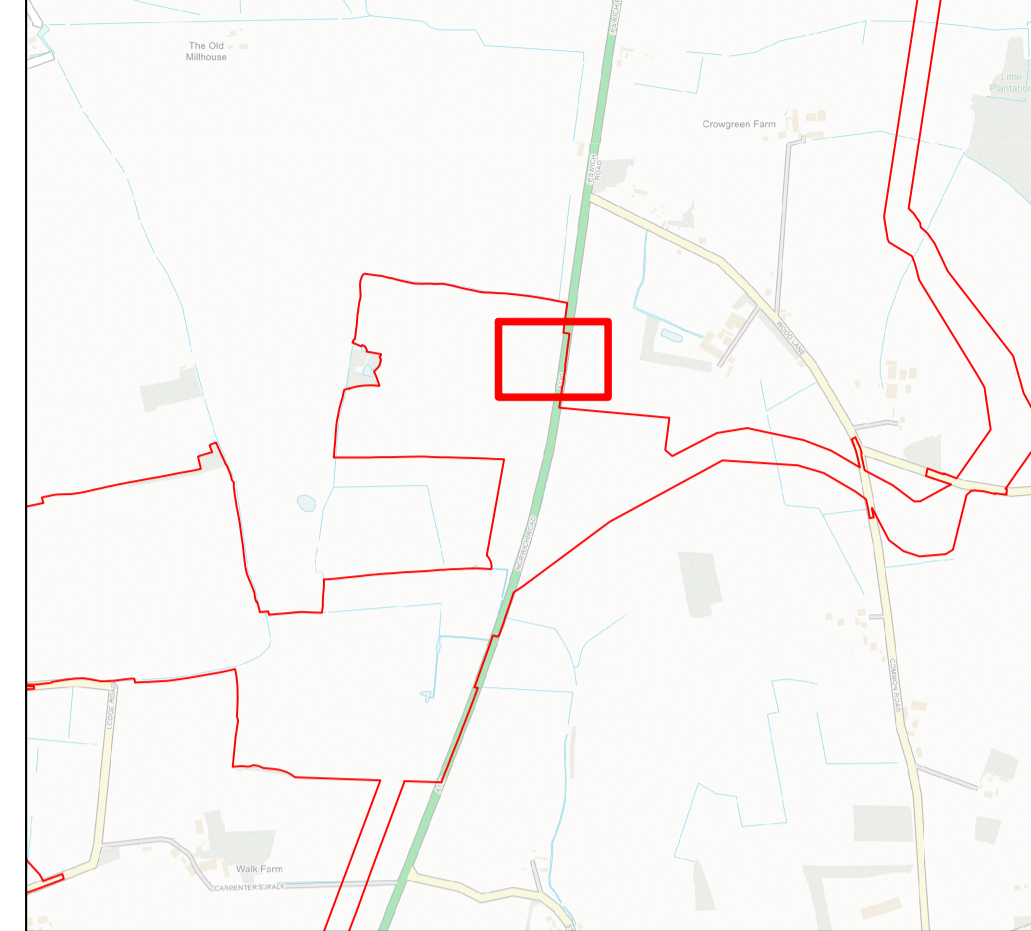
16.5m HGV VEHICLE SWEEP PATH - VEHICLE IN



INDICATIVE JUNCTION LAYOUT



LOCATION PLAN



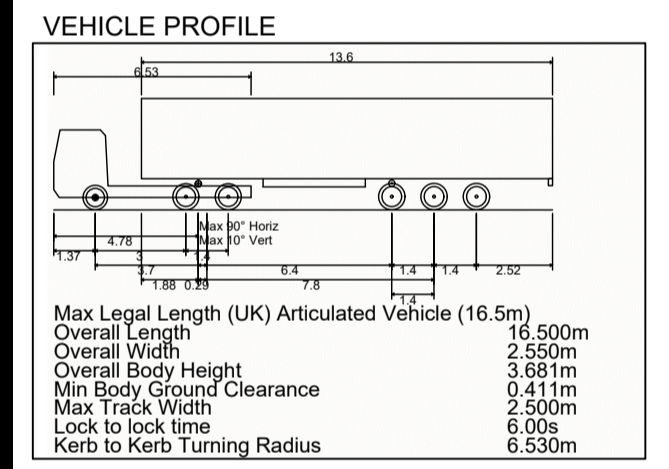
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 - COMMON ROAD IS SUBJECT TO THE NATIONAL SPEED LIMIT
 - VEHICLE TRACKING AND VISIBILITY IS BASED ON MAXIMUM LEGAL VEHICLE SIZE, THROUGH DETAILED DESIGN AND VEHICLE SELECTION THE REQUIREMENTS FOR HEDGE REMOVAL AND EXTENT OF HARD SURFACING WHERE PRACTICAL MAY BE REDUCED.
 - VEHICLES EXPECTED TO TRAVEL TO/FROM ACCESS A10 VIA A140 IPSWICH ROAD.
 - PHASE USE OF ACCESS A10 - CONSTRUCTION & OPERATION. A10 HIGHWAY ACCESS TO PROVIDE ACCESS TO 2C.

LEGEND

- PUBLIC RIGHT OF WAY (PROW)
- ORDER LIMIT
- 2.4 x 215m VISIBILITY SPLAY
- VISIBILITY ADDITIONAL AREA



FIRST ISSUE	JO	TH	2024.02.11
Issued/Revision	By	Appd	YYYY.MM.DD
	KWM	KWM	JO
	Dwn.	Dsgn.	Chkd.
			2025.12.09
			YYYY.MM.DD

Issue Status
S2 - FOR INFORMATION

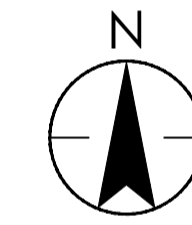
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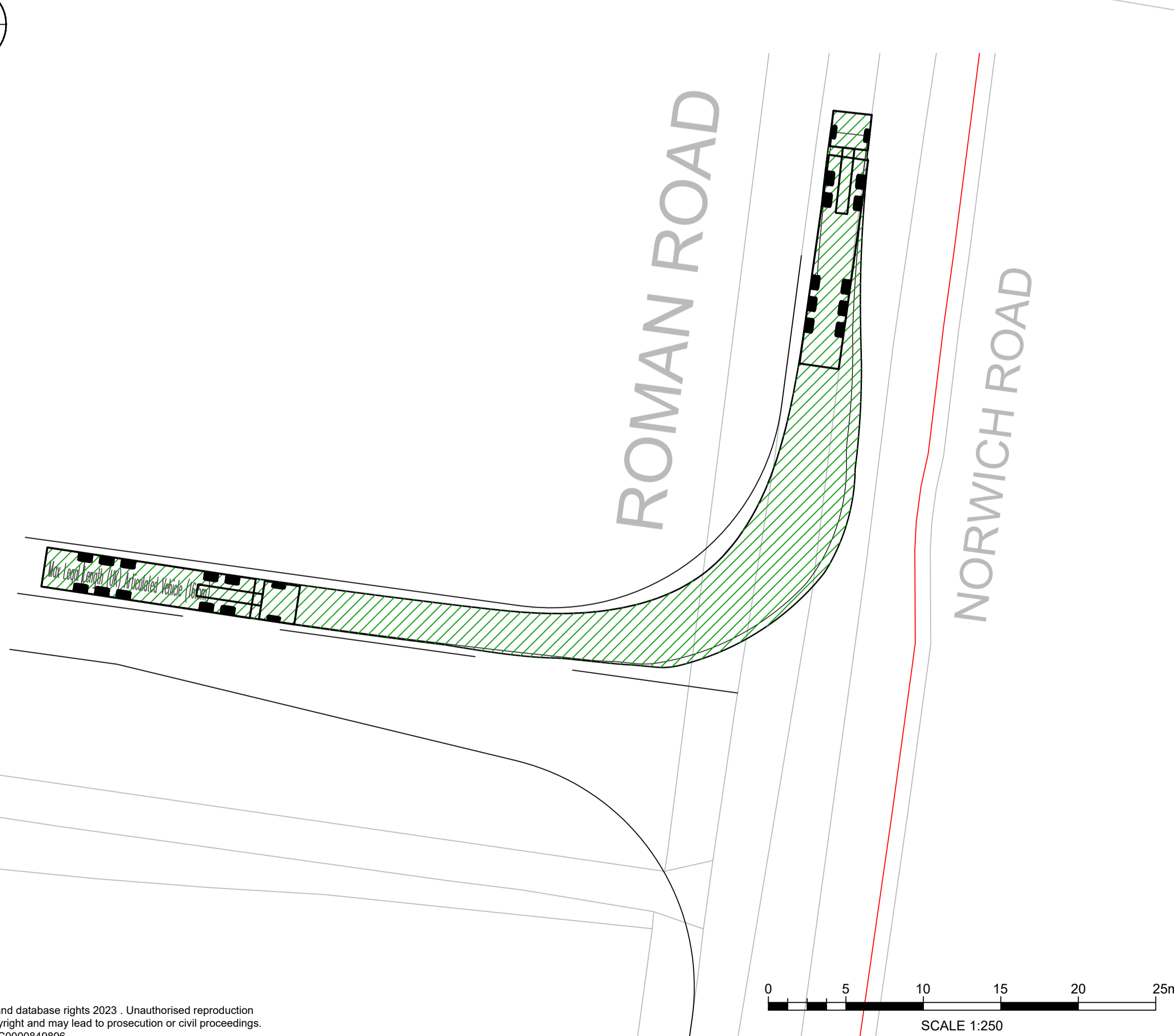
Client/Project
ISLAND GREEN POWER
EAST PYE SOLAR FARM
DEVELOPMENT CONSENT ORDER

Title
INDICATIVE SITE ACCESS A10
A140 IPSWICH ROAD
16.5m HGV
SHEET 55

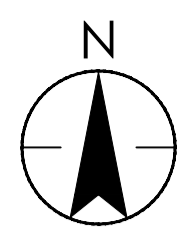
Project No. 333101678
Revision P01
A1 Scale AS SHOWN
Drawing No. 333101678-STN-HGN-XX-DR-TR-5500



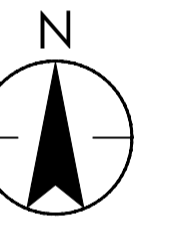
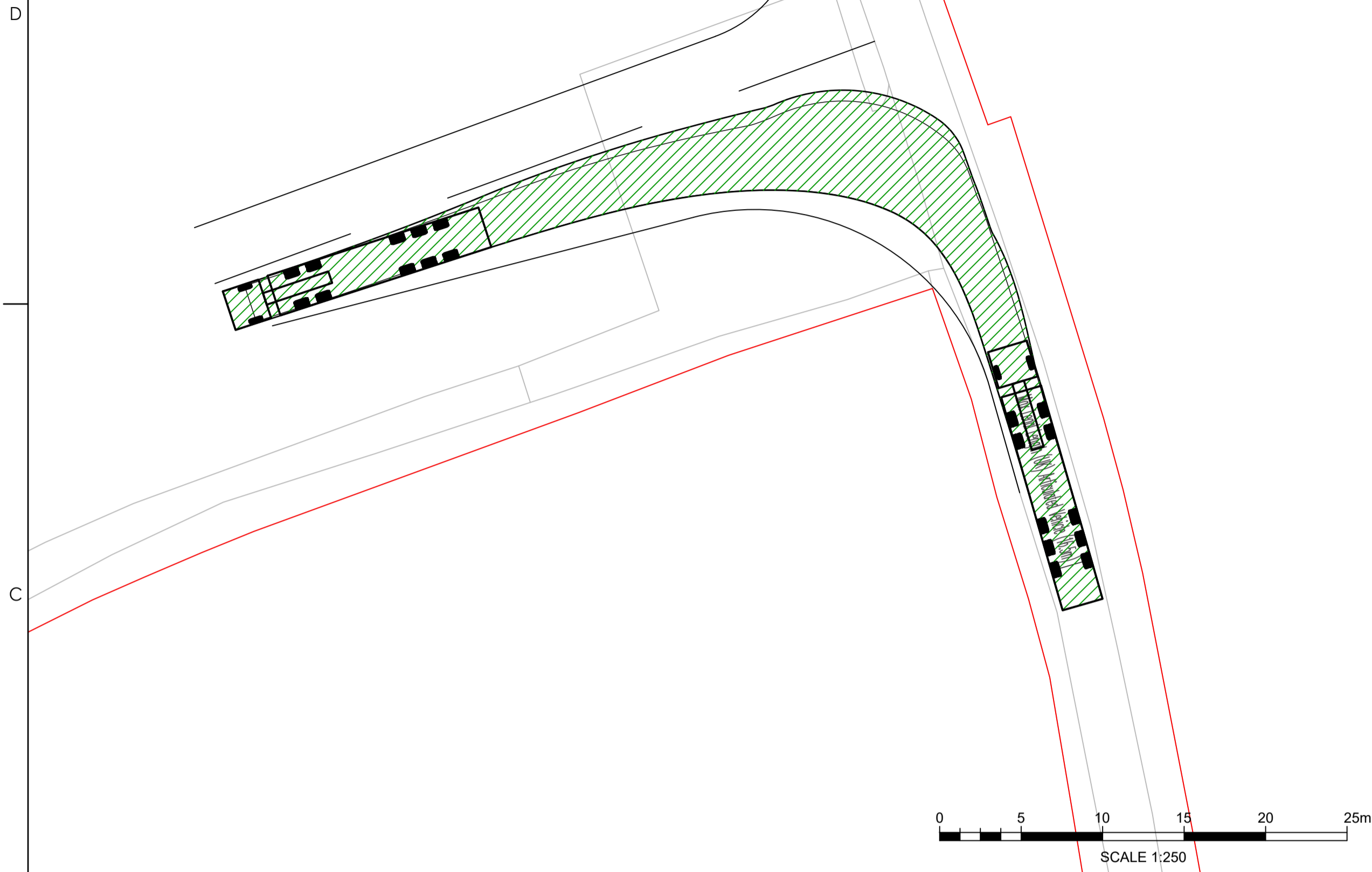
16.5m HGV VEHICLE SWEEP PATH - VEHICLE OUT



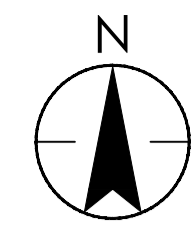
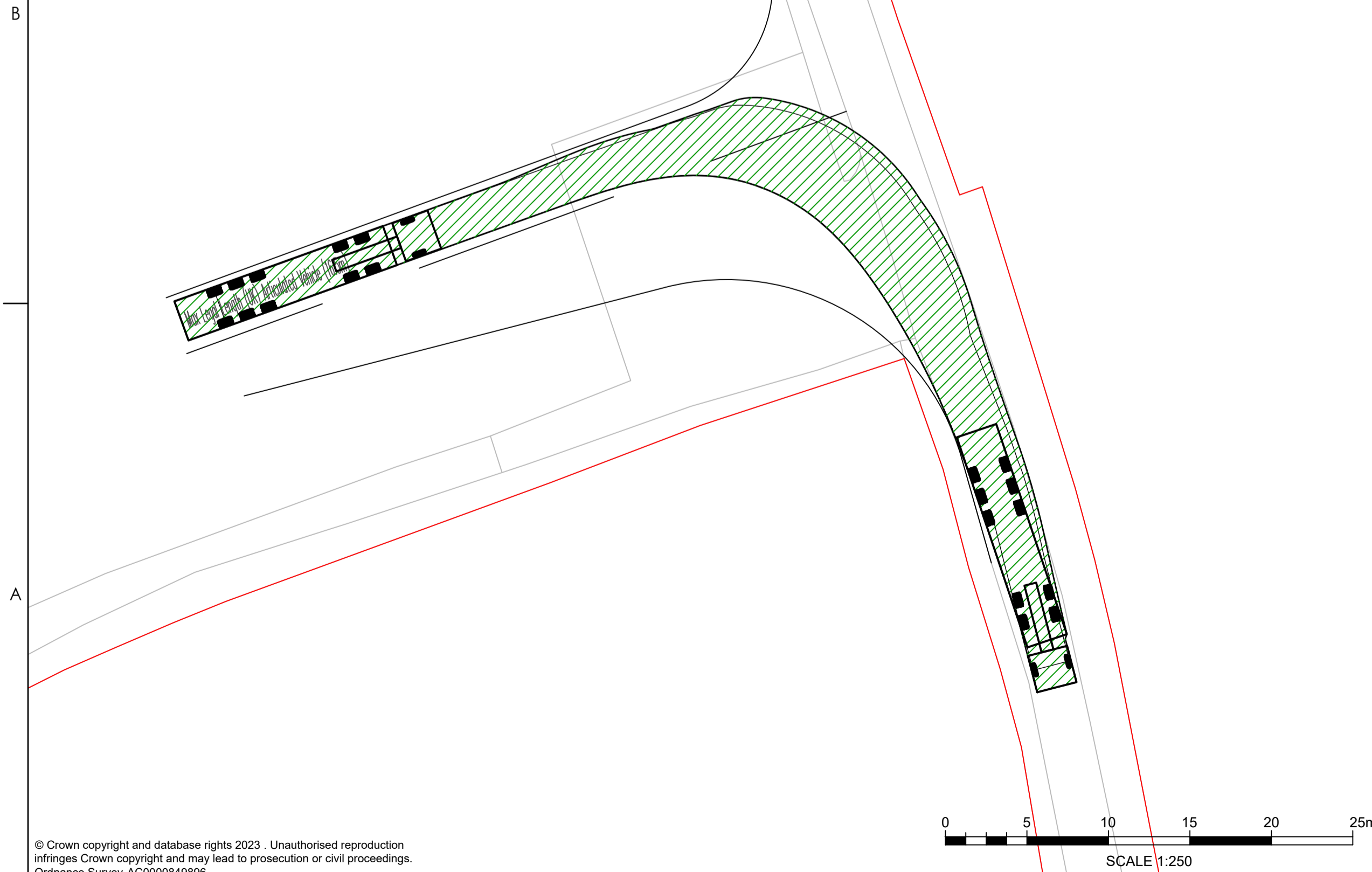
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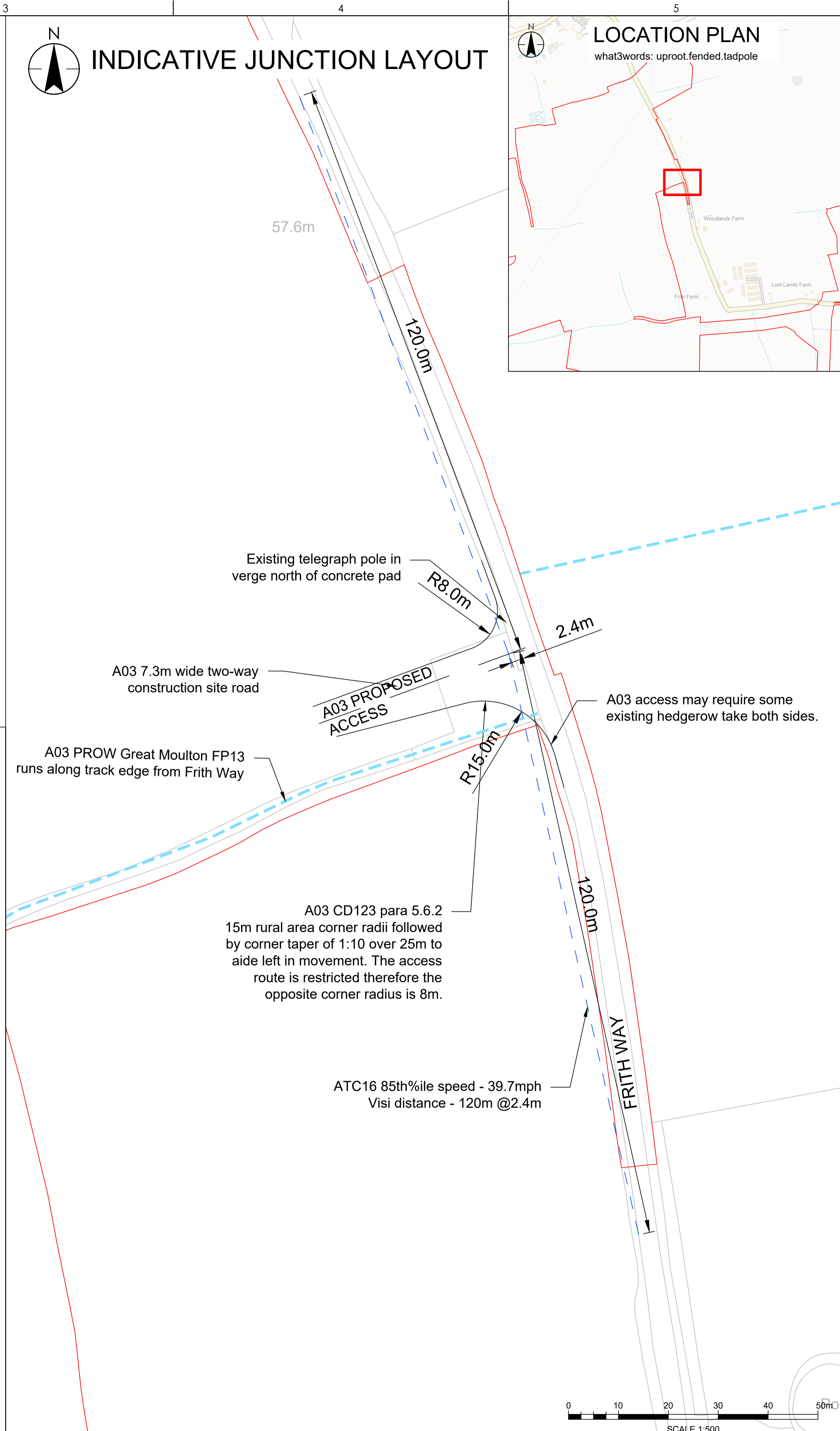
16.5m HGV VEHICLE SWEEP PATH - VEHICLE IN



16.5m HGV VEHICLE SWEEP PATH - VEHICLE OUT



INDICATIVE JUNCTION LAYOUT



LOCATION PLAN

what3words: uproot.fended.tadpole



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 - VEHICLE TRACKING MODELED AT A FORWARD DESIGN SPEED OF 5 KM/H FOR A 16.5 M HGV TRACKED RIGHT-IN/LEFT-OUT, FORWARD-DRIVE ONLY.
 - FRITH WAY IS SUBJECT TO THE NATIONAL SPEED LIMIT
 - VEHICLE TRACKING AND VISIBILITY IS BASED ON MAXIMUM LEGAL VEHICLE SIZE, THROUGH DETAILED DESIGN AND VEHICLE SELECTION THE REQUIREMENTS FOR HEDGE REMOVAL AND EXTENT OF HARD SURFACING WHERE PRACTICAL MAY BE REDUCED.
 - VEHICLES EXPECTED TO TRAVEL TO / FROM ACCESS A03 VIA A140 IPSWICH ROAD / B1134 STATION ROAD / LODGE ROAD / FRITH WAY.
 - PHASE USE OF ACCESS A03 - CONSTRUCTION & OPERATION. A03 HIGHWAY ACCESS TO PROVIDE ACCESS TO OHL WORKS.

LEGEND

	PUBLIC RIGHT OF WAY (PROW)
	ORDER LIMIT
	2.4 x 120m VISIBILITY SPLAY
	VISIBILITY ADDITIONAL AREA

VEHICLE PROFILE

Max Legal Length (UK) Articulated Vehicle (16.5m)	16.500m
Overall Length	2.550m
Overall Width	2.861m
Overall Body Height	0.411m
Min Body Ground Clearance	2.500m
Max Track Width	6.00s
Lock to lock time	6.530m
Kerb to Kerb Turning Radius	6.530m

FIRST ISSUE	JO	TH	2024.02.11
Issued/Revision	By	Appd	YYYY.MM.DD
	KWM	KWM	2025.12.09
	Dwn.	Dsgn.	Chkd.
			YYYY.MM.DD

Issue Status

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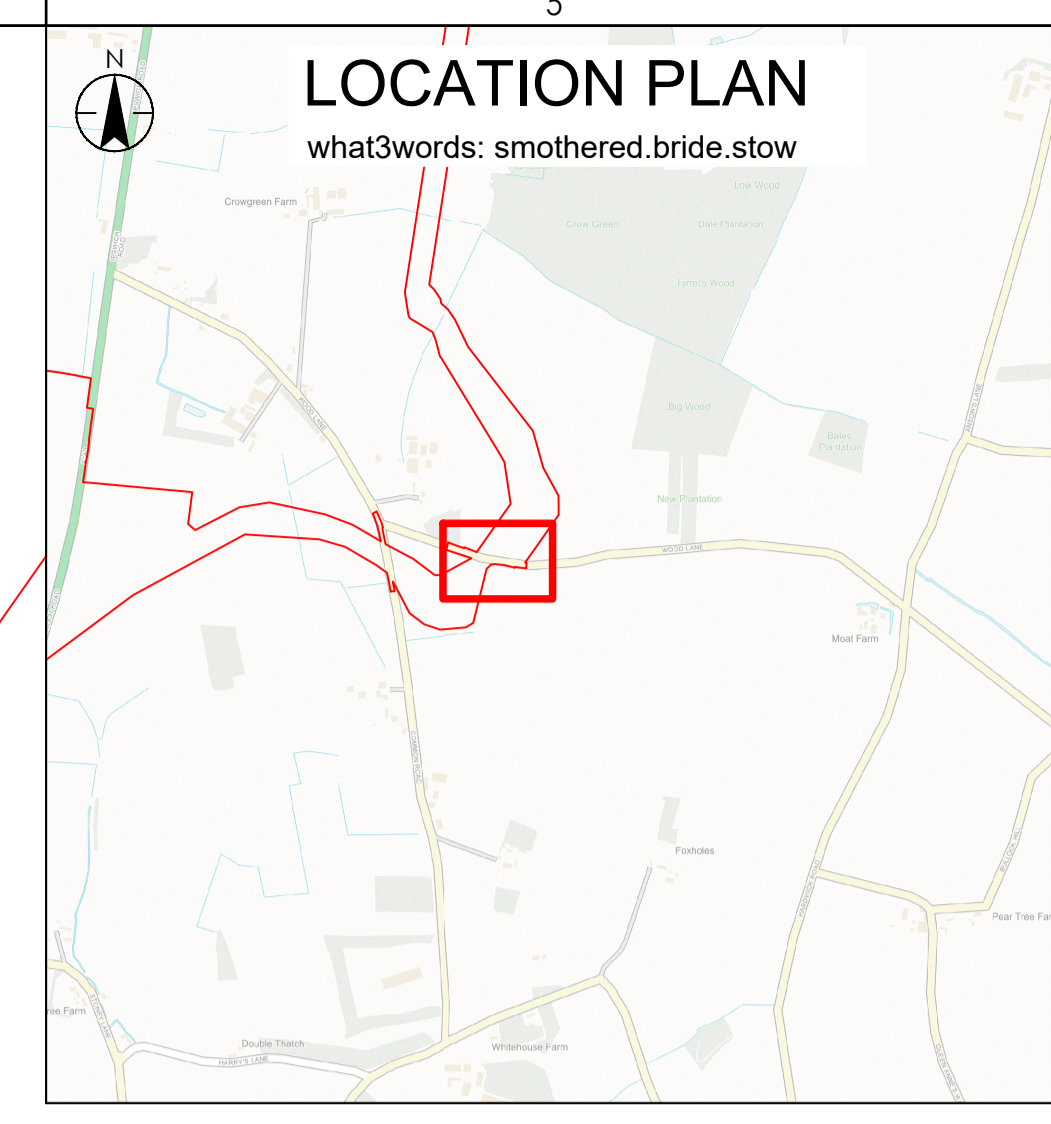
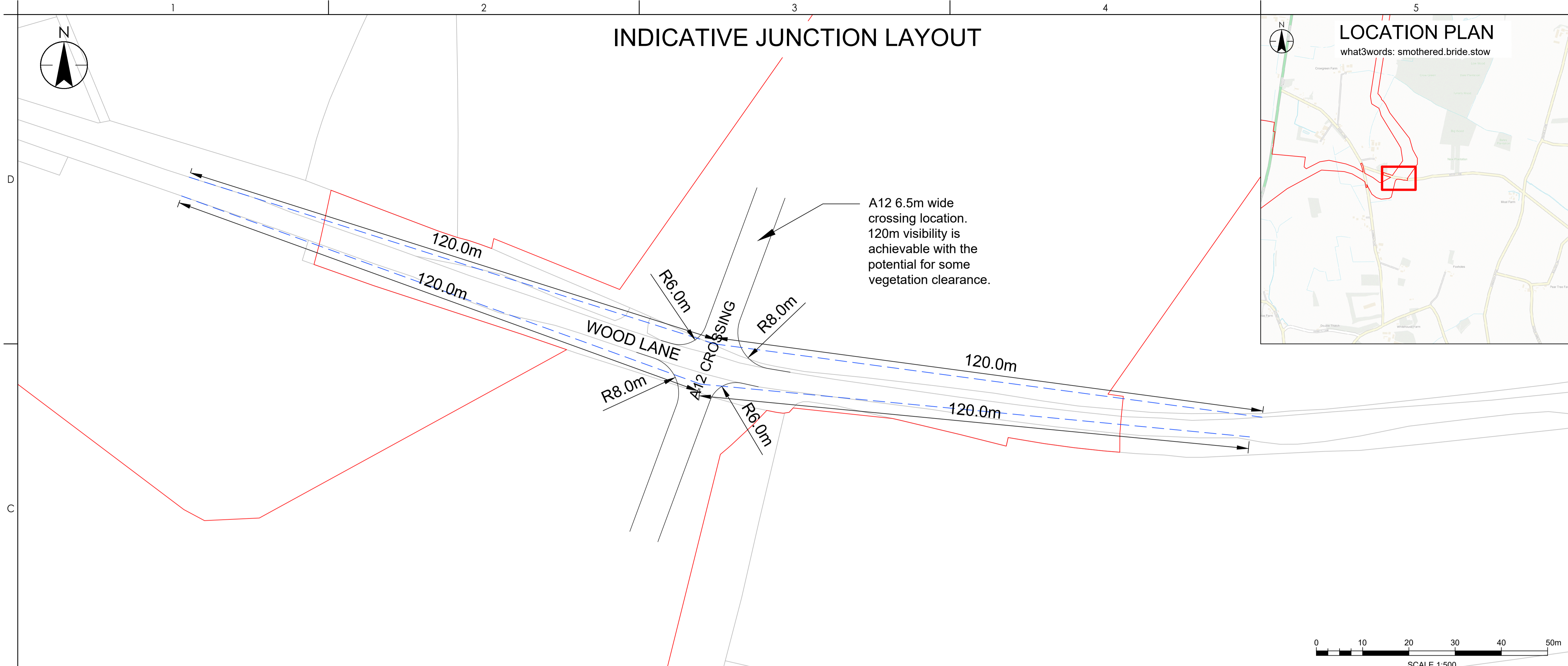
Client/Project
ISLAND GREEN POWER
EAST PYE SOLAR FARM
DEVELOPMENT CONSENT ORDER

Title
INDICATIVE SITE ACCESS A03
FRITH WAY
16.5m HGV
SHEET 56

Project No. 333101678	A1 Scale AS SHOWN
Revision P01	Drawing No. 333101678-STN-HGN-XX-DR-TR-5500

P:\03\333101678\03\28637.PW by: [Name] [Date] [Time]
Drawing No. 333101678-STN-HGN-XX-DR-TR-5500

INDICATIVE JUNCTION LAYOUT



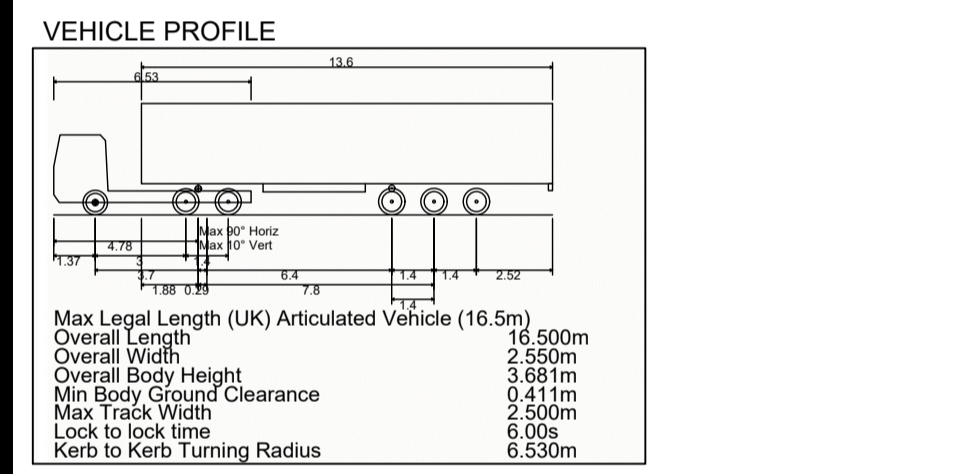
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 - VEHICLES EXPECTED TO TRAVEL TO / FROM ACCESS A12 VIA CABLE ROUTE CORRIDOR ONLY
 - PHASE USE OF ACCESS A12 - CONSTRUCTION & OPERATION. A12 HIGHWAY ACCESS TO PROVIDE ACCESS TO OHL WORKS.

LEGEND

- PUBLIC RIGHT OF WAY (PROW)
- ORDER LIMIT
- 2.4 x 120m VISIBILITY SPLAY
- VISIBILITY ADDITIONAL AREA



P01	FIRST ISSUE	JO	TH	2024.02.11
	Issued/Revision	By	Appd	YYYY.MM.DD
		KWM	KWM	JO
		Dwn.	Dsgn.	Chkd.
				2025.12.09
				YYYY.MM.DD

Issue Status

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Client/Project
ISLAND GREEN POWER

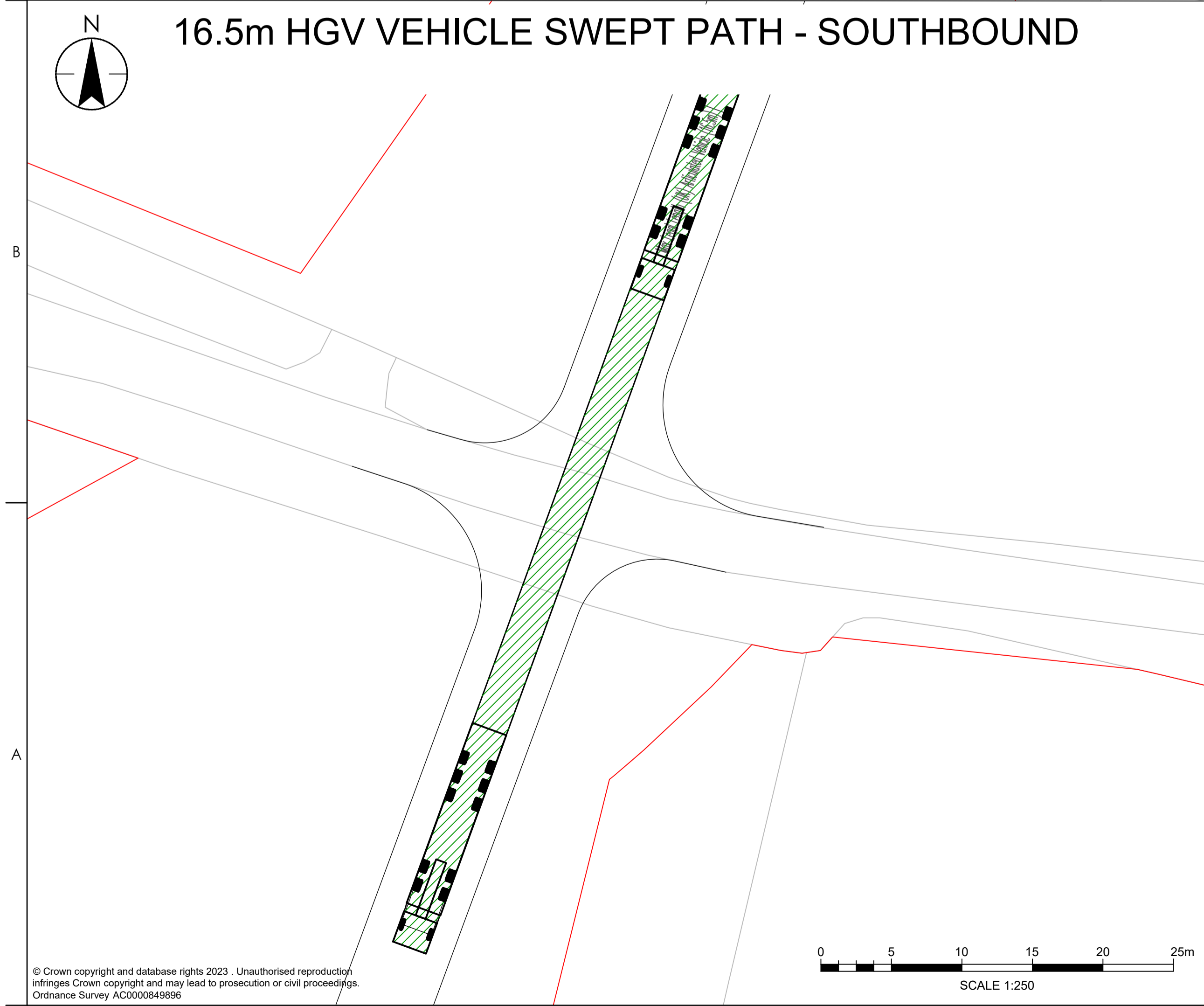
EAST PYE SOLAR FARM

DEVELOPMENT CONSENT ORDER

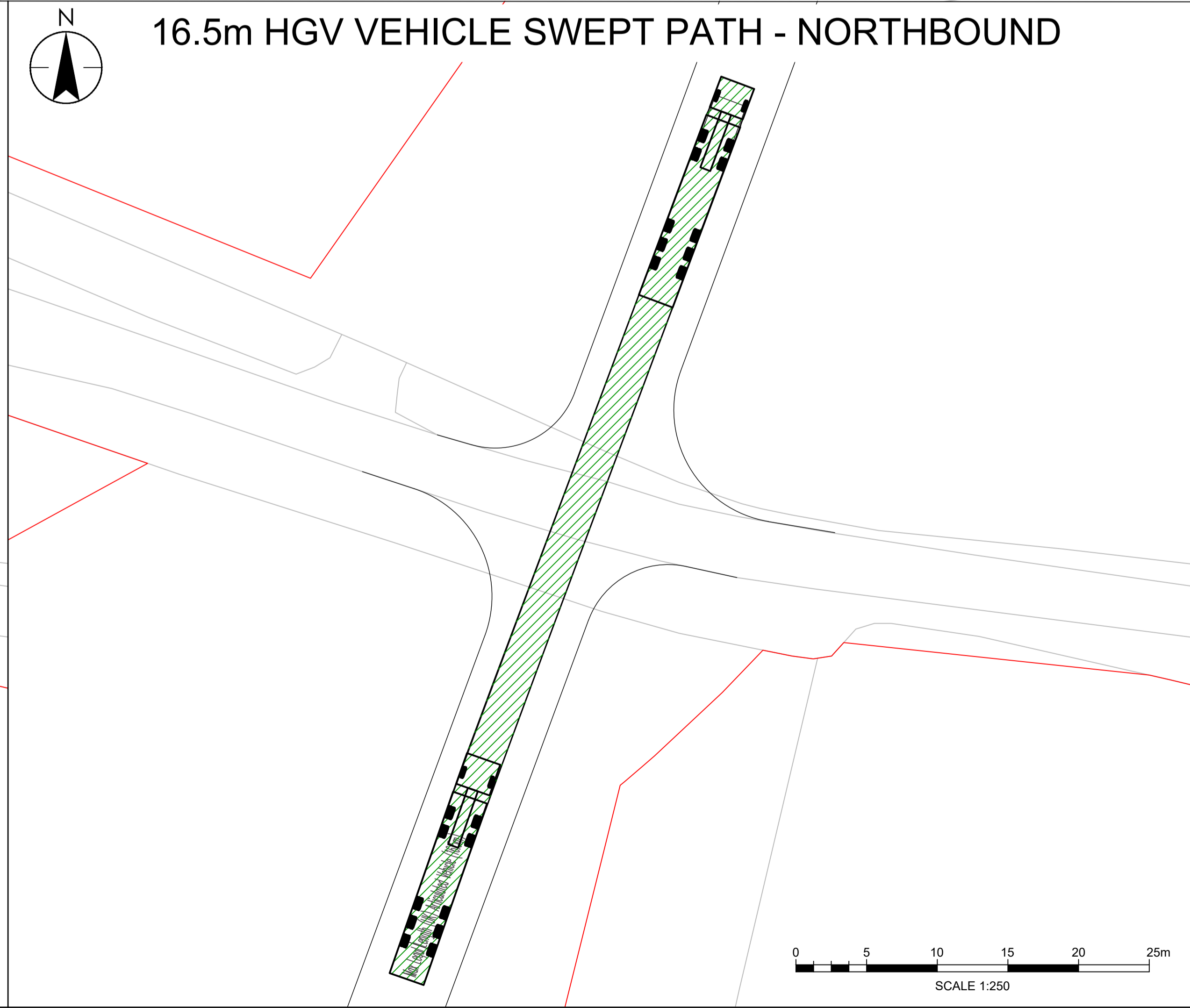
Title
INDICATIVE SITE ACCESS A12
WOOD LANE
16.5m HGV
SHEET 57

Project No.	333101678	A1 Scale	AS SHOWN
Revision	P01	Drawing No.	333101678-STN-HGN-XX-DR-TR-5500

16.5m HGV VEHICLE SWEEP PATH - SOUTHBOUND

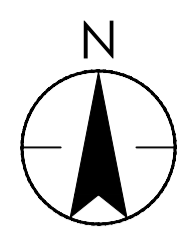


16.5m HGV VEHICLE SWEEP PATH - NORTHBOUND

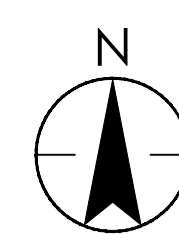
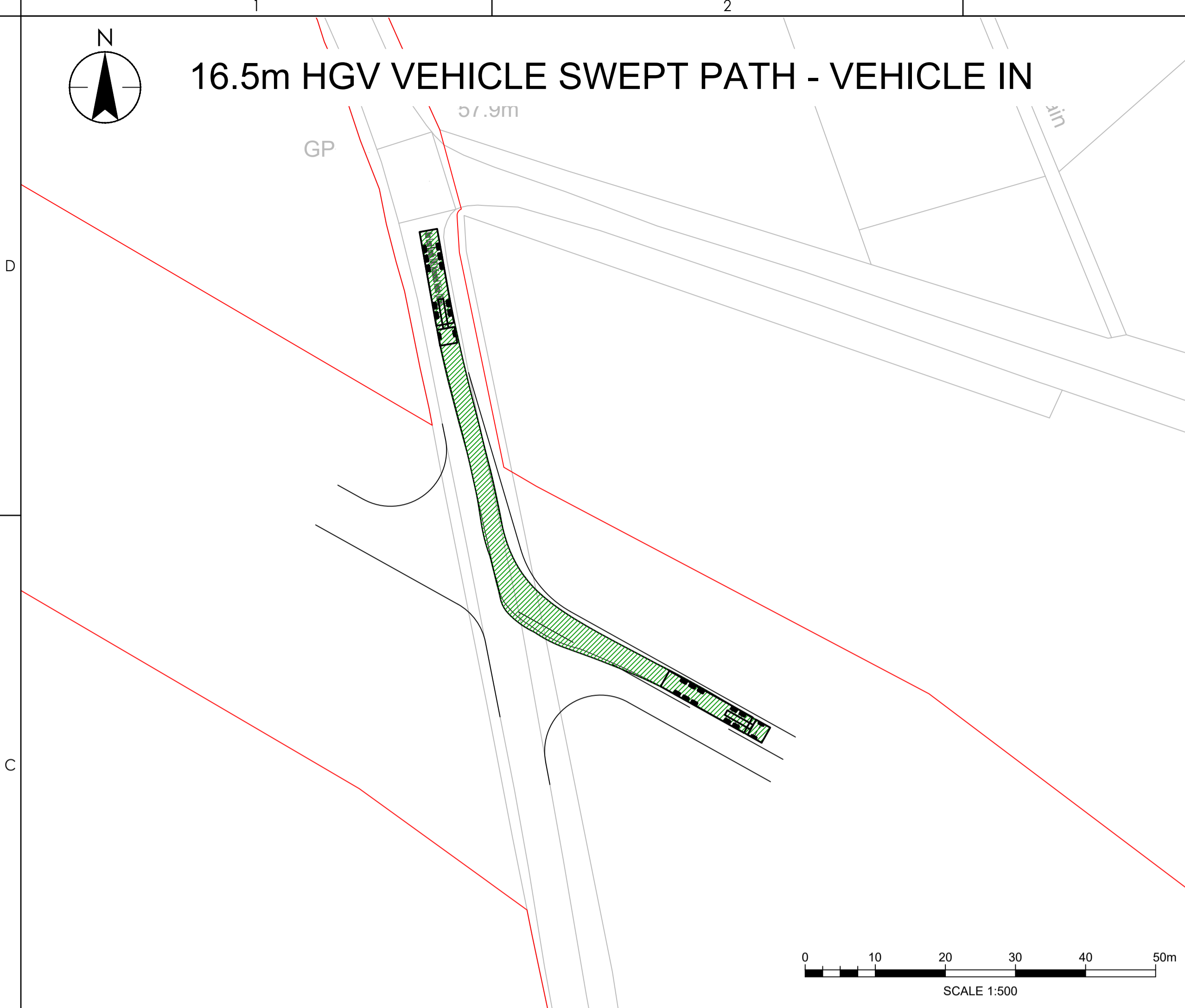


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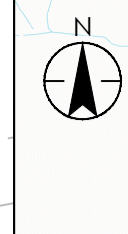
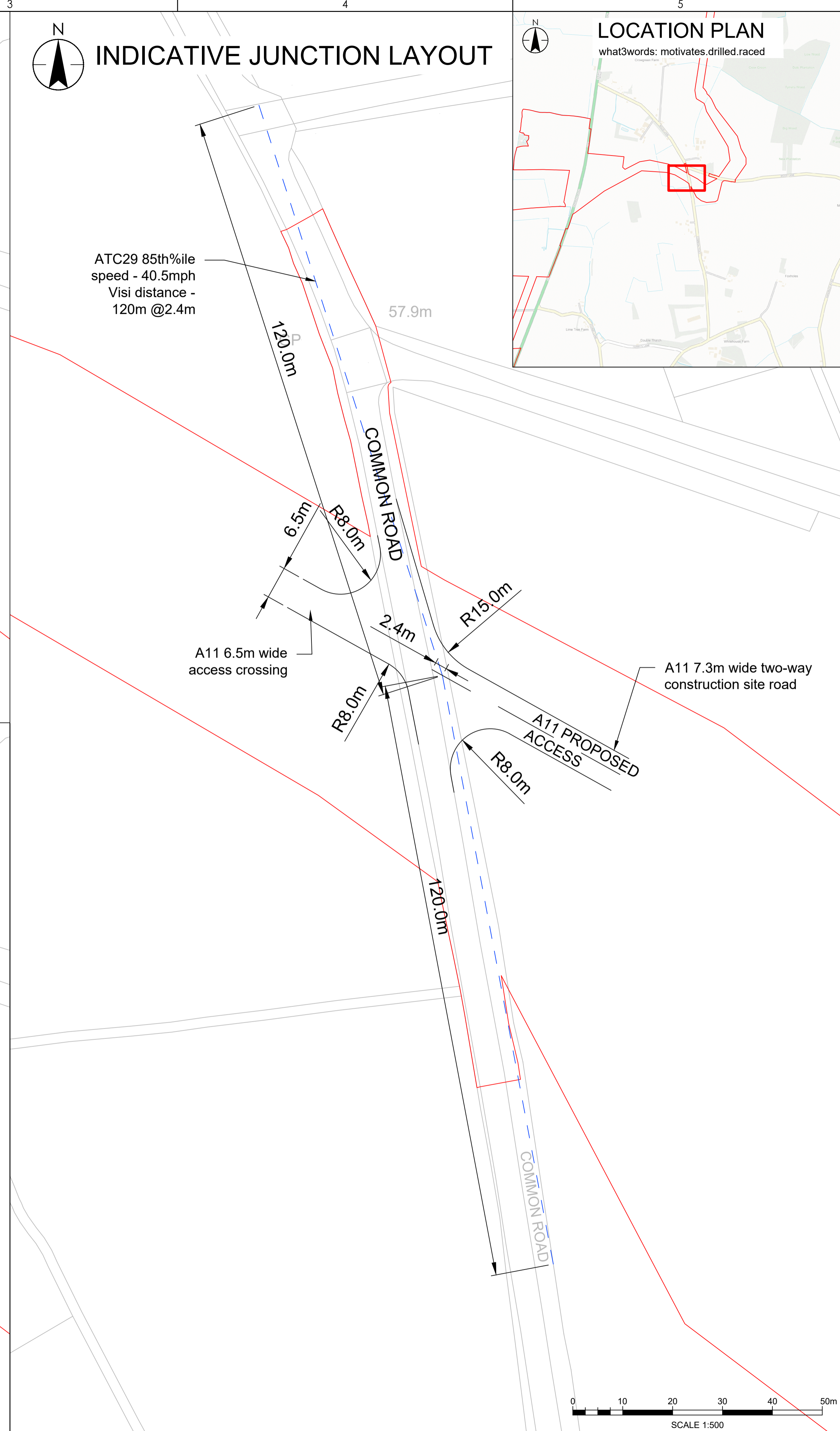
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16.5m HGV VEHICLE SWEEP PATH - VEHICLE IN

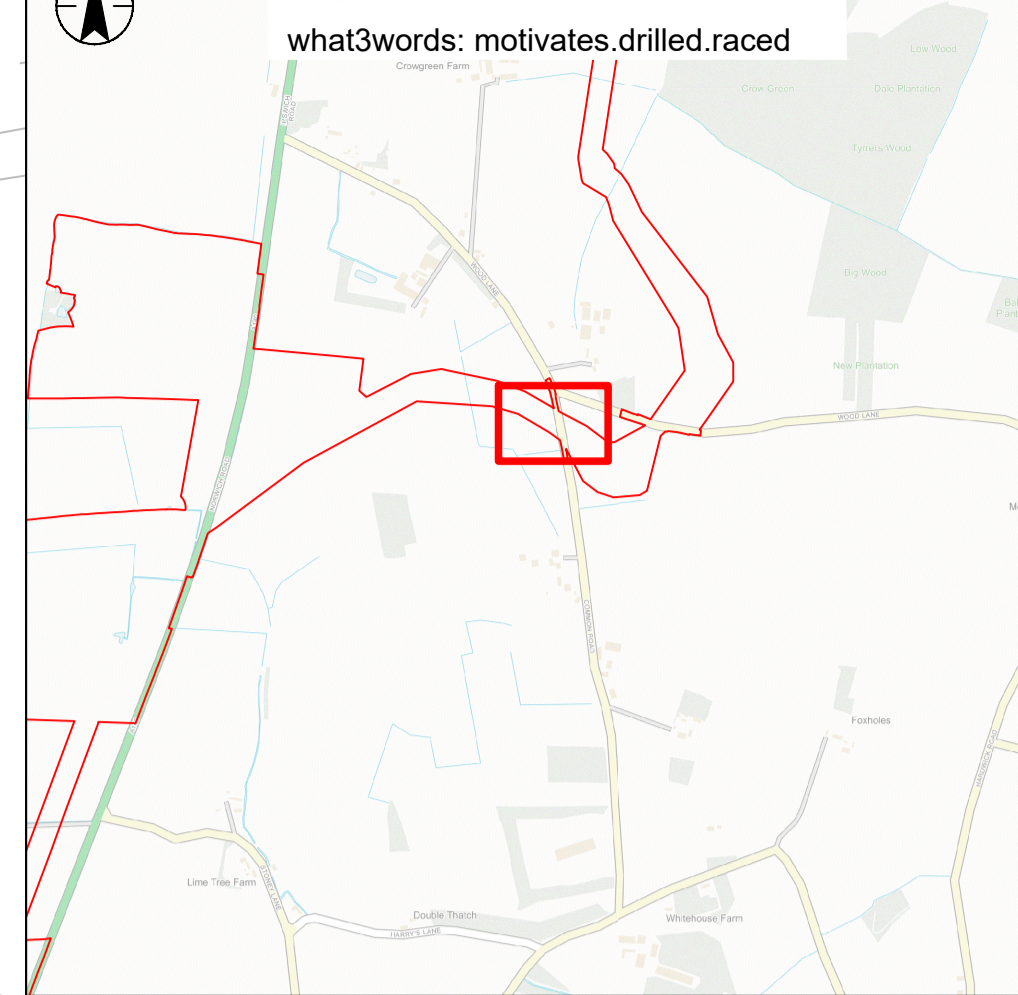


INDICATIVE JUNCTION LAYOUT



LOCATION PLAN

what3words: motivates.drilled.raced



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Notes

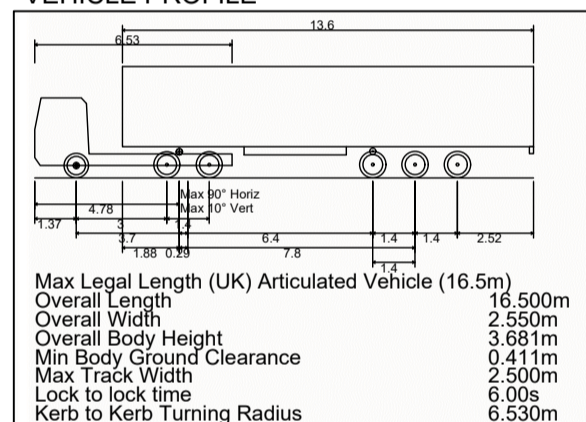
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- VEHICLE TRACKING MODELED AT A FORWARD DESIGN SPEED OF 5 KM/H.
- COMMON ROAD IS SUBJECT TO THE NATIONAL SPEED LIMIT
- VEHICLE TRACKING AND VISIBILITY IS BASED ON MAXIMUM LEGAL VEHICLE SIZE, THROUGH DETAILED DESIGN AND VEHICLE SELECTION THE REQUIREMENTS FOR HEDGE REMOVAL AND EXTENT OF HARD SURFACING WHERE PRACTICAL MAY BE REDUCED.
- VEHICLES EXPECTED TO TRAVEL TO/FROM ACCESS A11 VIA A140 IPSWICH ROAD / WOOD LANE / COMMON ROAD.
- PHASE USE OF ACCESS A11 - CONSTRUCTION. A11 HIGHWAY ACCESS TO PROVIDE ACCESS TO CRC4.

LEGEND

- PUBLIC RIGHT OF WAY (PROW)
- ORDER LIMIT
- 2.4 x 120m VISIBILITY SPLAY
- VISIBILITY ADDITIONAL AREA

VEHICLE PROFILE



FIRST ISSUE	JO	TH	2024.02.11
Issued/Revision	By	Appd	YYYY.MM.DD
	KWM	KWM	JO
	Dwn.	Dsgn.	Chkd.
			2025.12.09
			YYYY.MM.DD

Issue Status

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Client/Project
ISLAND GREEN POWER

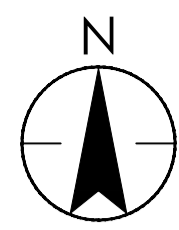
EAST PYE SOLAR FARM

DEVELOPMENT CONSENT ORDER

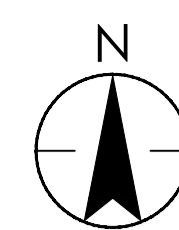
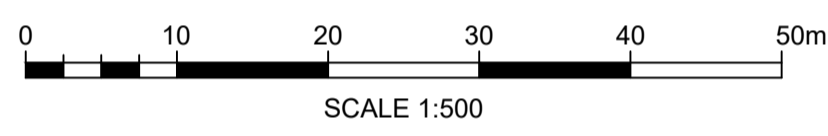
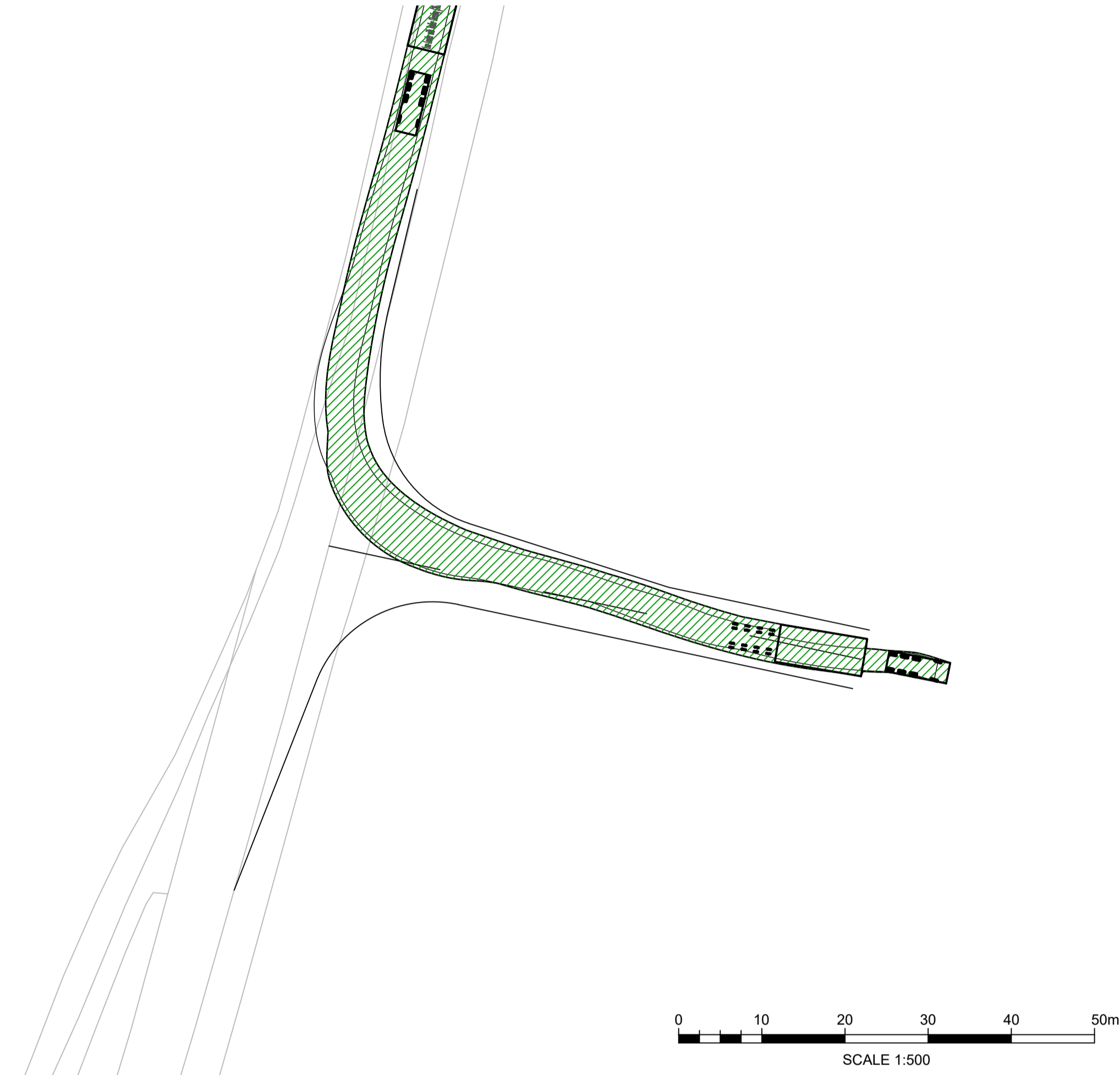
Title
INDICATIVE SITE ACCESS A11
COMMON ROAD
16.5m HGV
SHEET 58

Project No. 333101678	A1 Scale AS SHOWN
Revision P01	Drawing No. 333101678-STN-HGN-XX-DR-TR-5500

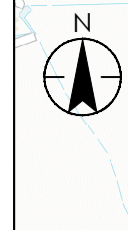
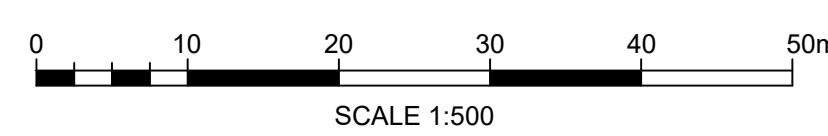
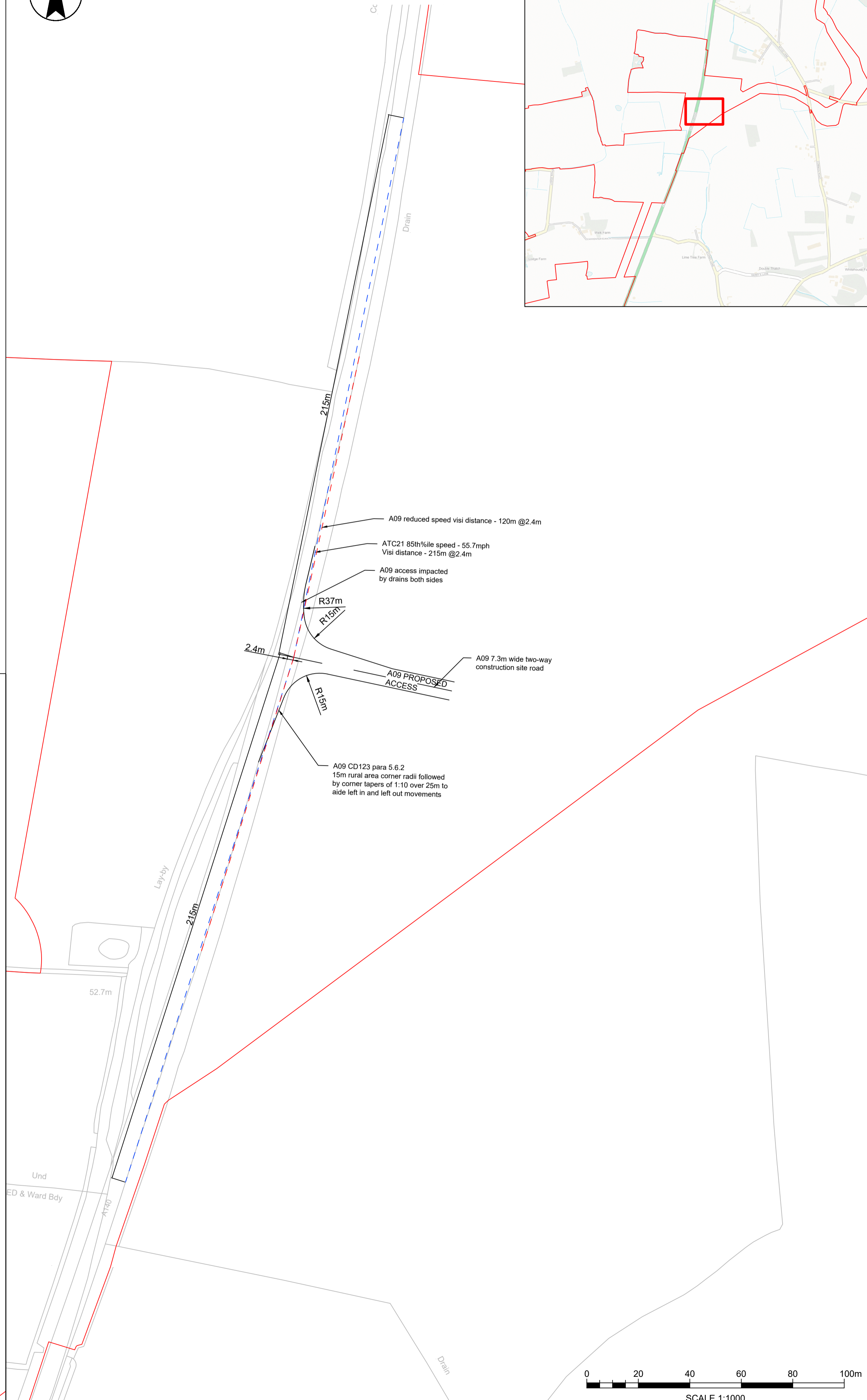
P:\033101678\024\030\A11\16.5m HGV\16.5m HGV Vehicle Sweep Path - Vehicle In.dwg (16.5m HGV Vehicle Sweep Path - Vehicle In.dwg) - 11/11/2024 10:00:00 AM



26.5m AIL VEHICLE SWEEP PATH - VEHICLE IN

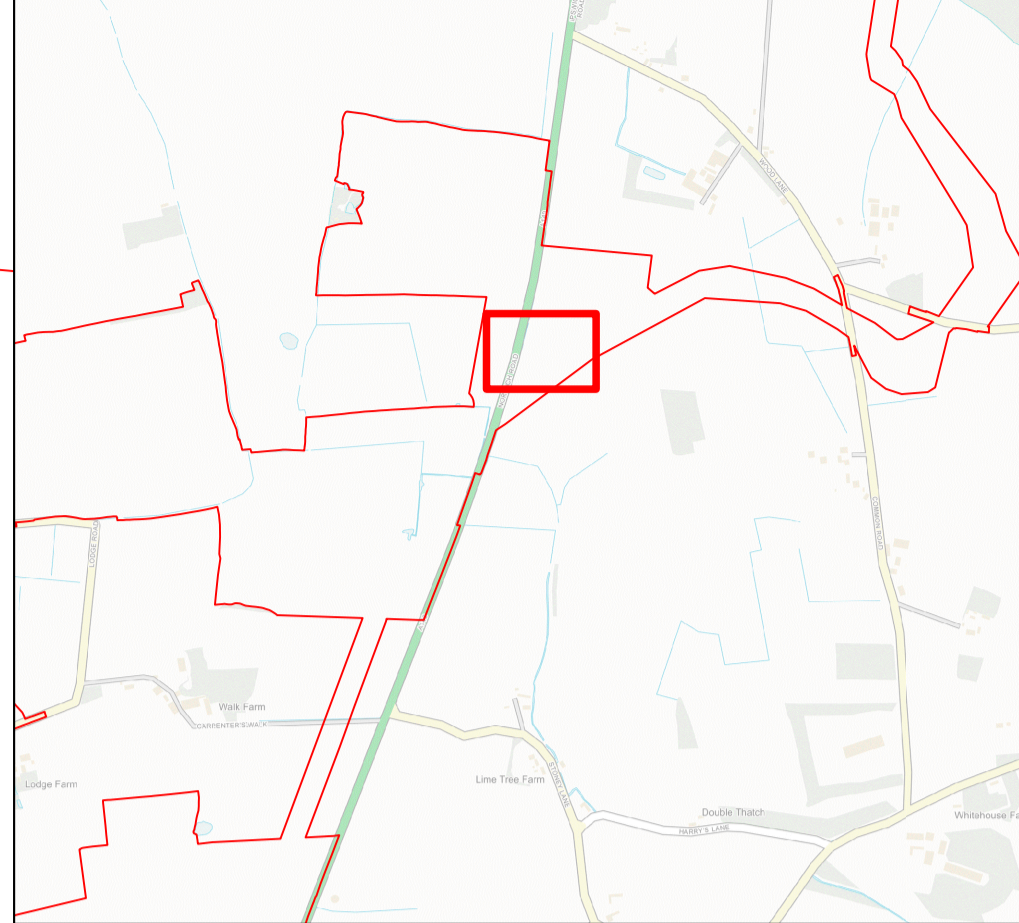


INDICATIVE JUNCTION LAYOUT



LOCATION PLAN

what3words: storyline.bigger.configure



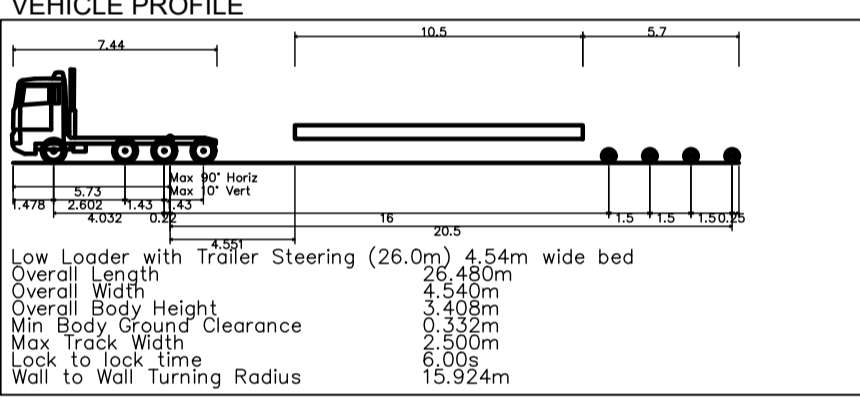
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 - A140 IPSWICH ROAD IS SUBJECT TO THE NATIONAL SPEED LIMIT
 - VEHICLE TRACKING AND VISIBILITY IS BASED ON MAXIMUM LEGAL VEHICLE SIZE, THROUGH DETAILED DESIGN AND VEHICLE SELECTION THE REQUIREMENTS FOR HEDGE REMOVAL AND EXTENT OF HARD SURFACING WHERE PRACTICAL MAY BE REDUCED.
 - VEHICLES EXPECTED TO TRAVEL TO / FROM ACCESS A09 VIA A140 IPSWICH ROAD.
 - PHASE USE OF ACCESS A09 - CONSTRUCTION. A09 HIGHWAY ACCESS TO PROVIDE ACCESS TO CRC4 (EAST OF A140).

LEGEND

	PUBLIC RIGHT OF WAY (PROW)
	ORDER LIMIT
	2.4 x 215m VISIBILITY SPLAY
	2.4 x 120m VISIBILITY SPLAY
	VISIBILITY ADDITIONAL AREA



FIRST ISSUE	JO	TH	2024.02.11
Issued/Revision	By	Appd	YYYY.MM.DD
	KWM	KWM	JO
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			2025.12.09
			YYYY.MM.DD

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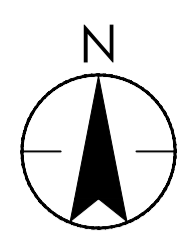
Client/Project
ISLAND GREEN POWER
EAST PYE SOLAR FARM
DEVELOPMENT CONSENT ORDER

Title
INDICATIVE SITE ACCESS A09
A140 IPSWICH ROAD
26.5m AIL
SHEET 59

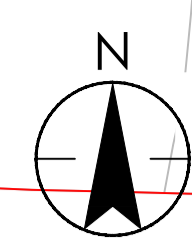
Project No.	333101678	A1 Scale	AS SHOWN
Revision	P01	Drawing No.	333101678-STN-HGN-XX-DR-TR-5500

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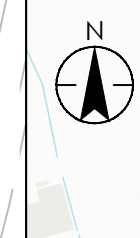
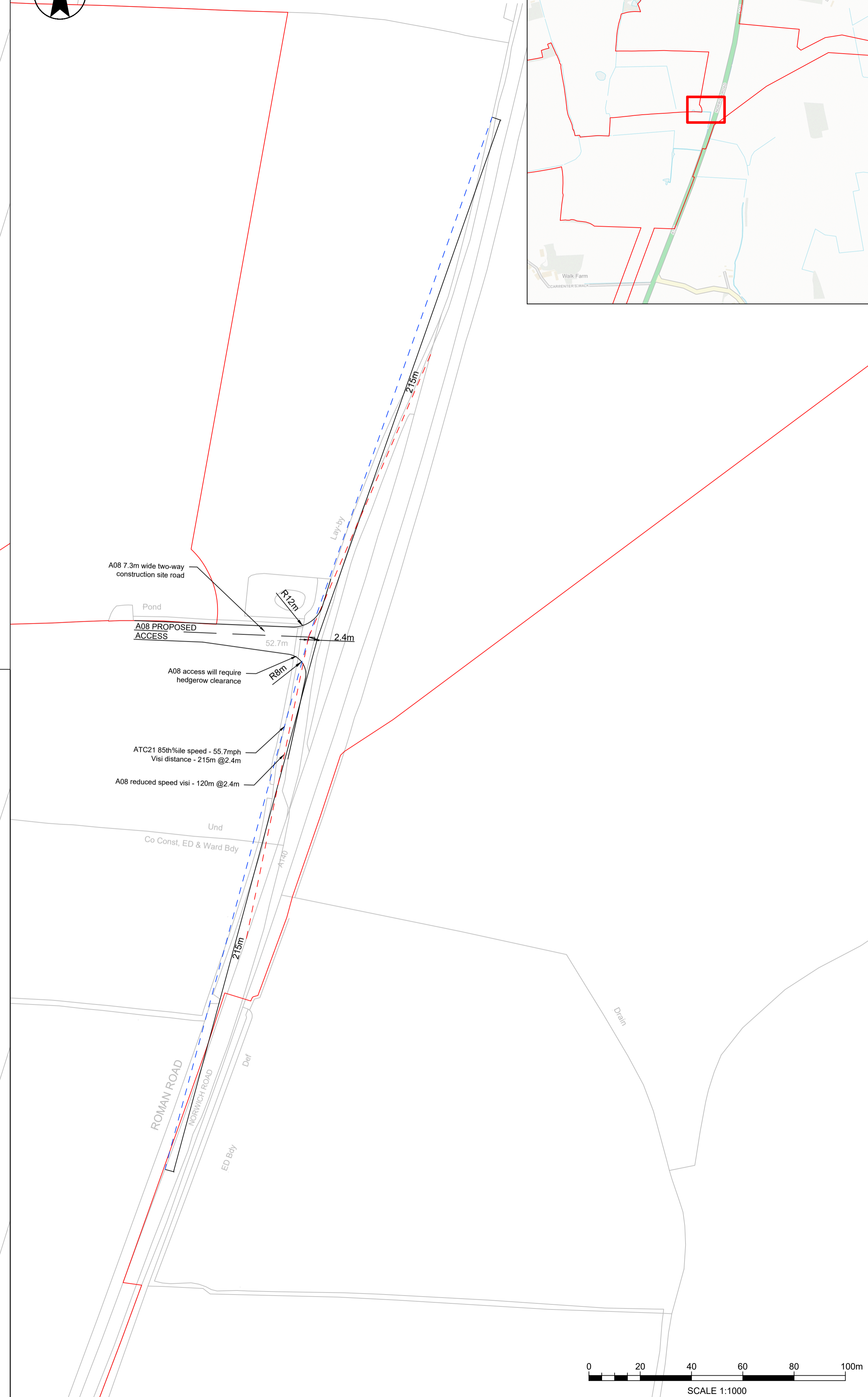
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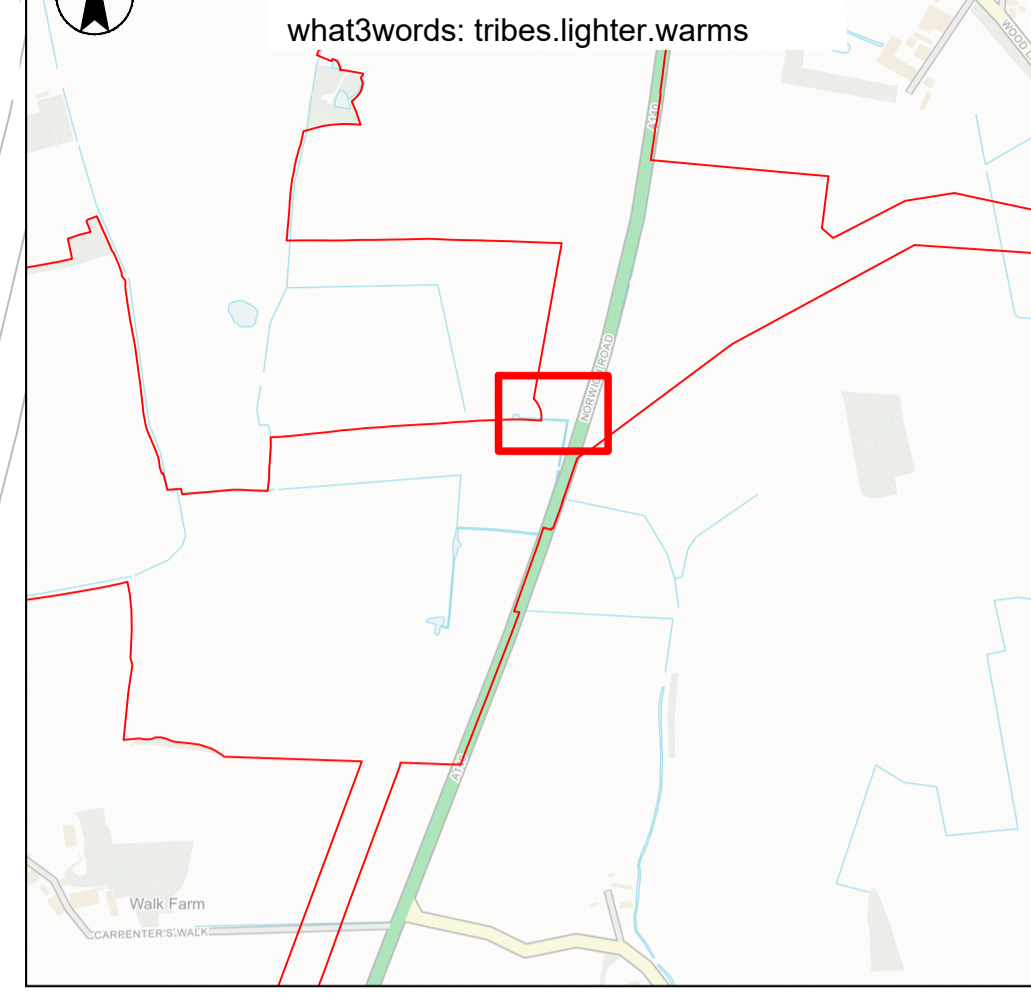
16.5m HGV VEHICLE SWEEP PATH - VEHICLE IN



INDICATIVE JUNCTION LAYOUT



LOCATION PLAN



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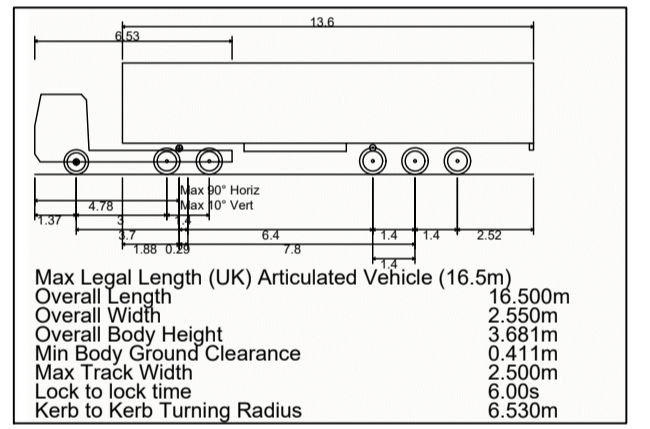
Notes

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- A140 IPSWICH ROAD IS SUBJECT TO THE NATIONAL SPEED LIMIT
- VEHICLE TRACKING AND VISIBILITY IS BASED ON MAXIMUM LEGAL VEHICLE SIZE. THROUGH DETAILED DESIGN AND VEHICLE SELECTION THE REQUIREMENTS FOR HEDGE REMOVAL AND EXTENT OF HARD SURFACING WHERE PRACTICAL MAY BE REDUCED.
- VEHICLES EXPECTED TO TRAVEL TO / FROM ACCESS A08 VIA A140 IPSWICH ROAD NORTHBOUND LAY-BY.
- PHASE USE OF ACCESS A08 - CONSTRUCTION & OPERATION. A08 HIGHWAY ACCESS TO PROVIDE ACCESS TO 2B & CRC4 (WEST OF A140).

LEGEND

- PUBLIC RIGHT OF WAY (PROW)
- ORDER LIMIT
- 2.4 x 160m VISIBILITY SPLAY
- 2.4 x 120m VISIBILITY SPLAY
- VISIBILITY ADDITIONAL AREA

VEHICLE PROFILE



FIRST ISSUE	JO	TH	2024.02.11
Issued/Revision	By	Appd	YYYY.MM.DD
	KWM	KWM	2025.12.09
	Dwn.	Dsgn.	Chkd. YYYY.MM.DD

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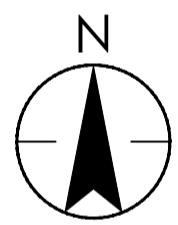
Client/Project Logo



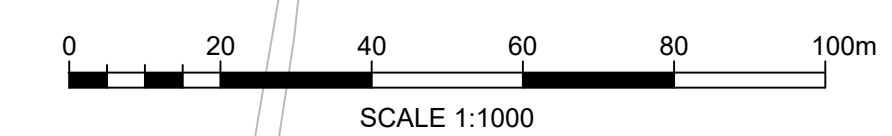
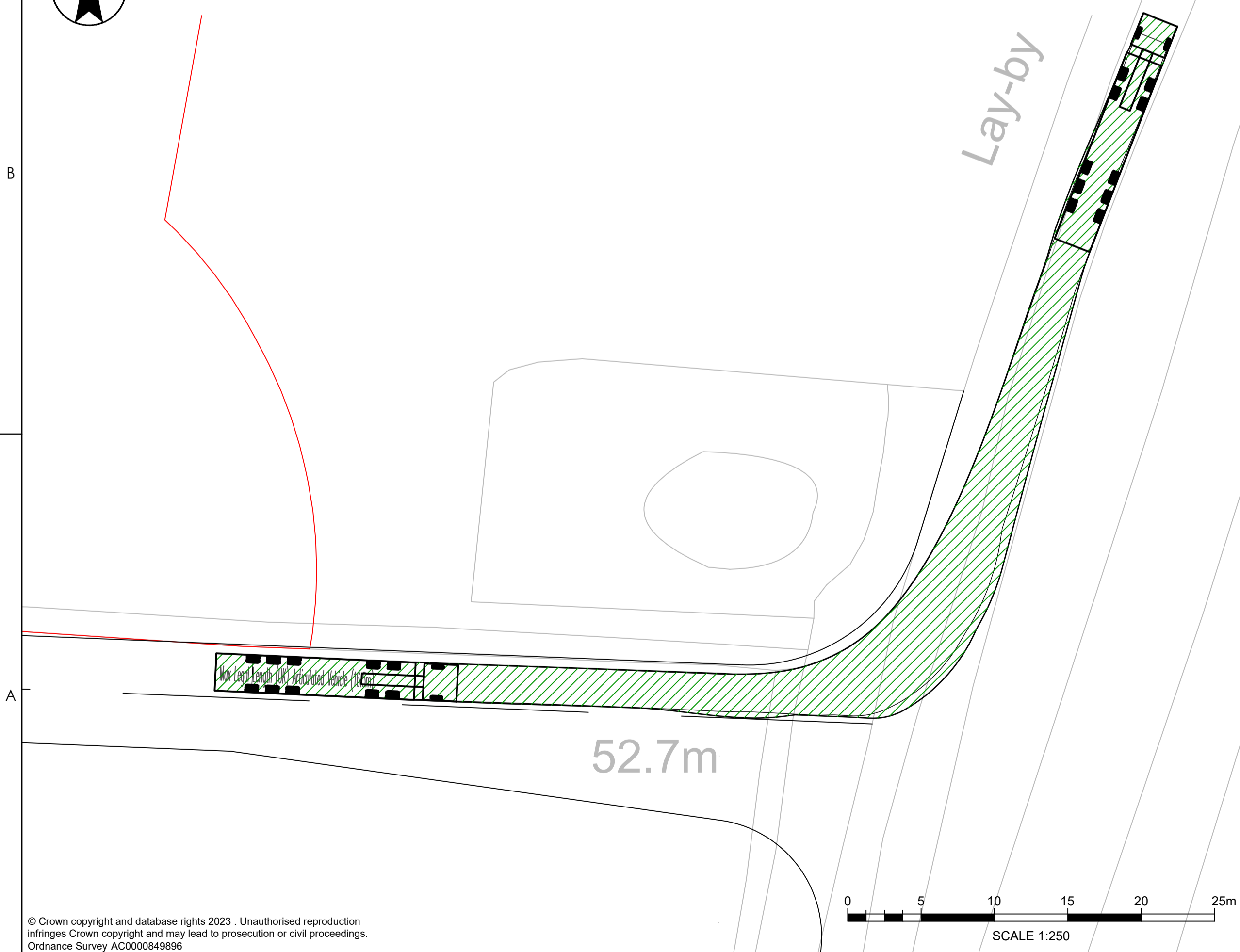
Client/Project
ISLAND GREEN POWER
EAST PYE SOLAR FARM
DEVELOPMENT CONSENT ORDER

Title
INDICATIVE SITE ACCESS A08
A140 IPSWICH ROAD
16.5m HGV
SHEET 60

Project No.	333101678	A1 Scale	AS SHOWN
Revision	P01	Drawing No.	333101678-STN-HGN-XX-DR-TR-5500

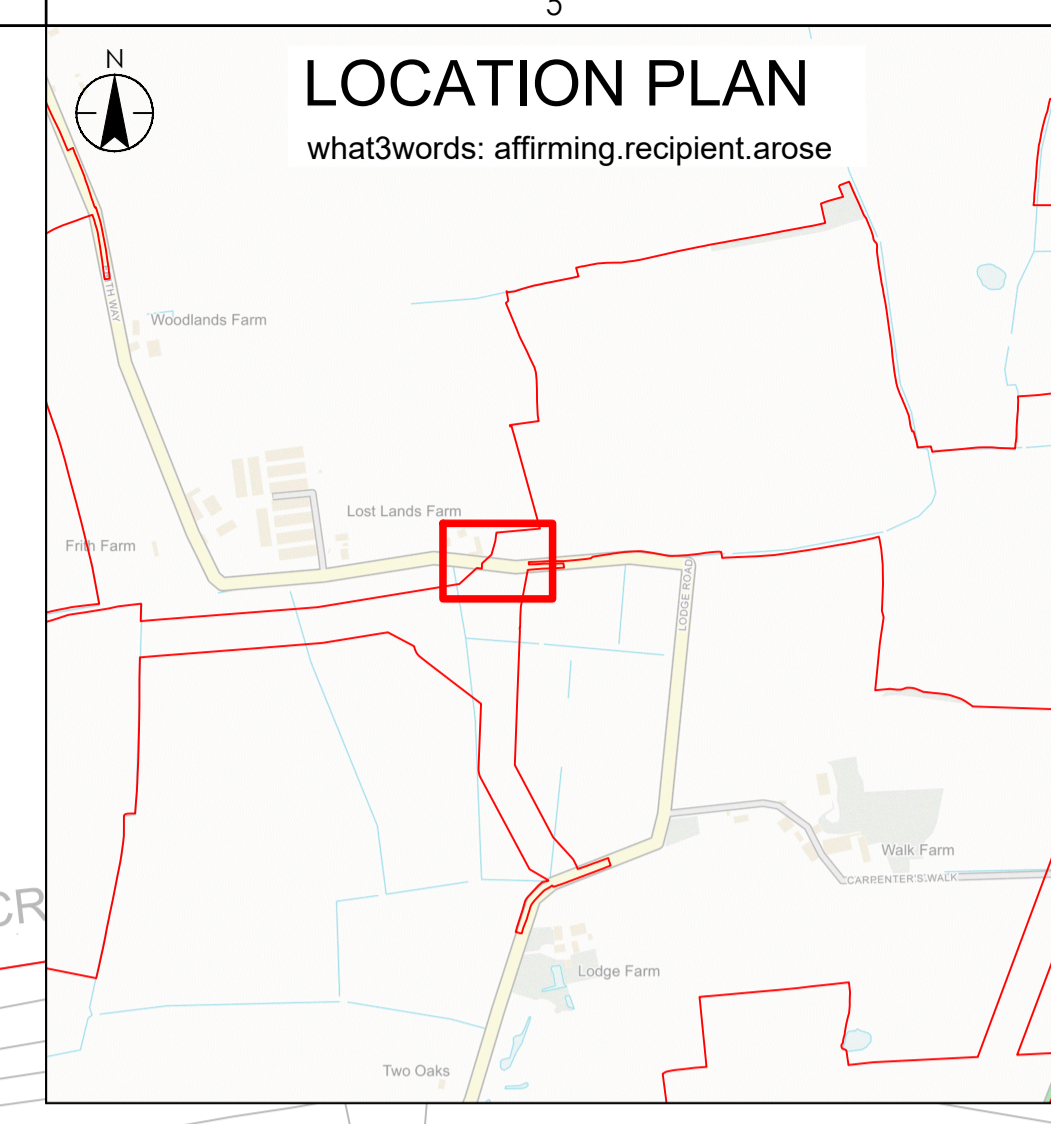
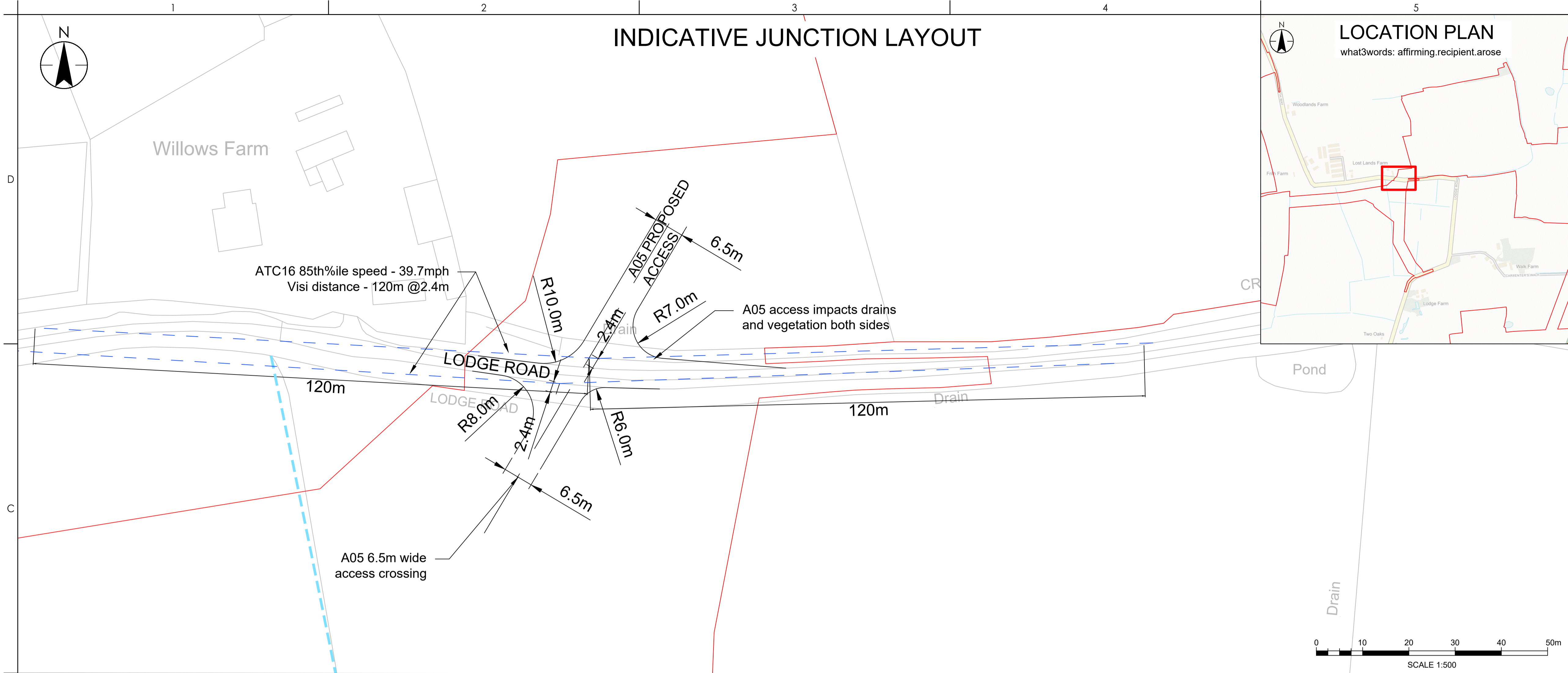


16.5m HGV VEHICLE SWEEP PATH - VEHICLE OUT



P:\01633101\333101678\16.5m HGV Swept Path - Vehicle Out.dwg (16.5m HGV Swept Path - Vehicle Out) - 11/11/2024 10:52:17 AM by: [Name Redacted]
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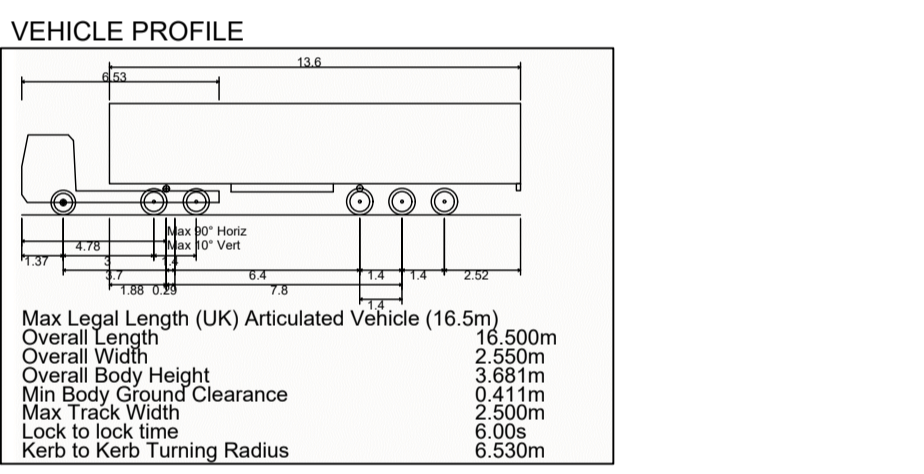
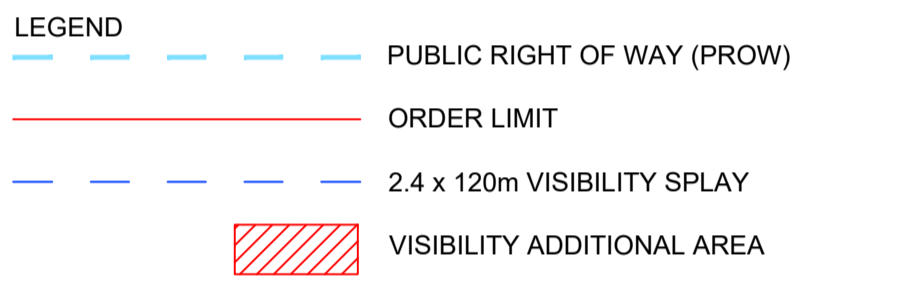
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 - VEHICLES EXPECTED TO TRAVEL TO / FROM ACCESS A05 VIA A140 IPSWICH ROAD / B1134 STATION ROAD / LODGE ROAD.
 - PHASE USE OF ACCESS A05 - CONSTRUCTION & OPERATION. A05 HIGHWAY ACCESS AND CROSSING TO PROVIDE ACCESS TO 2B & CRG2.



PO1 FIRST ISSUE	JO	TH	2024.02.11
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ISLAND GREEN POWER

EAST PYE SOLAR FARM

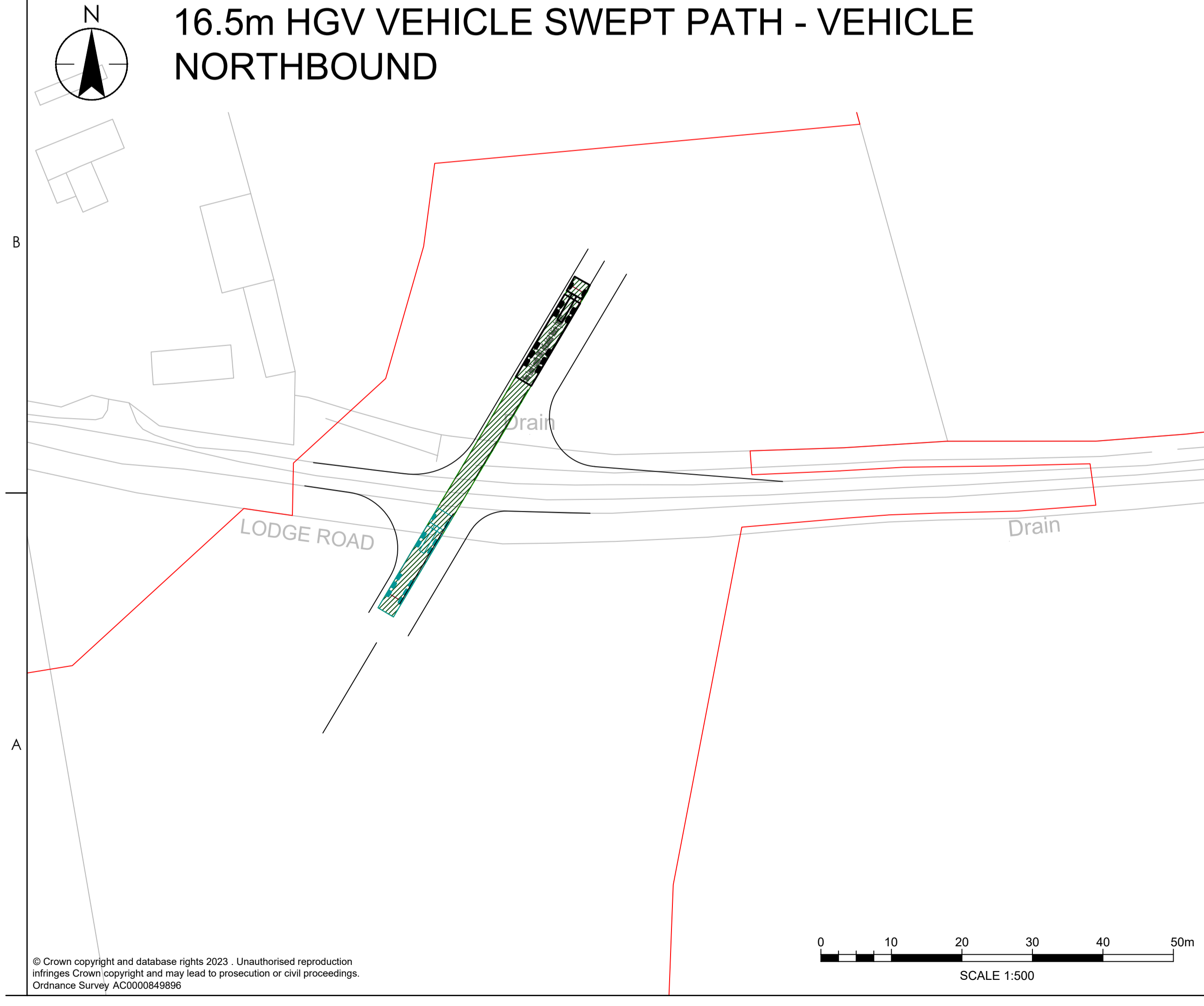
DEVELOPMENT CONSENT ORDER

Title
INDICATIVE SITE ACCESS A05
LODGE ROAD
16.5m HGV
SHEET 61

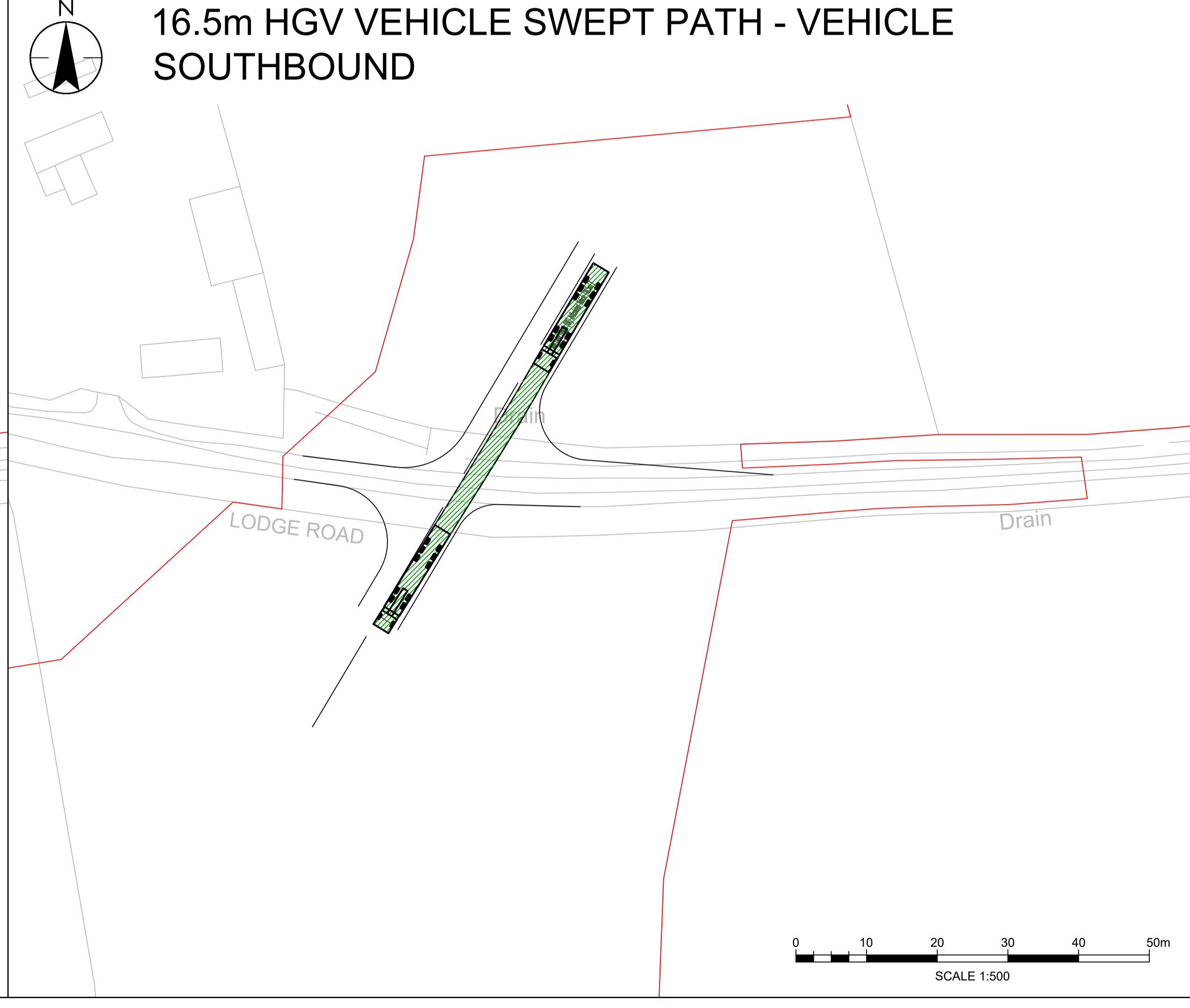
Project No. 333101678 A1 Scale AS SHOWN

Revision P01 Drawing No. 333101678-STN-HGN-XX-DR-TR-5500

16.5m HGV VEHICLE SWEEP PATH - VEHICLE NORTHBOUND

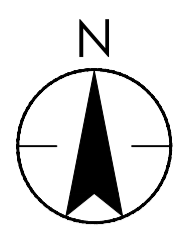


16.5m HGV VEHICLE SWEEP PATH - VEHICLE SOUTHBOUND

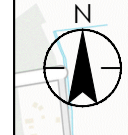


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LOCATION PLAN

what3words: blast.camcorder.creatures



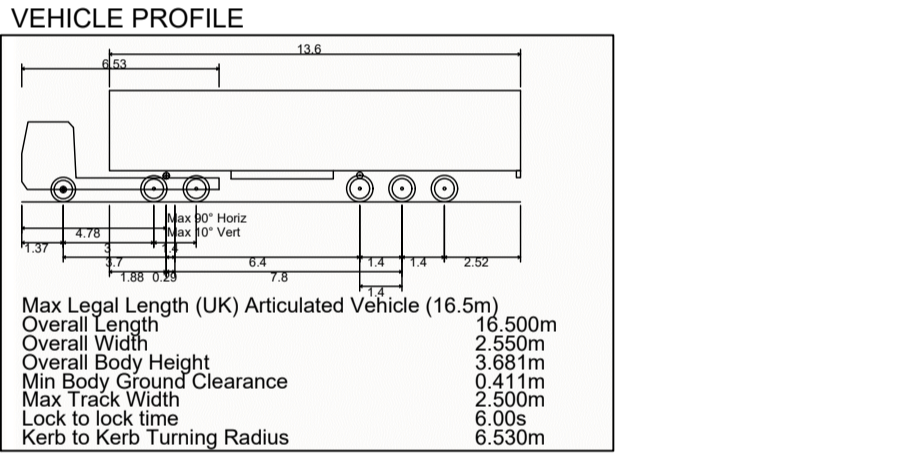
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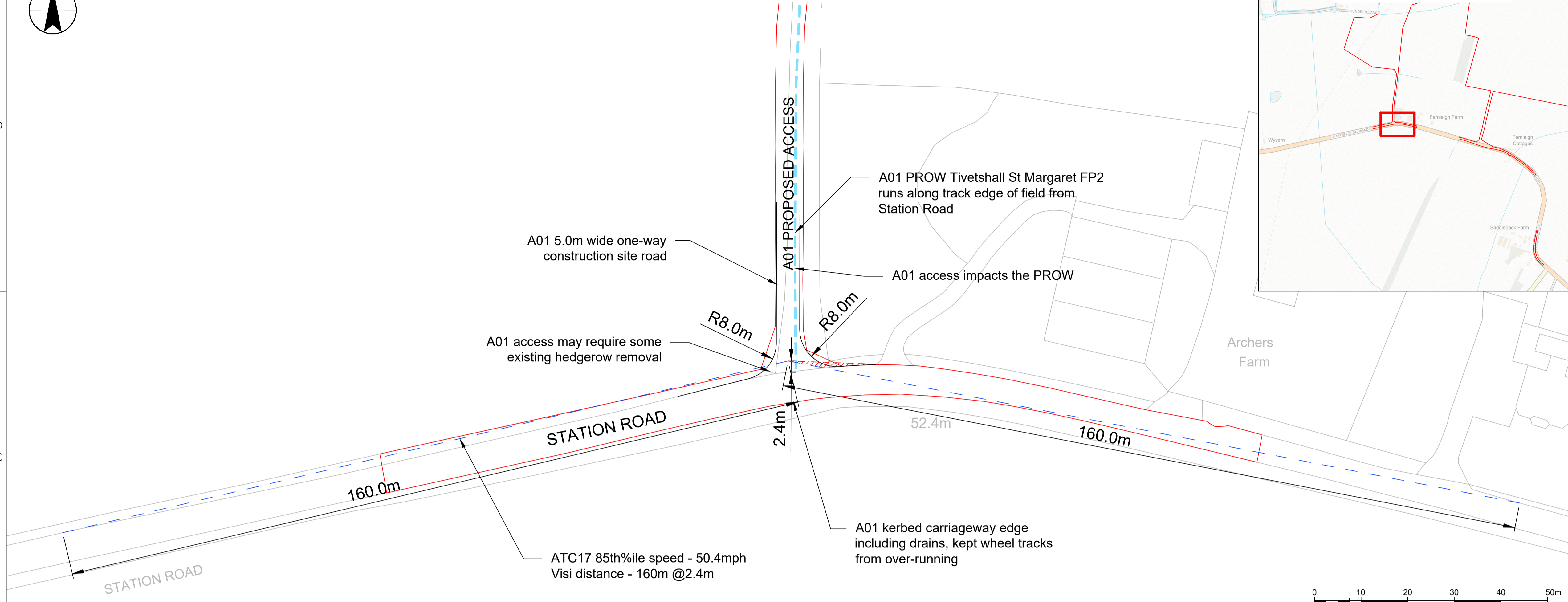
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 - STATION ROAD IS SUBJECT TO THE NATIONAL SPEED LIMIT
 - VEHICLE TRACKING AND VISIBILITY IS BASED ON MAXIMUM LEGAL VEHICLE SIZE, THROUGH DETAILED DESIGN AND VEHICLE SELECTION THE REQUIREMENTS FOR HEDGE REMOVAL AND EXTENT OF HARD SURFACING WHERE PRACTICAL MAY BE REDUCED.
 - VEHICLES EXPECTED TO TRAVEL TO / FROM ACCESS A01 VIA A140 IPSWICH ROAD / B1134 STATION ROAD.
 - PHASE USE OF ACCESS A01 - CONSTRUCTION & OPERATION. A01 HIGHWAY ACCESS TO PROVIDE ACCESS TO OHL WORKS.

- LEGEND**
- PUBLIC RIGHT OF WAY (PROW)
 - ORDER LIMIT
 - 2.4 x 160m VISIBILITY SPLAY
 - VISIBILITY ADDITIONAL AREA



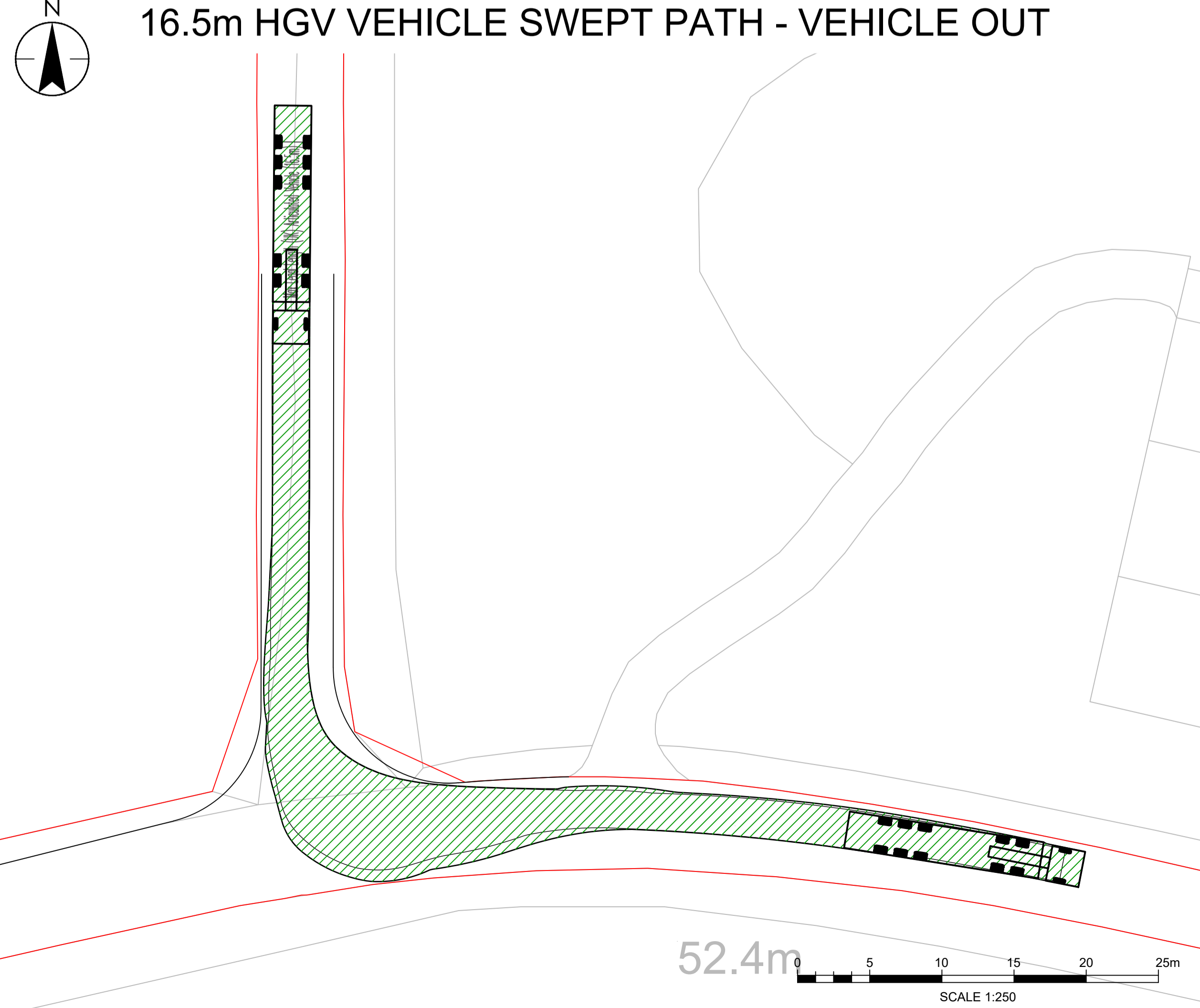
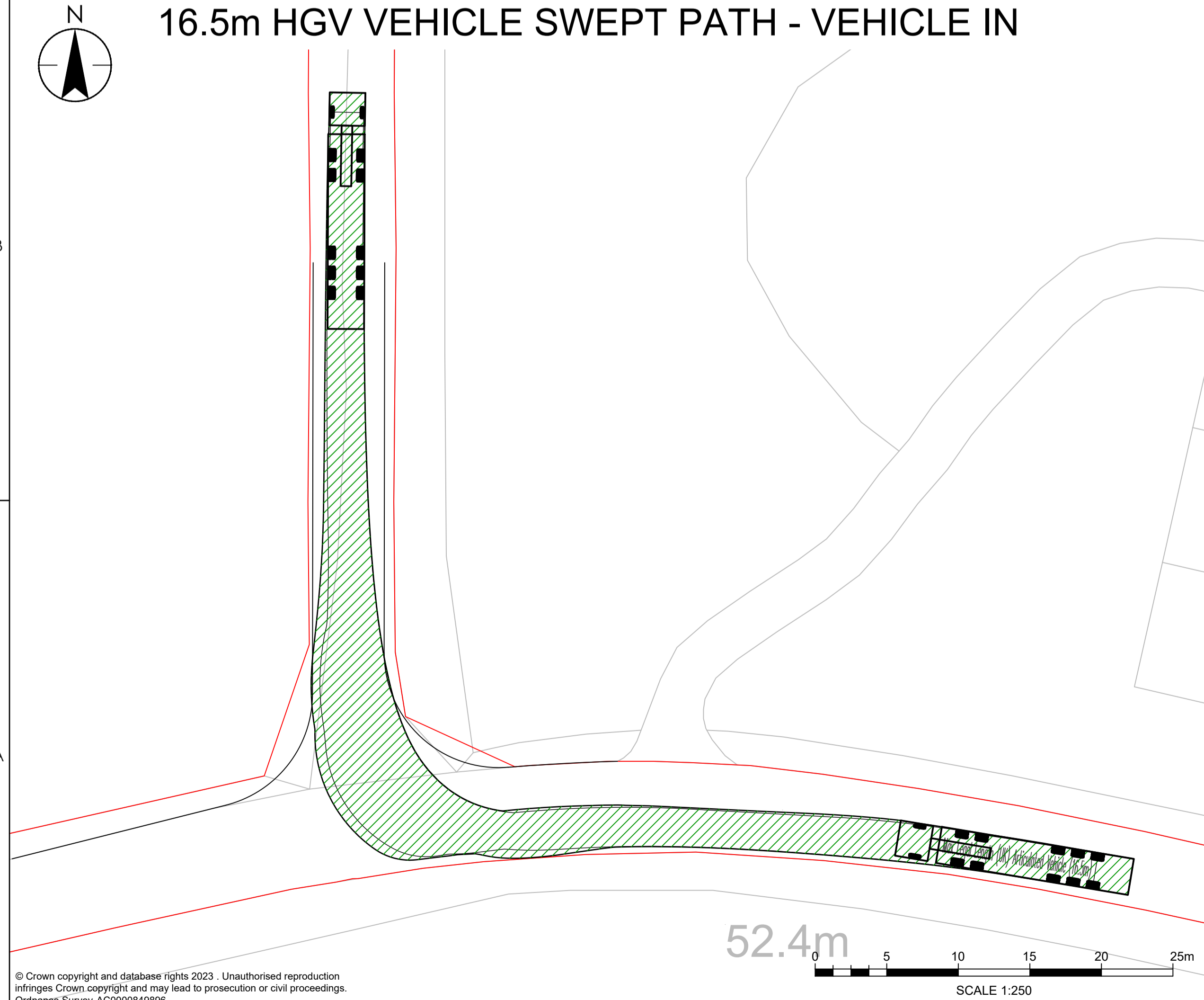
D

C



B

A



P01	FIRST ISSUE	JO	TH	2024.02.11
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		Dwn.	Dsgn.	Chkd.
				2025.12.09
				YYYY.MM.DD

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Client/Project
ISLAND GREEN POWER

EAST PYE SOLAR FARM

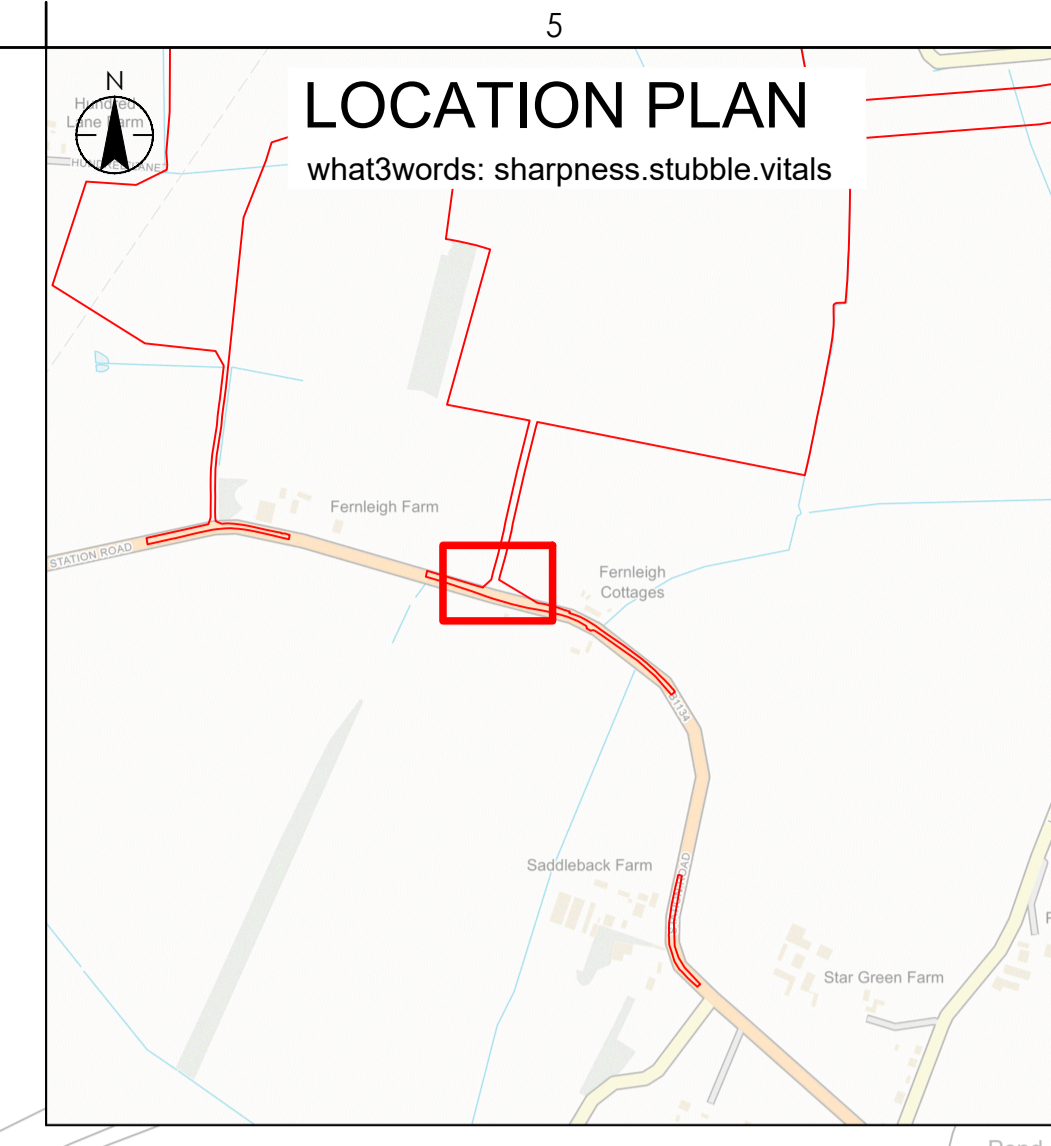
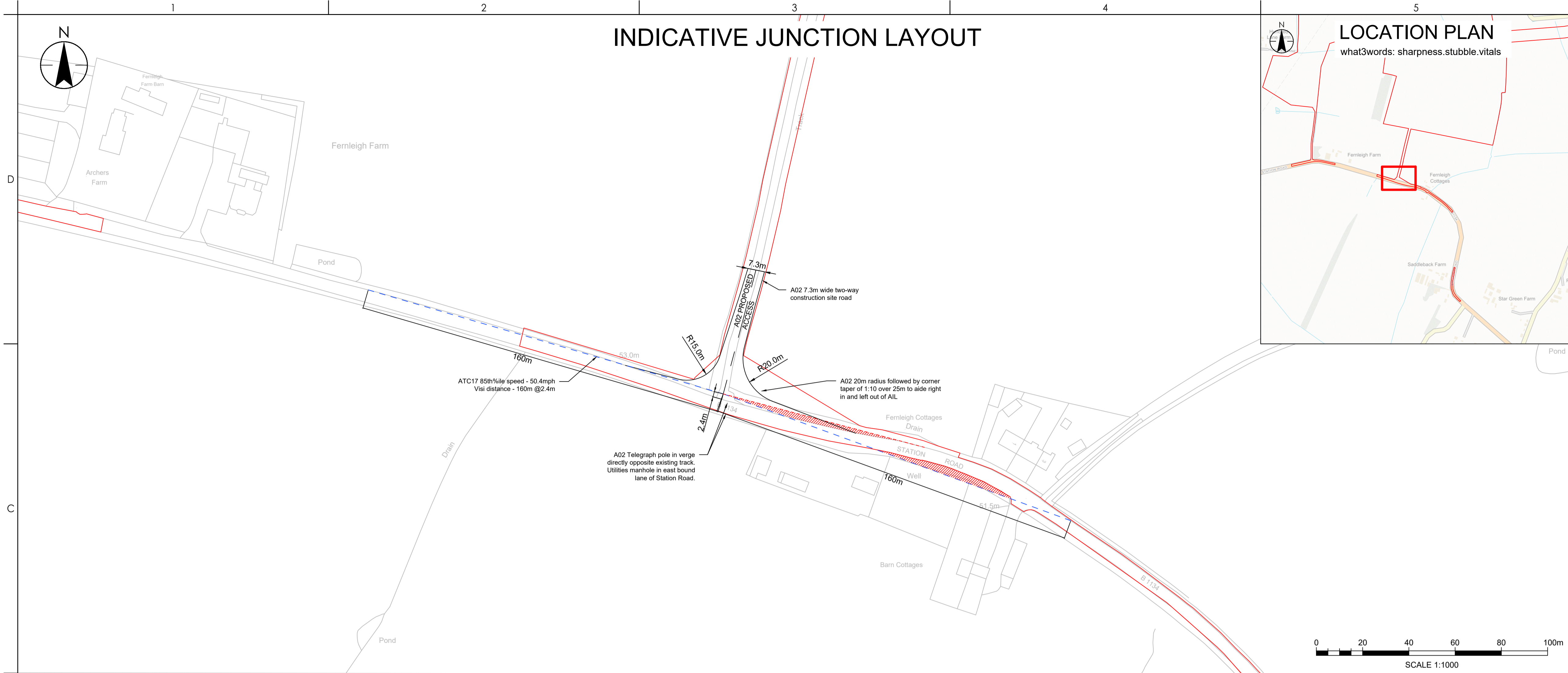
DEVELOPMENT CONSENT ORDER

Title
INDICATIVE SITE ACCESS A01
B1134 STATION ROAD
16.5m HGV
SHEET 63

Project No. 333101678	A1 Scale AS SHOWN
Revision P01	Drawing No. 333101678-STN-HGN-XX-DR-TR-5500

P:\03032024\20240303_211652 PM by: Dorian, Jemma
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INDICATIVE JUNCTION LAYOUT

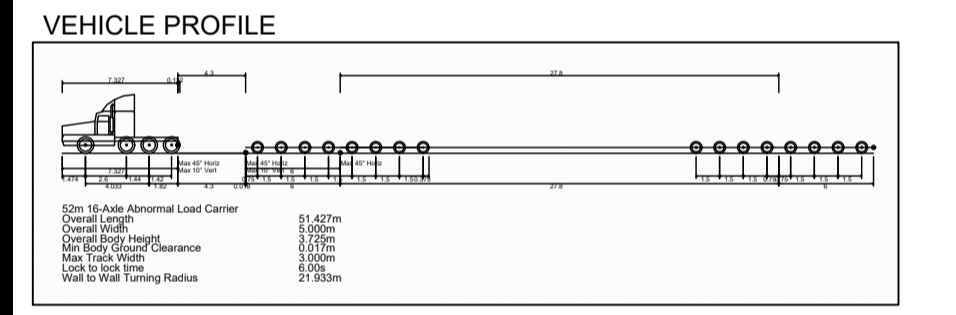
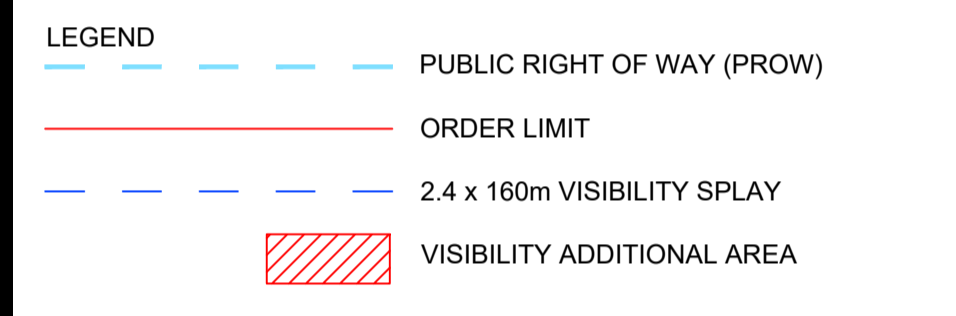


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- STATION ROAD IS SUBJECT TO THE NATIONAL SPEED LIMIT
- VEHICLE TRACKING AND VISIBILITY IS BASED ON MAXIMUM LEGAL VEHICLE SIZE, THROUGH DETAILED DESIGN AND VEHICLE SELECTION THE REQUIREMENTS FOR HEDGE REMOVAL AND EXTENT OF HARD SURFACING WHERE PRACTICAL MAY BE REDUCED.
- VEHICLES EXPECTED TO TRAVEL TO / FROM ACCESS A02 VIA A140 IPSWICH ROAD / B1134 STATION ROAD.
- ABNORMAL LOAD IS EXPECTED TO HAVE FRONT AND REAR DRIVE CABS SO IF REQUIRED IT CAN DRIVE FORWARD PAST THE ACCESS AND THEN USE THE REAR DRIVE CAB TO TURN LEFT INTO THE ACCESS.
- PHASE USE OF ACCESS A02 - CONSTRUCTION & OPERATION. A02 HIGHWAY ACCESS TO PROVIDE ACCESS TO BESS, OHL WORKS, 1A/B, CRC1 & CRC2.



PO1	FIRST ISSUE	JO	TH	2024.02.11
Issued/Revision		By	Appd	YYYY.MM.DD
		KWM	KWM	JO
		Dwn.	Dsgn.	Chkd.
				2025.12.09
				YYYY.MM.DD

Issue Status
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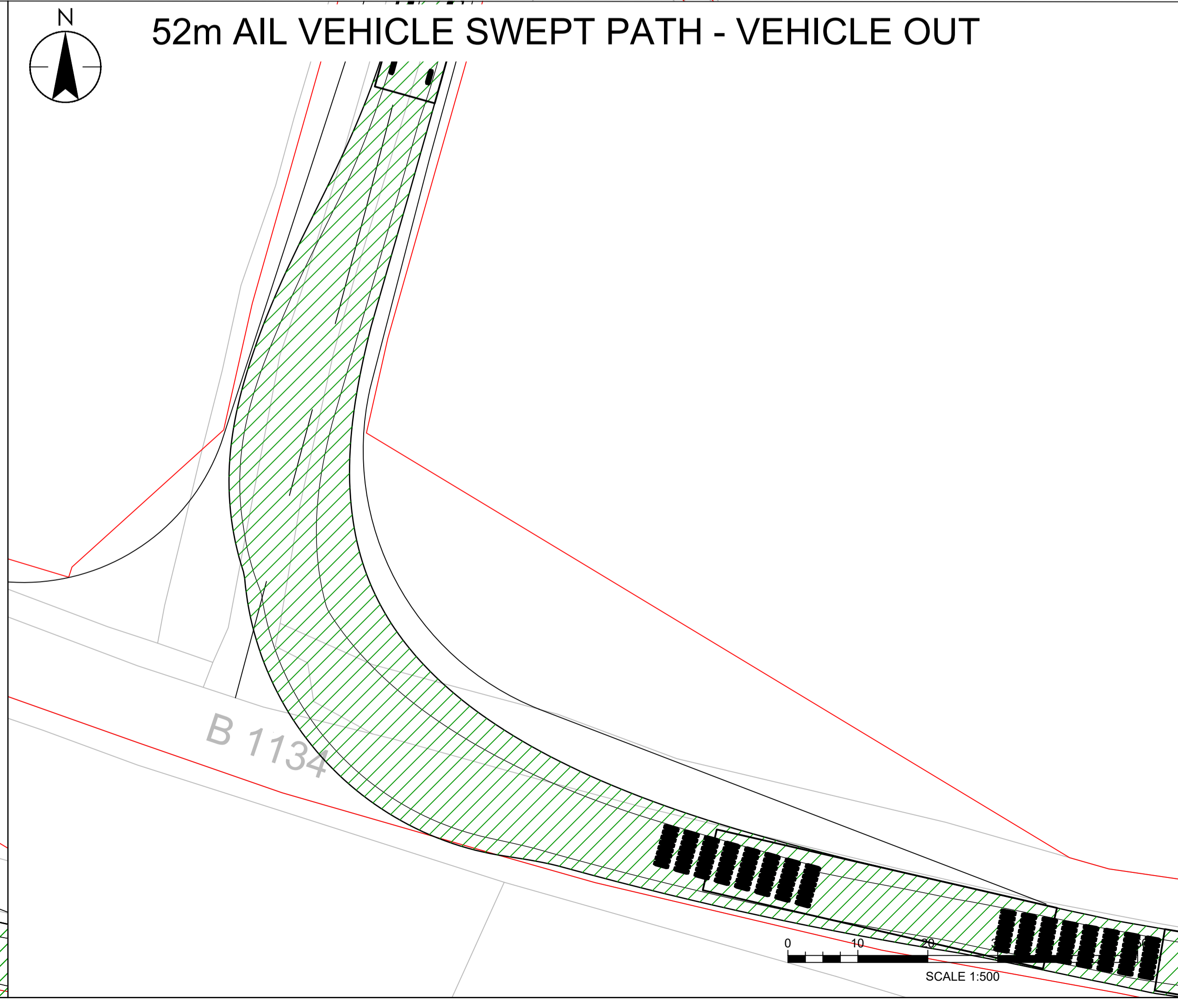
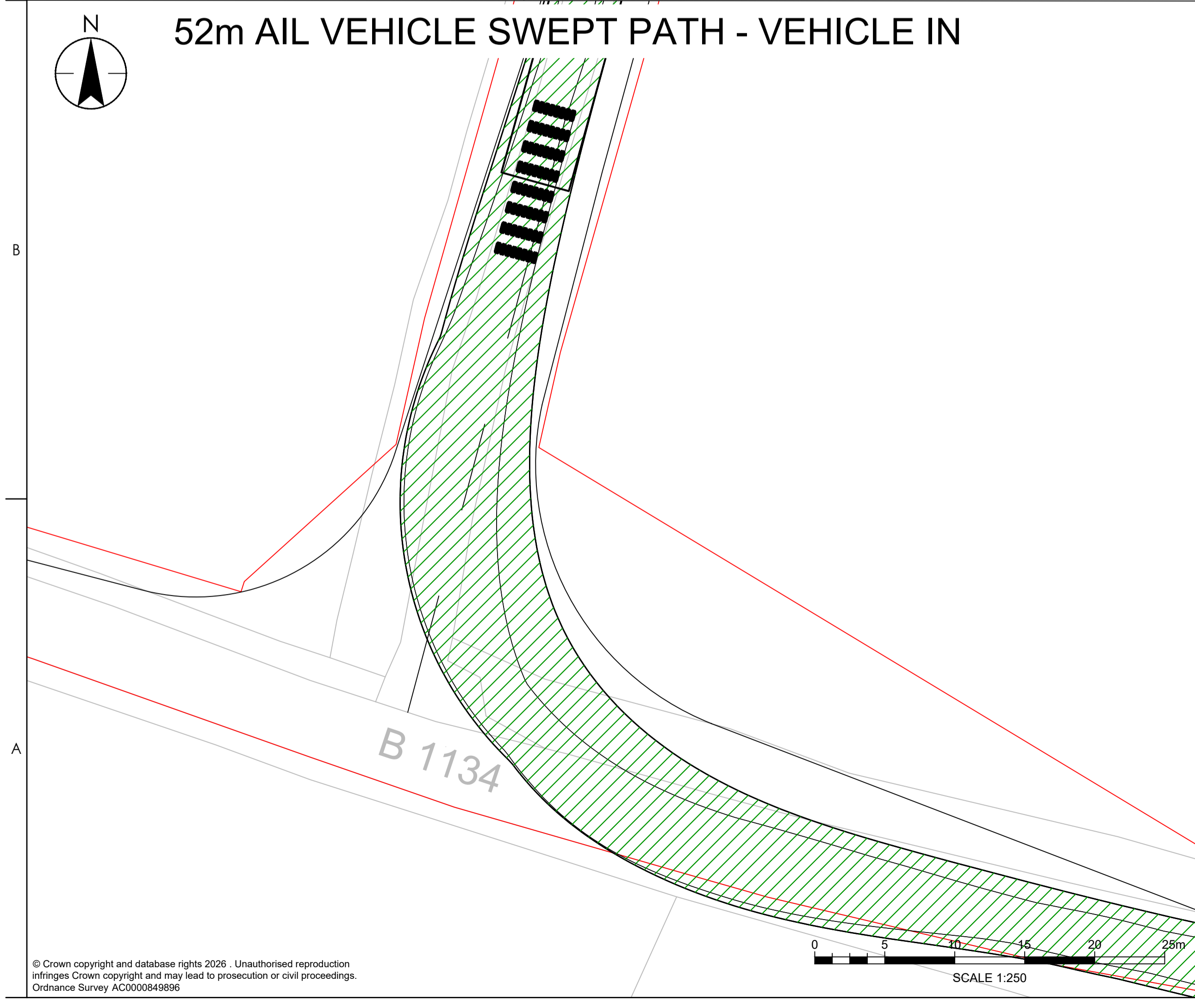
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Client/Project
ISLAND GREEN POWER
EAST PYE SOLAR FARM DEVELOPMENT
CONSENT ORDER

Title
**INDICATIVE SITE ACCESS A02
B1134 STATION ROAD
52m AIL
SHEET 64**

Project No. 333101678 A1 Scale AS SHOWN
Revision P01 Drawing No. 333101678-STN-HGN-XX-DR-TR-5500



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Appendix D Abnormal Indivisible Load Report



Abnormal Indivisible Load Access Summary for East Pye Solar

Prepared for Island Green Power





IGP | 24-1275 East Pye Solar | AIL Access Summary Report | 23.02.26 | V1.

NAME		SIGNATURE	DATE
Prepared by:			23.02.26
Checked by:			23.02.26
Approved by:			23.02.26

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DOCUMENT REVISIONS

Issue	Date	Details
0	20.02.26	AIL Access Summary Report
1	23.02.26	Revised with client comments
2		



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1. Introduction

- 1.1. This document includes high level summary reports in respect to Abnormal Indivisible Loads (AIL) access to the proposed substations that are expected to be required for the East Pye Solar Project.
- 1.2. This will require deliveries of transformer units to the following sites.
 - East Pye Site 4A & 4B – 95te Transformer
 - East Pye Site 5A & 5B – 95te and 183te Transformers
 - East Pye Site 7D, 7E & 7F – 95te Transformer
 - East Pye Site 10B & 10C – 95te Transformer
 - East Pye Site BESS – 155te Transformer
- 1.3. The report considers access to the proposed onshore substations in terms of AIL transportation of the main transformers and to the wider cable corridor for cable drums. Wider traffic and transport for Construction and Use vehicles is not within the scope of this document which details the issues on access for heavy transformers and cable drums only.
- 1.4. The report highlights preferred AIL access routes for transformer AILs via the public road network as far as is possible to date and highlights where additional remedial works will be necessary.
- 1.5. The report includes reference to the responses of highway and structural authorities where applicable including Norfolk County Council, Suffolk County Council and National Highways East Region.
- 1.6. The high level summary is intended to inform planning documentation. A more detailed report discussing the various issues raised and routes rejected will be issued to Island Green Power (IGP) under separate cover. This will include more information on legislative requirements, route negotiability and the structural status of the routes.

2. Transport Drawings

- 2.1. The anticipated transport dimensions of the transformers for each of the substation location are shown below in Table 1 as is the indicative AIL transport arrangement that has been used for initial consultation with highway authorities that are reproduced on the following pages. These are based on standard AIL transport configuration that are generally used for transformers of the dimensions stated and are shown in Attachment 1.
- 2.2. Drawings of indicative cable drum trailer arrangements are also included within Attachment 1 of this report.



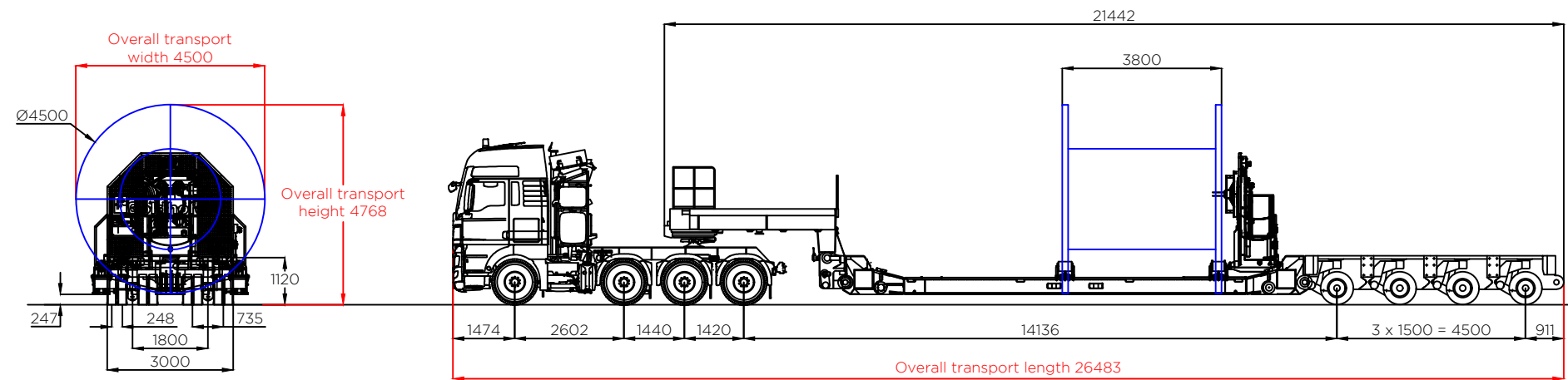
Table 1 – Transformer Transport Arrangements

Site	Length (mm)	Width (mm)	Height (mm)	Weight (kgs)	Transport Arrangements
4A & 4B	7900	4860	4500	95,000	5 bed 5 trailer as shown in drawing number 24-1275.TC03
5A & 5B	7900	4860	4500	95,000	5 bed 5 trailer as shown in drawing number 24-1275.TC03
	9400	3850	4900	183,000	16 axle girder frame as shown in drawing number 24-1275.TC05 20 axle girder frame as shown in drawing number 24-1275.TC06
7D, 7E & 7F	7900	4860	4500	95,000	5 bed 5 trailer as shown in drawing number 24-1275.TC03
10B & 10C	7900	4860	4500	95,000	5 bed 5 trailer as shown in drawing number 24-1275.TC03
East Pye BESS	7240	5000	4778	155,000	16 axle girder frame trailer as shown in drawing number 24-1275.TC04



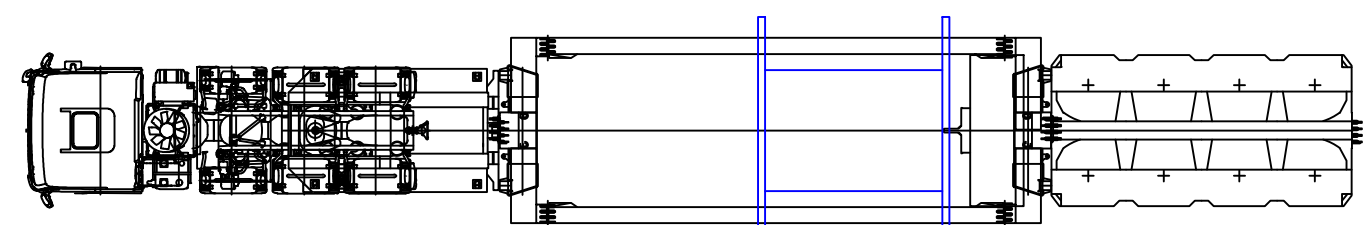
Attachment 1

Transport Arrangements

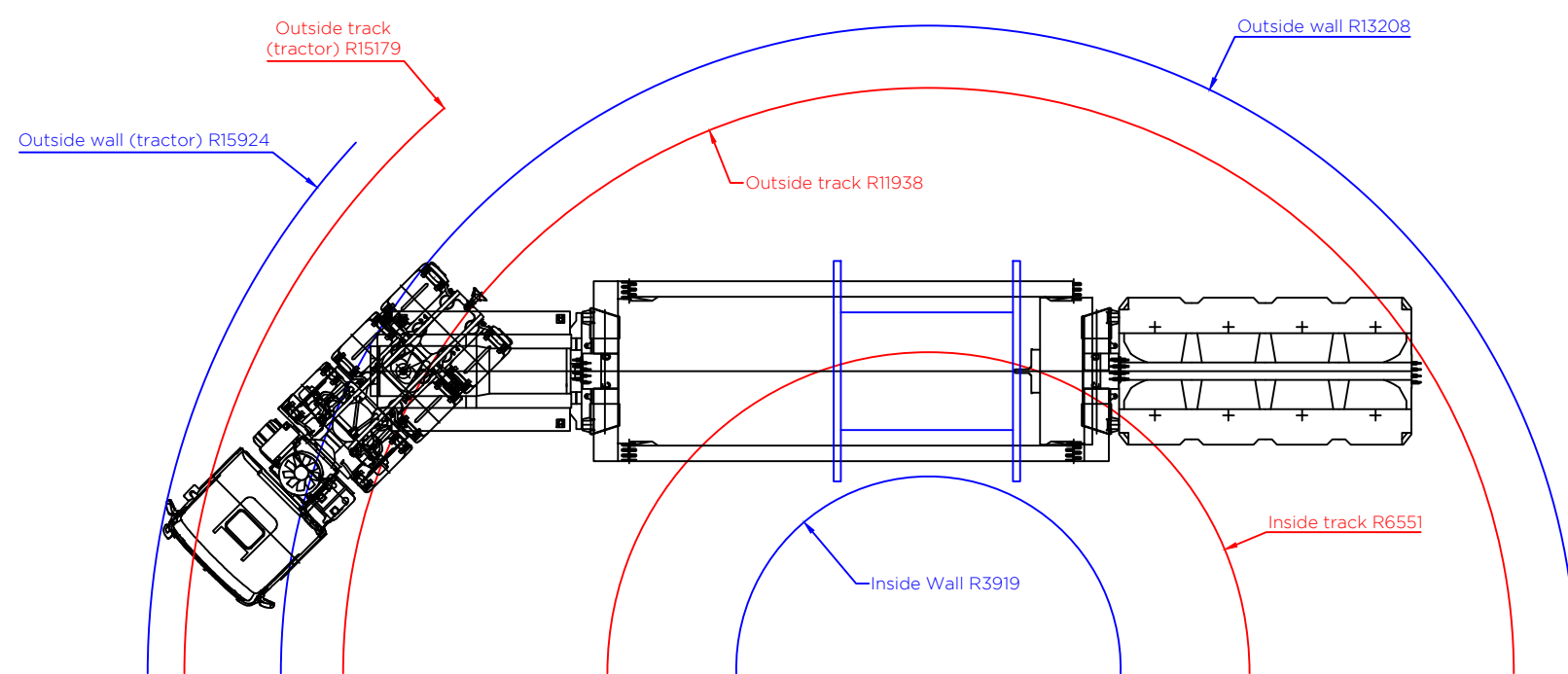


Profile view

Elevation view - 4 axle modular spooling trailer - concept model only
Indicative 30 te cable drum Scale 1:150



Plan view - 4 axle modular spooling trailer - concept model only
Indicative 30 te cable drum
Scale 1:150



Minimum turning radii information
4 axle modular spooling trailer - concept model only
Indicative 30 te cable drum
Scale 1:150

Load table	
4 axle modular spooling trailer	
Self weight of cable drum	30.0 te
Self weight of trailer	33.3 te
Self weight of tractor	15.0 te
Total combined weight	78.3 te
Load per axle line (trailer)	10.55 te
Load per axle	5.28 te
Load per wheel (4 per axle)	1.32 te
Overall ground bearing pressure	3.13 te/m ²
Tractor (15 te)	
Front axle	7.0 te
Second steer	8.0 te
Rear axle	10.55 te
Rear axle	10.55 te

Notes:
 [1] The figures shown above are representative of the transport configuration portrayed. However as tractor and trailer arrangements vary then the loads and dimensions indicated should be treated as probable values.
 [2] Actual dimensions, including axle spacing and mean running height, may vary slightly depending on manufacturer of trailer deployed.
 [3] All linear measures in millimetres unless stated otherwise.

Rev.	Date	Amendments
1		
0	14.04.25	Issued for comment

Revisions

Prepared by:



Shaftesbury House, 2 High Street,
Eccleshall, Stafford, ST21 6BZ
Tel: (01785) 850411

Independent Transportation Engineers

Client:

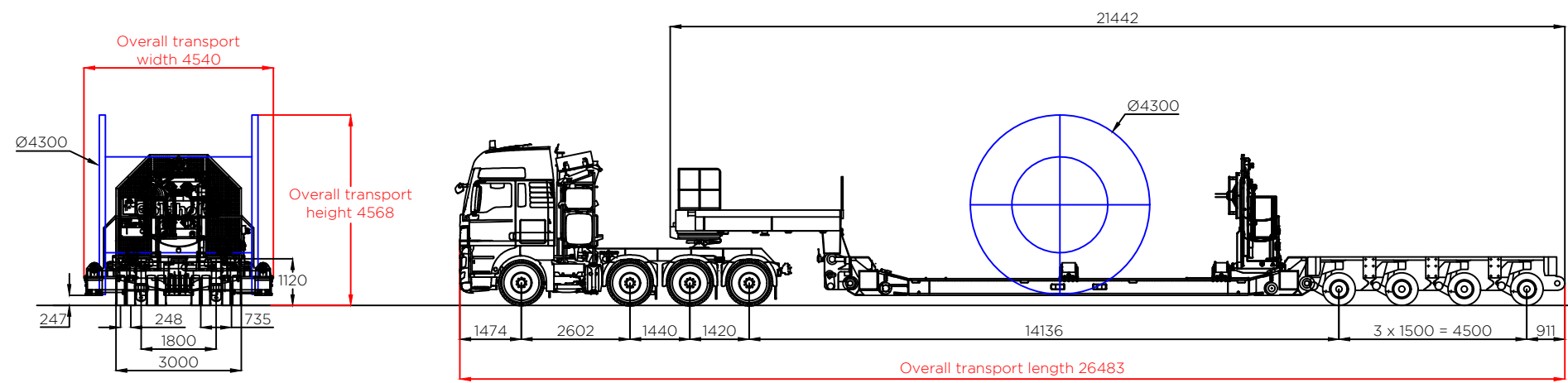


Project: **East Pye BESS**

Title:
Indicative transport configuration
 Indicative 30.0 te cable drum carried on
 4 axle modular spooling trailer
 showing minimum turning radii

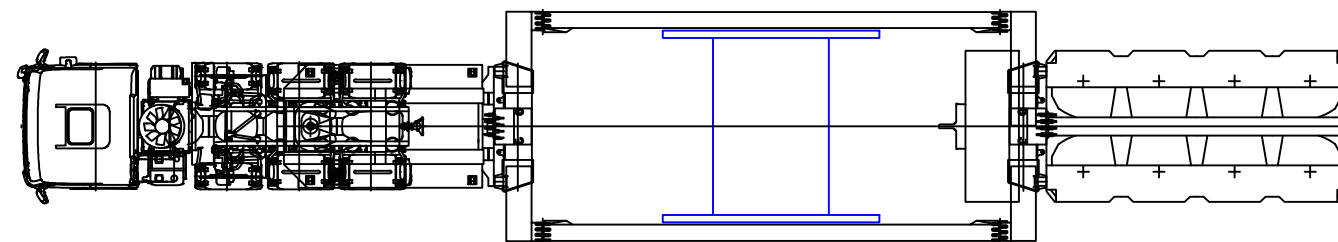
Drawing status: **Final report**

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As shown	MTO	ARP
Dwg. no:	Sheet:	Rev:
24-1275.TC01	1 of 1	0

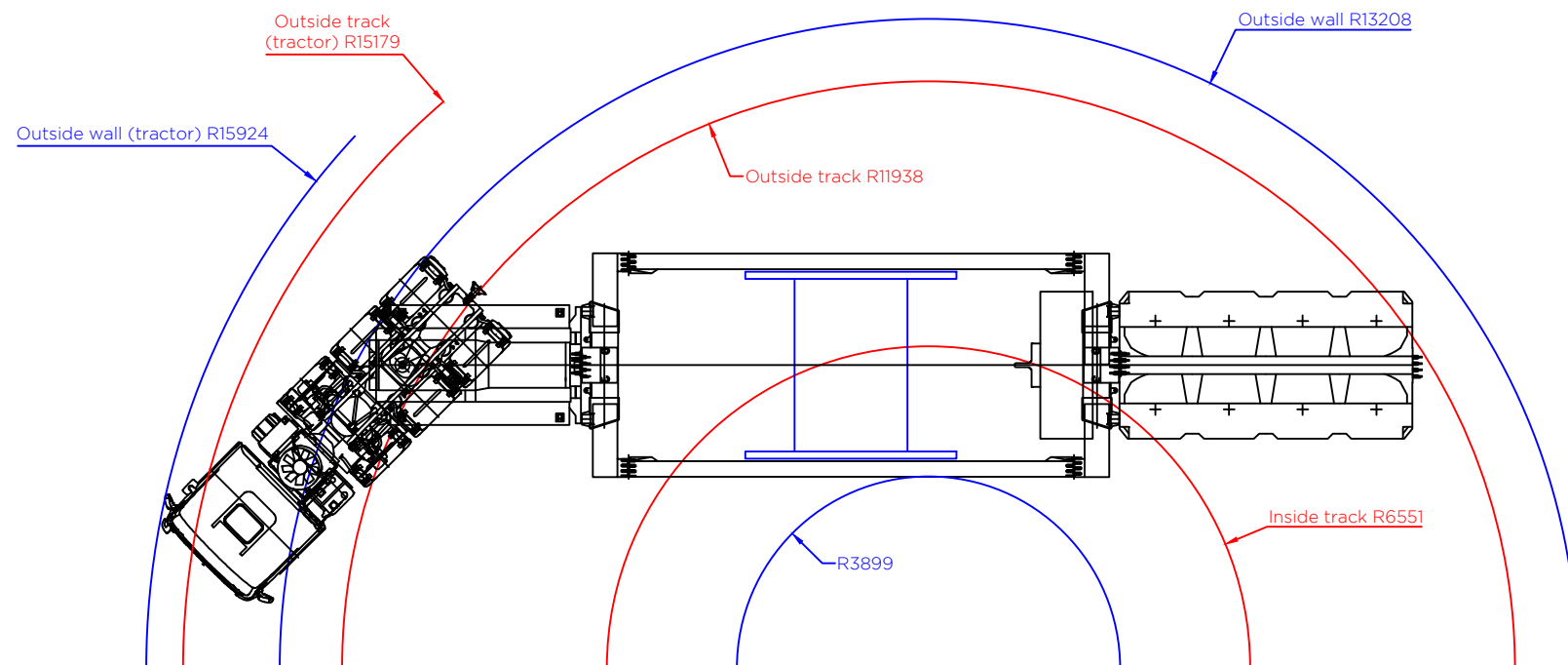


Profile view

Elevation view - 4 axle modular reeling trailer - concept model only
Indicative 30 te cable drum
Scale 1:150



Plan view - 4 axle modular reeling trailer - concept model only
Indicative 30 te cable drum
Scale 1:150



Minimum turning radii information
4 axle modular reeling trailer - concept model only
Indicative 30 te cable drum
Scale 1:150

Load table

4 axle modular reeling trailer

Self weight of cable drum	30.0 te
Self weight of trailer	33.3 te
Self weight of tractor	15.0 te
Total combined weight	78.3 te
Load per axle line (trailer)	10.55 te
Load per axle	5.28 te
Load per wheel (4 per axle)	1.32 te
Overall ground bearing pressure	3.13 te/m ²

Tractor (15 te)

Front axle	7.0 te
Second steer	8.0 te
Rear axle	10.55 te
Rear axle	10.55 te

Notes:

[1] The figures shown above are representative of the transport configuration portrayed. However as tractor and trailer arrangements vary then the loads and dimensions indicated should be treated as probable values.

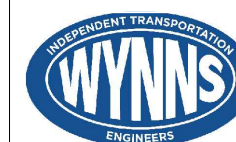
[2] Actual dimensions, including axle spacing and mean running height, may vary slightly depending on manufacturer of trailer deployed.

[3] All linear measures in millimetres unless stated otherwise.

1		
0	14.04.25	Issued for comment
Rev.	Date	Amendments

Revisions

Prepared by:



Shaftesbury House, 2 High Street,
Eccleshall, Stafford, ST21 6BZ
Tel: (01785) 850411

Independent Transportation Engineers

Client:



Project:

East Pye BESS

Title:

Indicative transport configuration
Indicative 30.0 te cable drum carried on
4 axle modular reeling trailer
showing minimum turning radii

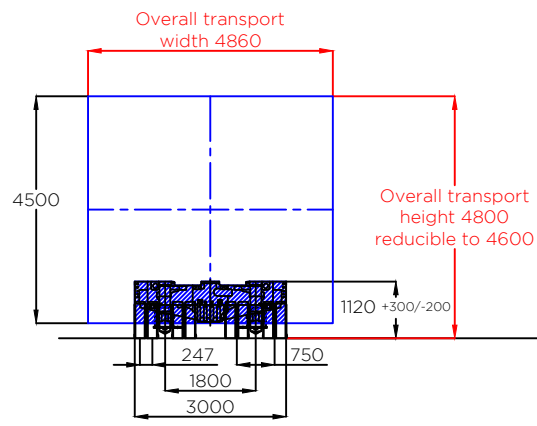
Drawing status:

Final report

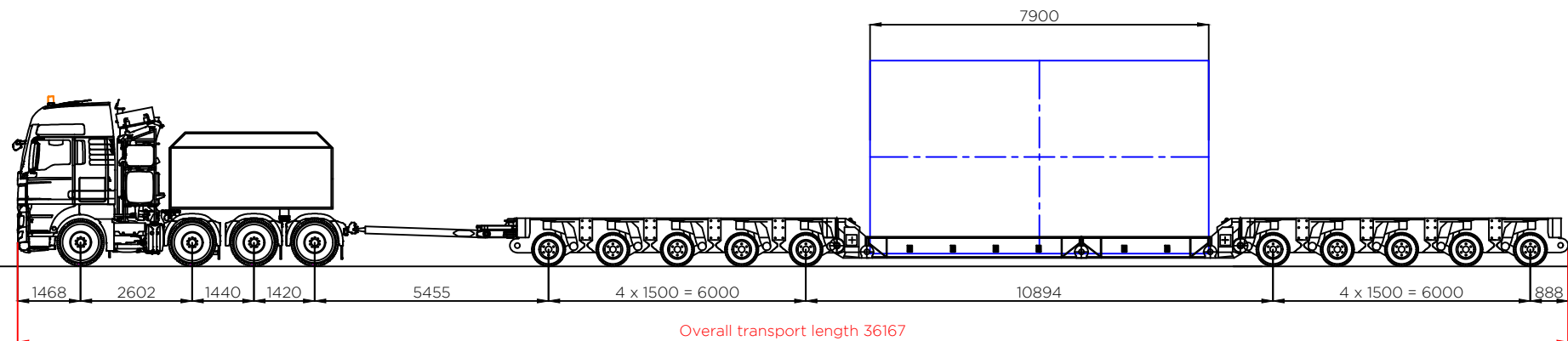
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Dwg. no: 24-1275.TC02	Sheet: 1 of 1	Rev: 0

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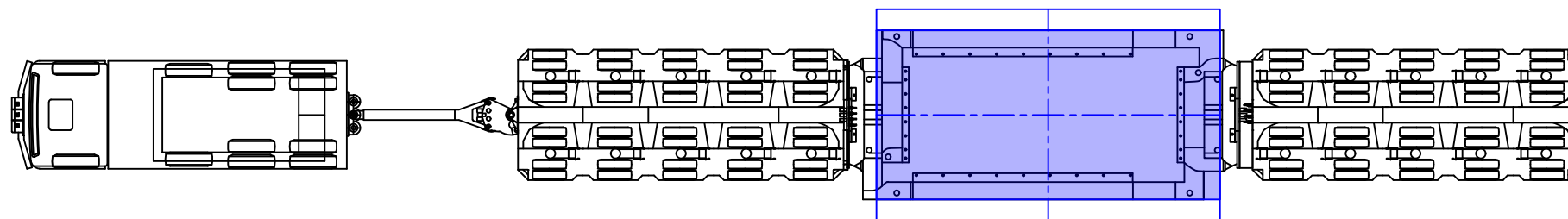
P:\Clients\Existing Clients\Island Green Power\24-1275 East Pye BESS
Norfolk\Transport Configurations\24-1275.TC02 East Pye BESS 30t Cable
Drum 4 Axle reeling trailer R0.dwg



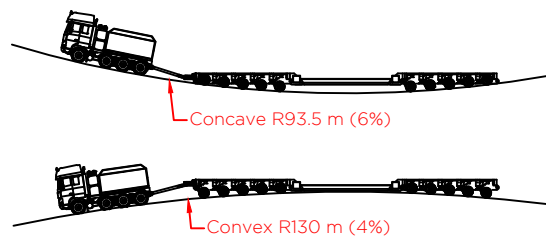
Profile view
Scale 1:150



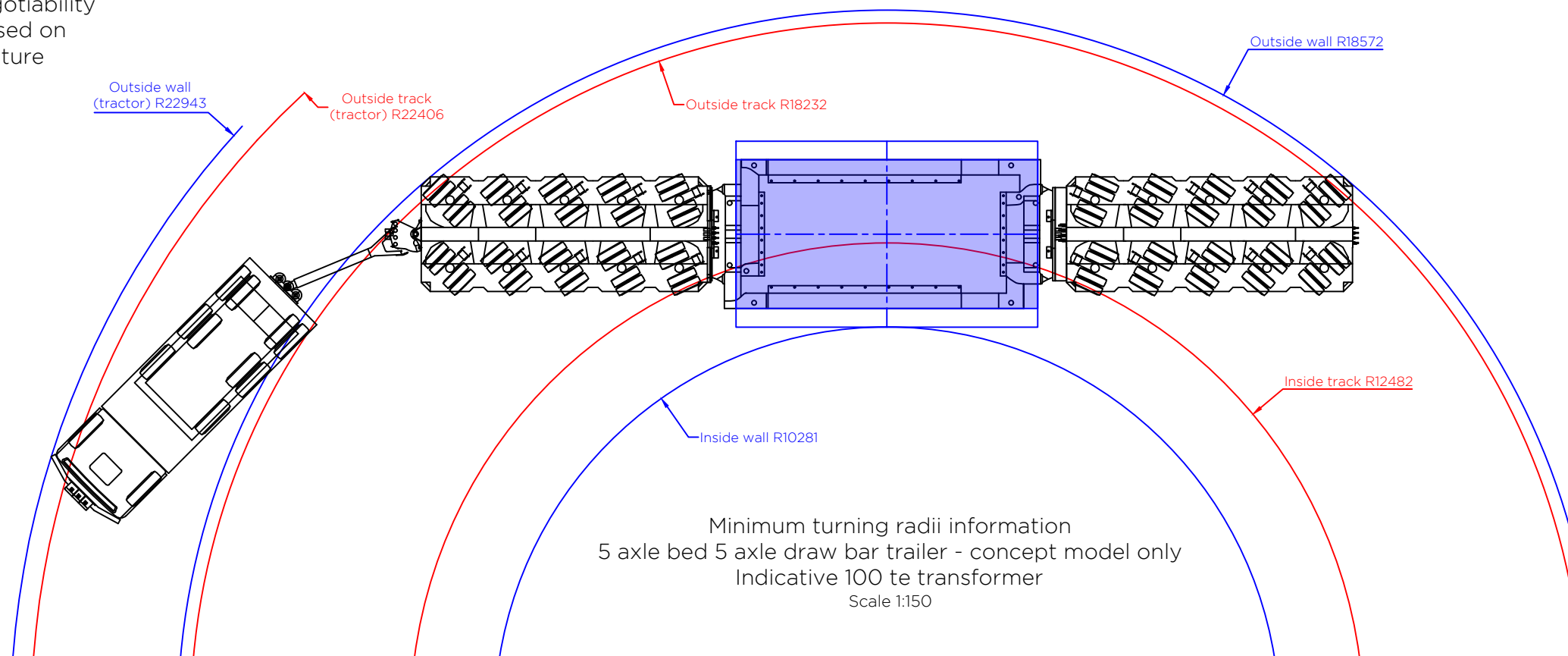
Elevation view - 5 axle bed 5 axle draw bar trailer - concept model only
Indicative 100 te transformer
Scale 1:150



Plan view - 5 axle bed 5 axle draw bar trailer - concept model only
Indicative 100 te transformer
Scale 1:150



Vertical curve negotiability
information based on
hauliers literature
Scale 1:600



Minimum turning radii information
5 axle bed 5 axle draw bar trailer - concept model only
Indicative 100 te transformer
Scale 1:150

Load table

5 axle bed 5 axle draw bar trailer

Self weight of transformer	100.0 te
Self weight of trailer	Say 46.0 te
Self weight of aux. steelwork (for L&S)	0.0 te
Total combined weight	146.0 te
Load per axle line	14.6 te
Load per axle	7.3 te
Load per wheel (4 per axle)	1.83 te
Overall ground bearing pressure	4.06 te/m ²

Tractor (40 te)

Front axle	7.0 te
Second steer	7.0 te
Rear axle	13.0 te
Rear axle	13.0 te

Notes:

[1] The figures shown above are representative of the transport configuration portrayed. However as tractor and trailer arrangements vary then the loads and dimensions indicated should be treated as probable values.

[2] Actual dimensions, including axle spacing and mean running height, may vary slightly depending on manufacturer of trailer deployed.

[3] All linear measures in millimetres unless stated otherwise.

[4] Indicative transformer shown only.

[5] Running height dependent upon tank base and transport lug arrangement.

1		
0	21.05.25	Issued for comment
Rev.	Date	Amendments

Revisions

Prepared by:



Shaftesbury House, 2 High Street,
Eccleshall, Stafford, ST21 6BZ
Tel: (01785) 850411

Independent Transportation Engineers

Client:



Project:

East Pye BESS

Title:

Indicative transport configuration
Indicative 100.0 te transformer carried on
5 axle bed 5 axle draw bar trailer
showing minimum turning radii

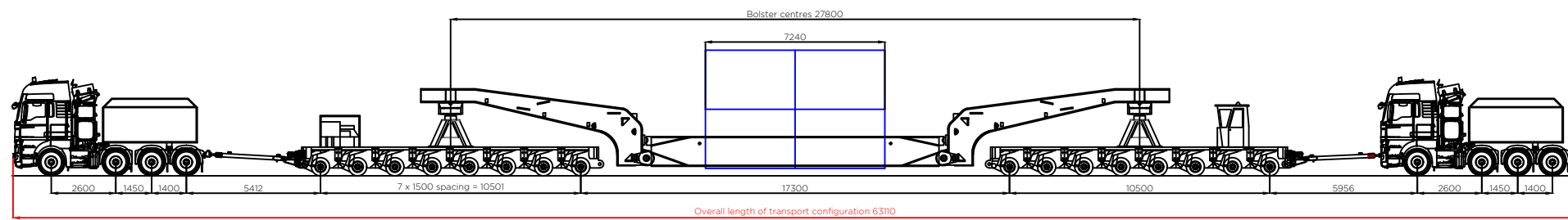
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Final report

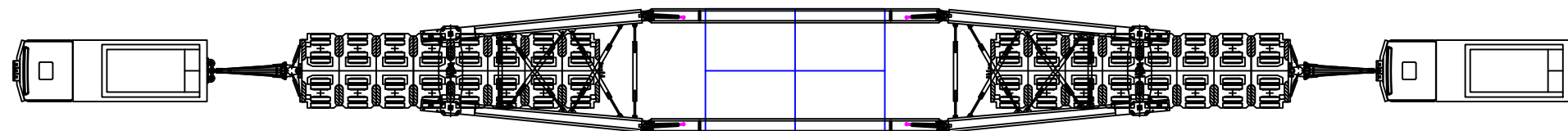
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Dwg. no: 24-1275.TC03	Sheet: 1 of 1	Rev: 0

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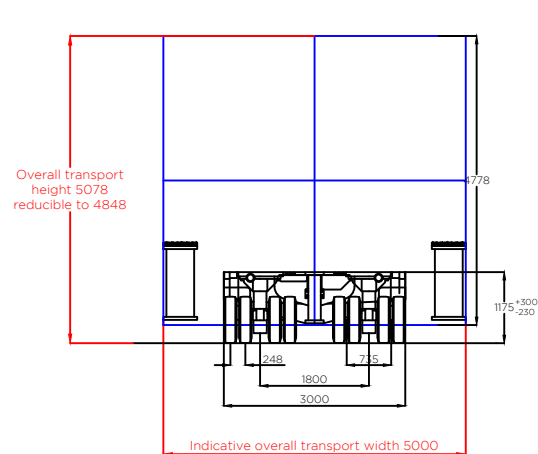
\\wynns-serv-dc\Data\Clients\Existing Clients\Island Green Power\24-1275 East Pye BESS Norfolk\Transport Configurations\22-1275.TC03 East Pye BESS 100 te transformer 5 bed 5 R0.dwg



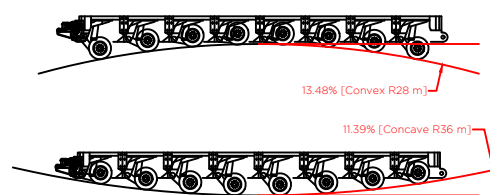
Elevation view - 16-axle girder frame trailer - concept model only
Indicative 157 te transformer
Scale 1:250



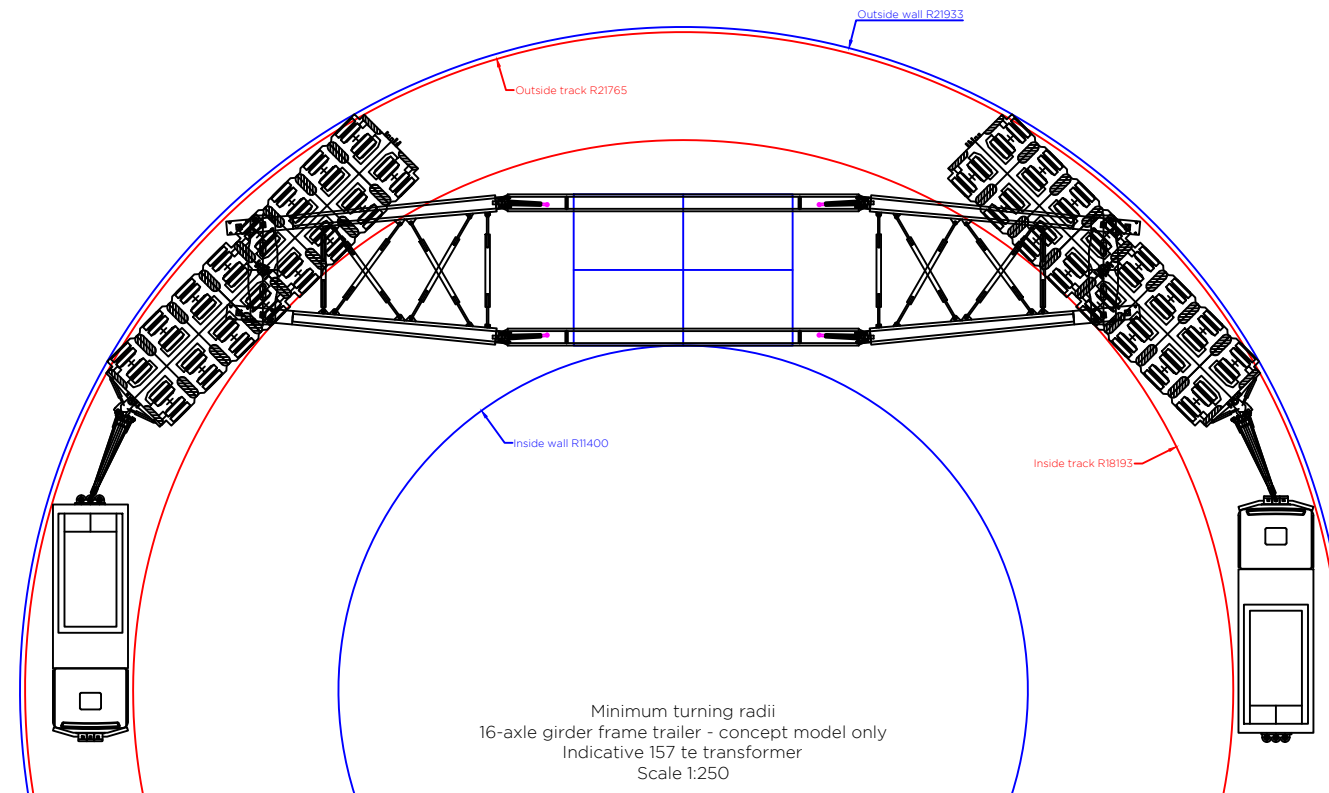
Plan view - 16-axle girder frame trailer - concept model only
Indicative 157 te transformer
Scale 1:250



Profile view
Scale 1:125



Vertical curve negotiability information
based on manufacturers literature
(Scale 1:250)



Load table

16-axle girder frame trailer

Self weight of transformer	157.0 te
Self weight of trailer	92.0 te
Self weight of aux. steelwork (for L&S)	0.0 te
Total combined weight	249.0 te
Load per trailer	124.5 te
Load per axle line	15.56 te
Load per axle	7.78 te
Load per wheel (4 per axle)	1.95 te
Overall ground bearing pressure	3.95 te/m ²

Tractor(s) (42 te)

Front axle	8.0 te
Second steer	10.0 te
Rear axle	12.0 te
Rear axle	12.0 te

Notes:

[1] The figures shown above are representative of the transport configuration portrayed. However, as tractor and trailer arrangements vary then the loads and dimensions indicated should be treated as probable values.

[2] Actual dimensions, including axle spacing and mean running height, may vary slightly depending on manufacturer of trailer deployed.

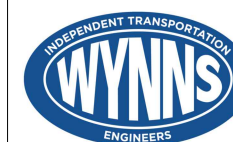
[3] All linear measures in millimetres unless stated otherwise.

[4] Indicative transformer shown only.

1		
0	11.08.25	Issued for comment
Rev.	Date	Amendments

Revisions

Prepared by:



Shaftesbury House, 2 High Street,
Eccleshall, Stafford, ST21 6BZ
Tel: (01785) 850411

Independent Transportation Engineers

Client:



Project:

East Pye BESS

Title:

Indicative transport configuration
Conceptual 157 te 400/33 kV transformer carried within 16-axle girder frame trailer with 3 m track width showing minimum turning radii

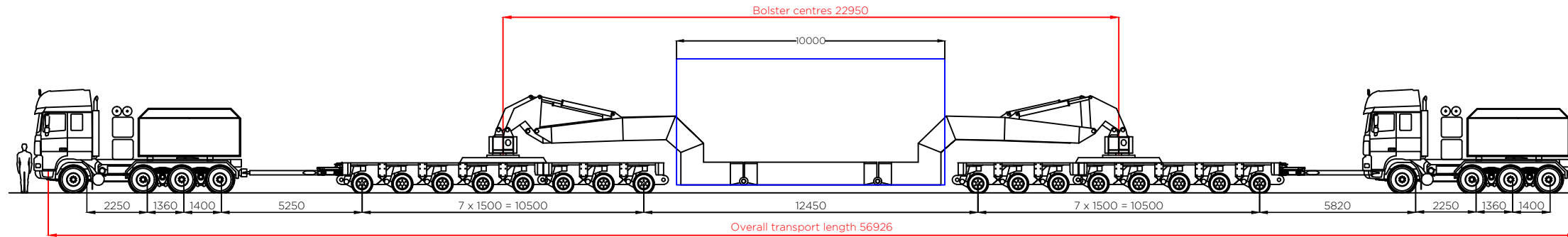
Drawing status:

Final report

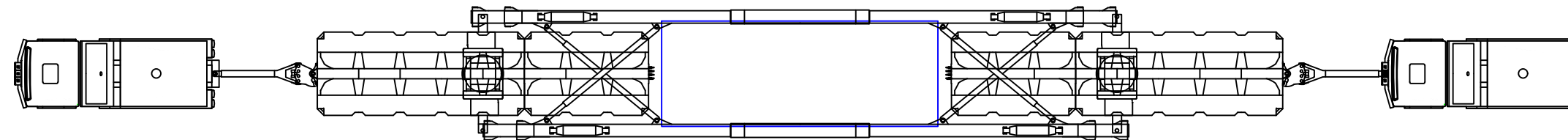
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Dwg. no: 24-1275.TC01	Sheet: 1 of 1	Rev: 0

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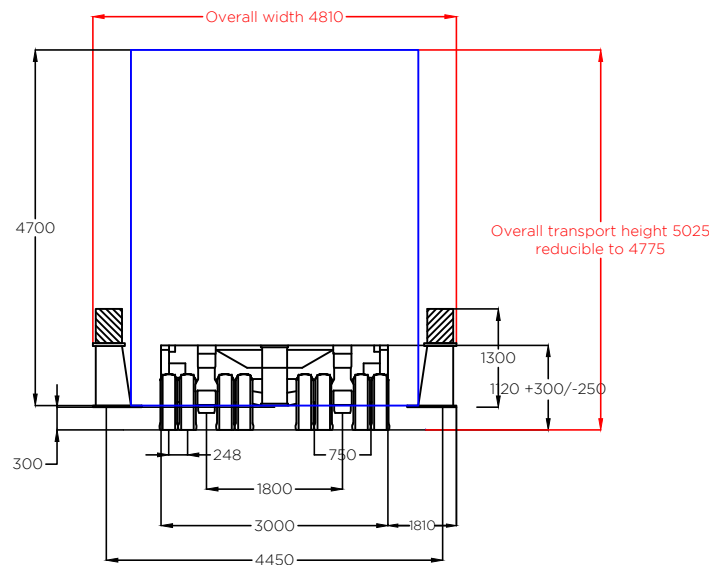
P:\Clients\Existing Clients\Island Green Power\24-1275 East Pye BESS Norfolk\Transport Configurations\24-1275.TC04 East Pye BESS 157te transformer 16 axle frame 3 m track width R0.dwg



Side Elevation - 16 axle girder frame trailer - concept model only
Indicative 183 te transformer
Scale 1:200

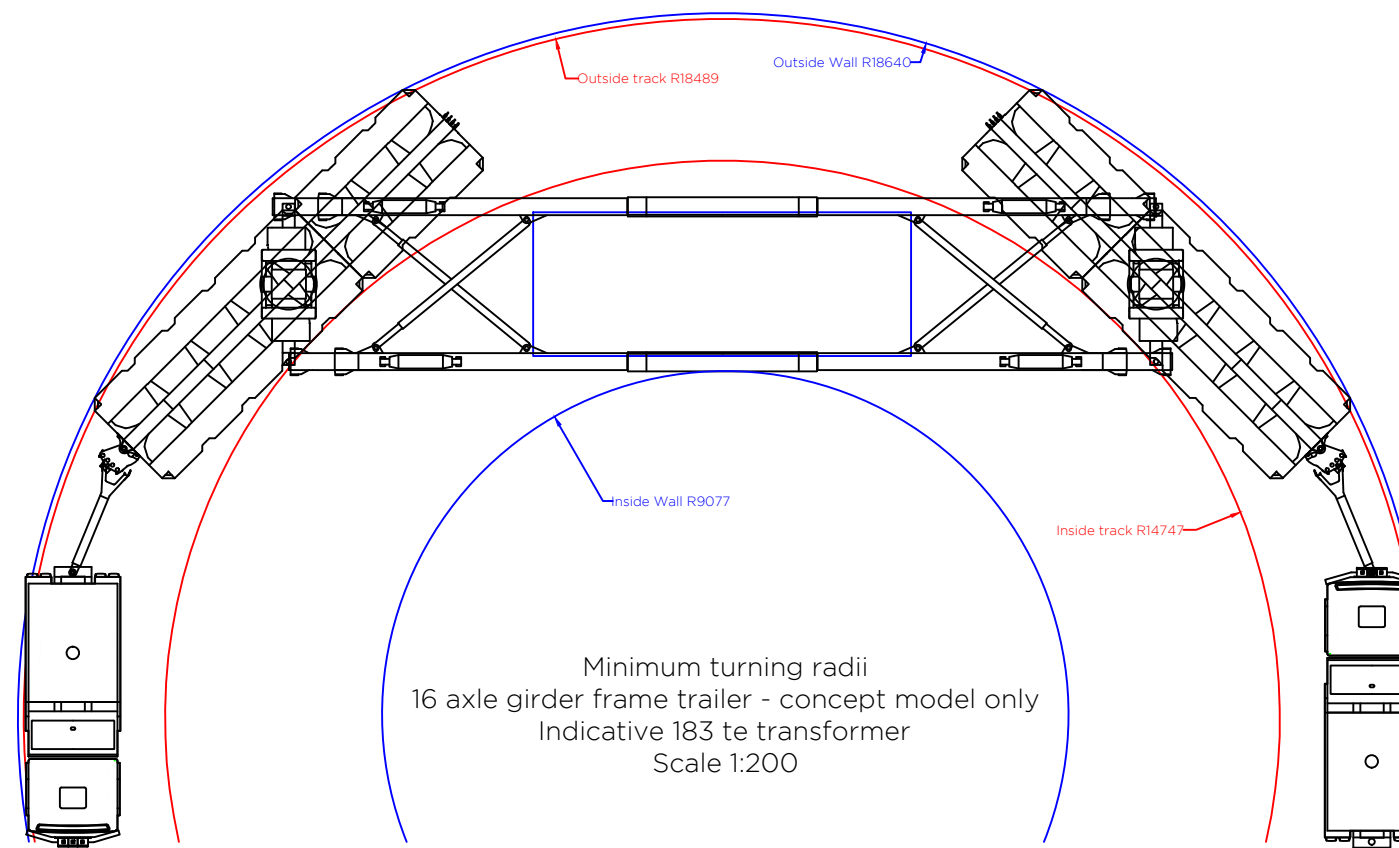


Plan View - 16 axle girder frame trailer - concept model only
Indicative 183 te transformer
Scale 1:200

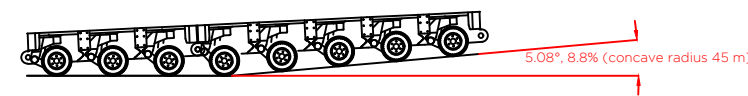
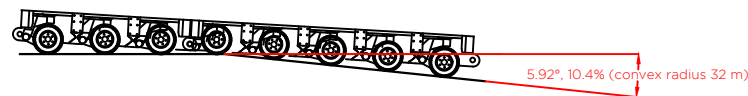


Profile view
Indicative 183 te transformer
Scale 1:100

NOTE: Final Design/Transport Arrangement to be Determined.
Detail is Illustrative Only.



Minimum turning radii
16 axle girder frame trailer - concept model only
Indicative 183 te transformer
Scale 1:200



Vertical curve negotiability information
based on manufacturers literature
(Scale 1:200)

Load Table	
16 axle girder frame trailer	
Self weight of load	183.0 te
Self weight of trailer	86.8 te
Self weight of aux. steelwork (for L&S)	0.0 te
Total combined weight	269.6 te
Load per trailer	134.8 te
Load per axle line (2 axles per line)	16.85 te
Load per wheel (4 wheels per axle)	2.11 te
Overall ground bearing pressure	4.28 te/m ²

Tractors x2 (42 te)	
Front axle	8.0 te
Second steer	10.0 te
Rear axle	12.0 te
Rear axle	12.0 te

Notes:-
[1] The figures shown above are representative of the transport configuration portrayed. However as tractor and trailer arrangements vary then the loads and dimensions indicated should be treated as probable values.

[2] Actual dimensions, including axle spacing and mean running height, may vary slightly depending on manufacturer of trailer deployed.

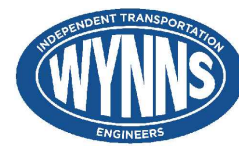
[3] All linear measures in millimeters unless stated otherwise.

[4] Transformer drawing indicative only.

Rev.	Date	Amendments
1		
0	20.02.26	Issued for comment

Revisions

Prepared By:



Shaftesbury House, 2 High Street,
Eccleshall, Stafford, ST21 6BZ
Tel: (01785) 850411

Independent Transportation Engineers

Client:



Project: East Pye BESS

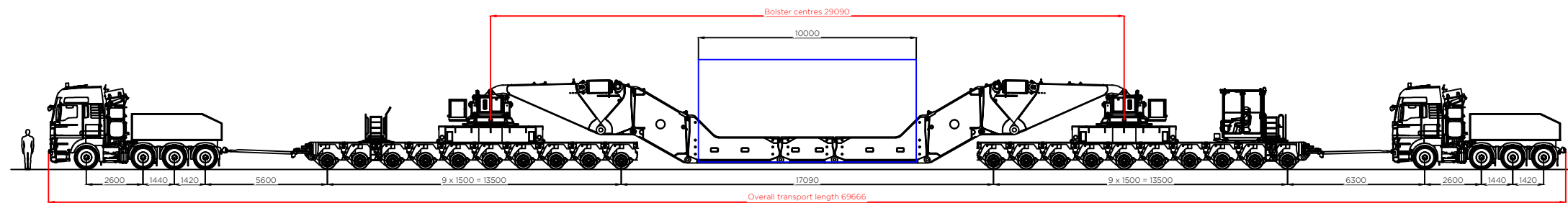
Title: Indicative Transport Configuration
183 te transformer carried within
16 axle girder frame trailer
showing minimum turning radii

Drawing Status: Final Report

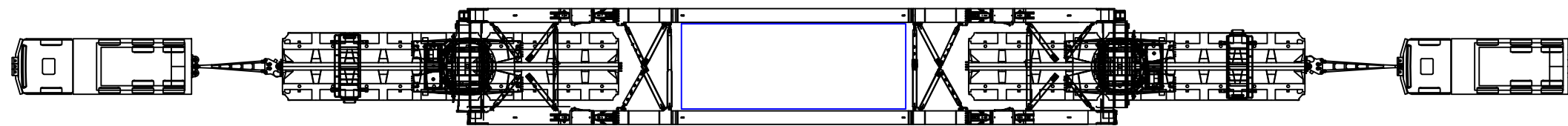
Scale (A3): As shown	Drawn By: JMB	Checked By: ---
DWG. No: 24-1275.TC05	Sheet: 1 of 1	Rev: 0

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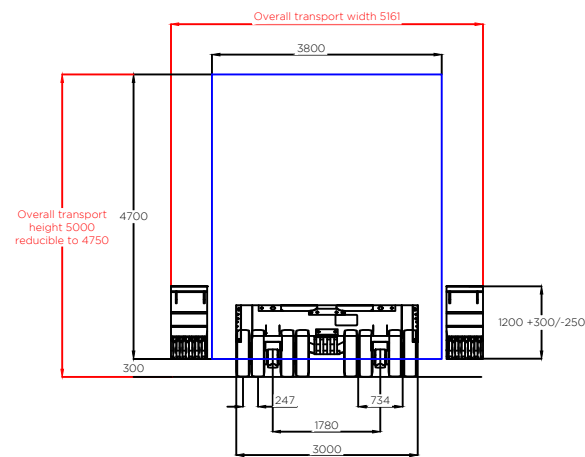
C:\Users\james.barrett\OneDrive - Wynns\Documents\Wynns\Existing Clients\Island Green Power\24-1275 East Pye BESS Norfolk\24-1275.TC05 East Pye BESS 183te transformer 16 Axle Girder Frame.R0.dwg



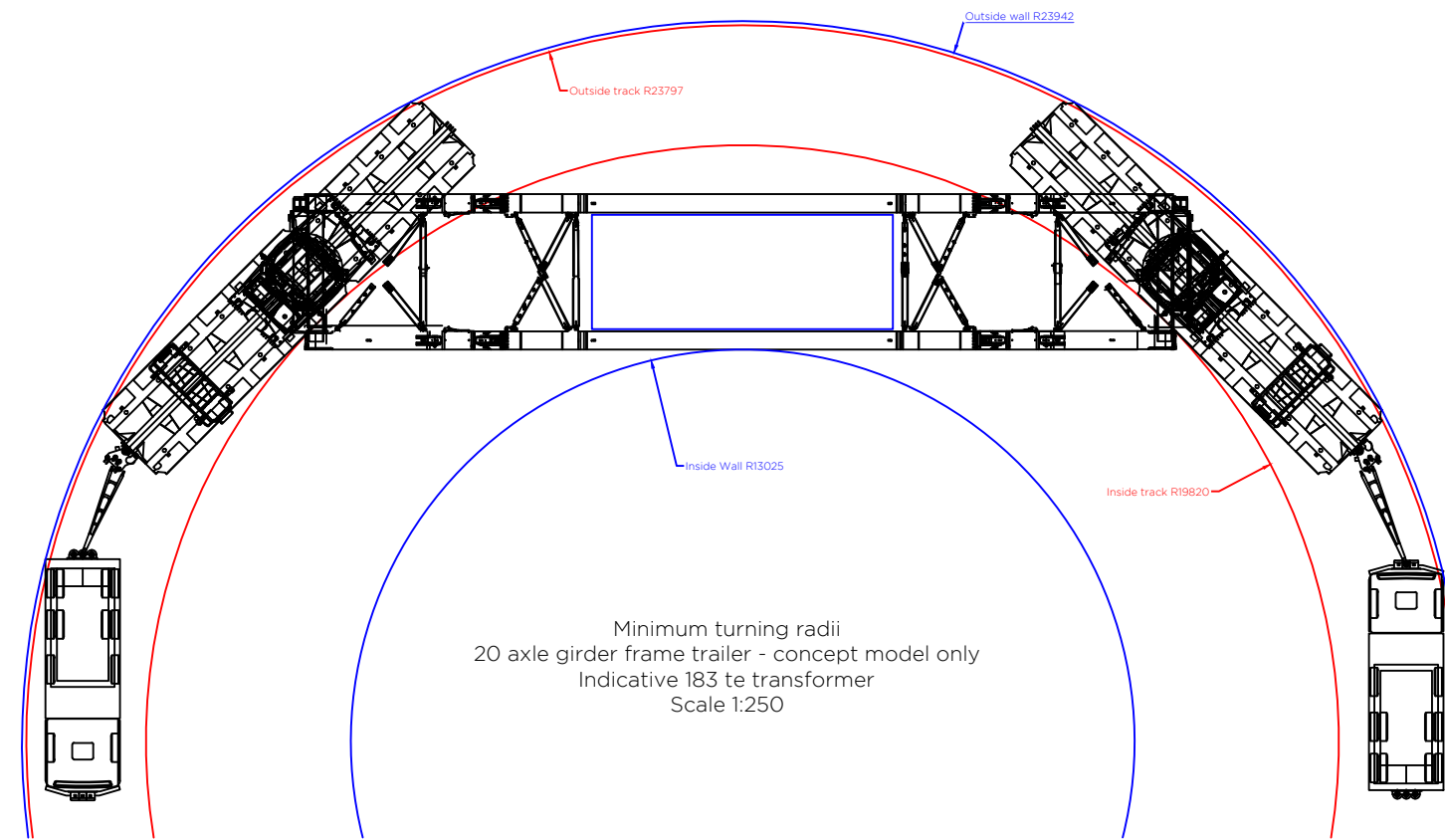
Side Elevation - 20 axle girder frame trailer - concept model only
Indicative 183 te transformer
Scale 1:250



Plan View - 20 axle girder frame trailer - concept model only
Indicative 183 te transformer
Scale 1:250



Profile view
Indicative 183 te transformer
Scale 1:125
NOTE: Final Design/Transport Arrangement to be Determined.
Detail is Illustrative Only.



Minimum turning radii
20 axle girder frame trailer - concept model only
Indicative 183 te transformer
Scale 1:250

Load table

20 axle girder frame trailer

Self weight of transformer	183.0 te
Self weight of trailer	134.0 te
Self weight of aux. steelwork (for L&S)	0.0 te
Total combined weight	317.0 te
Load per trailer	158.50 te
Load per axle line	15.85 te
Load per axle	7.92 te
Load per wheel (4 per axle)	1.98 te
Overall ground bearing pressure	3.91 te/m ²

Tractor(s) (42 te)

Front axle	7.0 te
Second steer	8.0 te
Rear axle	13.5 te
Rear axle	13.5 te

Notes:-

[1] The figures shown above are representative of the transport configuration portrayed. However as tractor and trailer arrangements vary then the loads and dimensions indicated should be treated as probable values.

[2] Actual dimensions, including axle spacing and mean running height, may vary slightly depending on manufacturer of trailer deployed.

[3] All linear measures in millimeters unless stated otherwise.

[4] Transformer drawing indicative only.

1		
0	20.02.26	Issued for comment
Rev.	Date	Amendments

Revisions

Prepared by:

Shaftesbury House, 2 High Street,
Eccleshall, Stafford, ST21 6BZ
Tel: (01785) 850411

Independent Transportation Engineers

Client:



Project:

East Pye BESS

Title:

Indicative Transport Configuration
183 te transformer carried within
20 axle girder frame trailer
showing minimum turning radii

Drawing status:

Final Report

Scale (A3): As shown	Drawn By: JMB	Checked By: ---
Dwg. no: 24-1275.TC06	Sheet: 1 of 1	Rev: 0

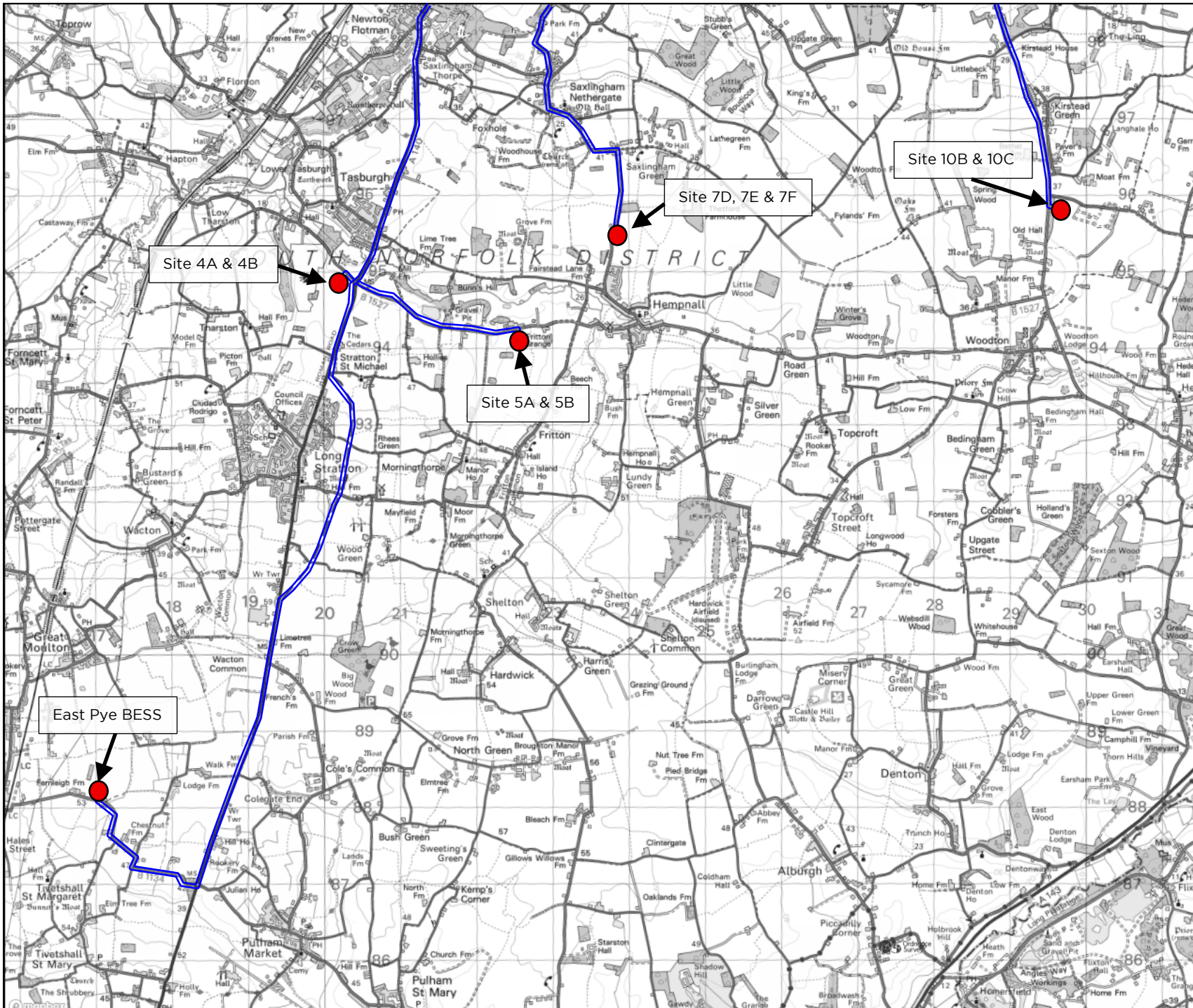
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C:\Users\james.barrett\OneDrive - Wynns\Documents\Wynns\Existing Clients\Island Green Power\24-1275 East Pye BESS Norfolk\24-1275.TC06 East Pye BESS 183te transformer 20 Axle Girder Frame.R0.dwg



Attachment 2

Overview Map



Key		
	Proposed Routes	
	Points of Interest	
	East Pye Locations Requiring AIL Deliveries	
B		
A	23.02.26	Revised with client comments
O	20.02.26	First Issue
Rev	Date	Amendments:
Revisions		
		Wynns Ltd. Independent Transportation Engineers
Shaftesbury House, 2 High Street, Eccleshall, Stafford, ST21 6BZ. Tel: (01785) 850411		
Client:		
Project:	East Pye Solar	
Title:	Site Overview Map	
Drawing Status:	Summary Report	
Scale (A4):	Drawn by: NTS	Checked by: ARP
Ref No.:	Sheet:	Rev.:
24-1275-Map1	1 of 1	0
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3. East Pye Solar Project Individual Summary Reports

3.1. East Pye Site 4A & 4B

Site	East Pye Solar Site 4A & 4B
Route Inspection and AIL Access Report Recently undertaken by Wynns?	Yes
Has Agreement in Principle (AIP) been provided by National Highways in line with the Water Preferred Policy	Not applicable as 95te nett transformer will be moved within STGO Category 3 and as such will not require Special Order permissions from National Highways.
National Highways AIP Reference Number	N/A
Proposed port/Marine access point of Delivery	Port of Immingham The port of Immingham is well established for heavy project cargo and no issues are expected in respect to marine access. It should be noted that as the load is STGO it will not be specifically limited to Immingham as the closest port but Immingham does provide suitable facilities.
Transformer Transport Weight considered during the most recent report in line with future project requirements	Weight - 95te nett transformer Length - 7.90m Width - 4.86m Height - 4.50m
Typical trailer used in Route Clearance works	A 5 bed 5 trailer at 146te gross weight as shown in drawing number 24-1275.TC03
Expected delivery date of next planned transformer if known	To be confirmed
Last Recorded Special Order Movement (according to available records)	No movements to this site as is a new development
Suggested route based on historical information	Take A47 exit onto A140 (OS Grid Ref: TG 21868 03961) Turn right A140 Ipswich Rd Turn right onto Unclassified Rd Continue to site (OS Grid Ref: TM 20278 95014)
Is a map available of the proposed route(s)?	Yes - See Attachment 3
Any Known Problems for AIL Access in terms of structures?	Yes - A140 Dunston Railway Bridge (OS Grid Ref: TG 22321 02853) is currently under assessment by load comparison exercise to confirm structural AIL loading capacity. Should A140 Dunston Rail Bridge assessment provide a negative result, further investigation will be required in order to gain access from direction of Ipswich. There is

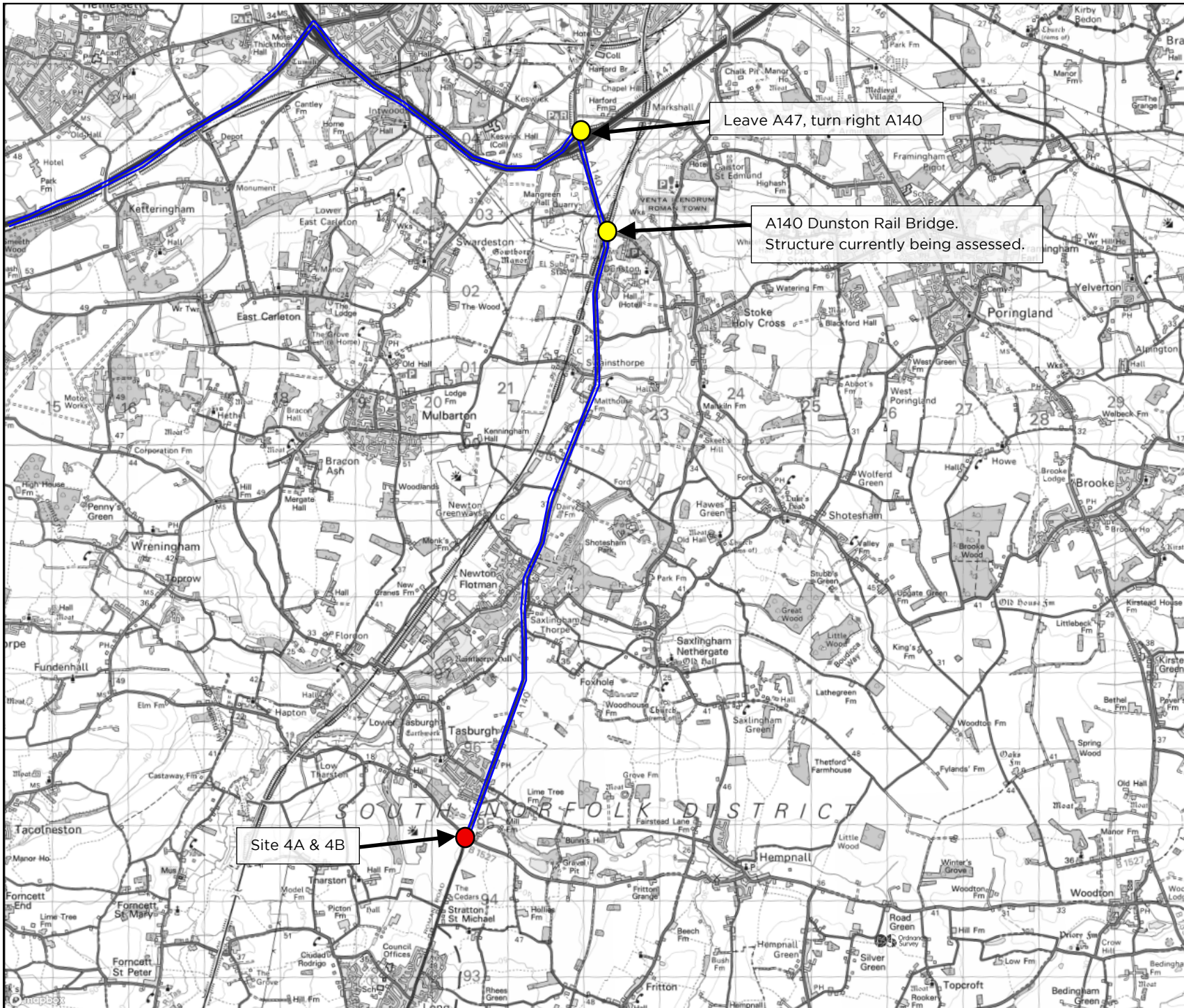


Site	East Pye Solar Site 4A & 4B
	however the need for the overbridging of A140 Brockford Street Bridge.
Authorities consulted in respect to AIL Access	<ul style="list-style-type: none"> • A1(M) Alconbury to Peterborough DBFO • Bedfordshire/Hertfordshire/Cambridgeshire Constabulary • Humberside Police • Leicestershire Police • Lincolnshire County Council • Lincolnshire Police • National Highways Area 7 • National Highways East Region • National Highways Yorkshire & North East Region • Network Rail LC & Rail over Road • Norfolk County Council Abnormal Load Service • Norfolk and Suffolk Constabulary • North Lincolnshire Council • Northamptonshire Police • Nottinghamshire Police • PRAX Lindsey Oil Refinery • Phillips 66 Oil Refinery
Any Known Problems for AIL Access in terms of Negotiability and other Route Comments?	No
Any Known Problems for AIL Access in terms of Onsite issues?	N/A – Proposal Stage
Do routing issues currently present a serious risk that access to the site may be restricted?	N/A
Any other Relevant Information and Notes: N/A	



Attachment 3

Map



Key		
	Proposed Routes	
	Points of Interest	
	East Pye Substation Location	
B		
A		
O	20.02.26	First Issue
Rev	Date	Amendments:
Revisions		
Wynns Ltd. Independent Transportation Engineers Shaftesbury House, 2 High Street, Eccleshall, Stafford, ST21 6BZ. Tel: (01785) 850411		
Client:		
Project:	East Pye Solar	
Title:	Site 4A & 4B Map	
Drawing Status:	Summary Report	
Scale (A4):	Drawn by:	Checked by:
NTS	SMB	ARP
Ref No.:	Sheet:	Rev.:
24-1275-Map1	1 of 1	0
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3.2. East Pye Site 5A & 5B

Site	East Pye Solar Site 5A & 5B
Route Inspection and AIL Access Report Recently undertaken by Wynns?	Yes
Has Agreement in Principle (AIP) been provided by National Highways in line with the Water Preferred Policy	<p>Not applicable for 95te nett as transformer will be moved within STGO Category 3 and as such will not require Special Order permissions from National Highways.</p> <p>For 183te nett transformer an AIP application will be made when route clearance works are complete. This is anticipated to be either Port of Kings Lynn or Port of Ipswich.</p>
National Highways AIP Reference Number	N/A
Proposed port/Marine access point of Delivery	<p>Port of Immingham (For 95te Transformer Only) The port of Immingham is well established for heavy project cargo and no issues are expected in respect to marine access. It should be noted that as the load is STGO it will not be specifically limited to Immingham as the closest port but Immingham does provide suitable facilities.</p> <p>Port of Kings Lynn The port of Kings Lynn is also a well established port and has been used recently for access to National Grid Norwich Main Substation AIL project cargo. In early 2026 Port of Kings Lynn received heavy project cargo for the Hornsea 3 project.</p> <p>Port of Ipswich The port of Ipswich East Quay is a well established for AIL project cargo and has most recently seen the delivery of large synchronous condenser to the National Grid Yaxley project. The onward route requires the traversal of Orwell Bridge which is possible for movements of this size under a full road closure whilst the transport traverses.</p> <p>ABP Heavy Lift Policy Both Port of Kings Lynn and Port of Ipswich are owned and operated by Associated British Ports. Therefore, the ABP Heavy Lift Policy must be adhered to.</p>
Transformer Transport Weight considered during the most recent report in line with future project requirements	<p>Weight - 95te nett transformer Length - 7.90m Width - 4.86m Height - 4.50m</p> <p>Weight - 183te nett transformer</p>



Site	East Pye Solar Site 5A & 5B
	Length - 9.40m Width - 3.85m Height - 4.9m
Typical trailer used in Route Clearance works	95te Tx - A 5 bed 5 trailer at 146te gross weight as shown in drawing number 24-1275.TC03 183te Tx - A 16 axle girder frame trailer at 269.6te gross weight as shown in drawing number 24-1275.TC05 along with a 20 axle girder frame trailer at 317.0te gross weight as shown in drawing number 24-1275.TC06.
Expected delivery date of next planned transformer if known	To be confirmed
Last Recorded Special Order Movement (according to available records)	No movements to this site as is a new development
Suggested route based on historical information	Take A47 exit onto A140 (OS Grid Ref: TG 21868 03961) Turn right A140 Ipswich Rd Turn left B1527 Continue to site (OS Grid Ref: TM 22417 94149)
Is a map available of the proposed route(s)?	Yes - See Attachment 4
Any Known Problems for AIL Access in terms of structures?	Yes - A140 Dunston Railway Bridge, (OS Grid Ref: TG 22321 02853) is currently under assessment to confirm structural AIL loading capacity. Dunston Railway Bridge is responsibility of Norfolk County Council. Should A140 Dunston Rail Bridge assessment provide a negative result, further investigation will be required in order to gain access from direction of Ipswich. There is however the need for the overbridging of A140 Brockford Street Bridge. Detailed discussions with Suffolk County Council and Norfolk and Suffolk Constabulary resulted in objection of overbridging due to the associated disruption caused.
Authorities consulted in respect to AIL Access	<ul style="list-style-type: none"> • A1(M) Alconbury to Peterborough DBFO • Bedfordshire/Hertfordshire/Cambridgeshire Constabulary • Humberside Police • Leicestershire Police • Lincolnshire County Council • Lincolnshire Police

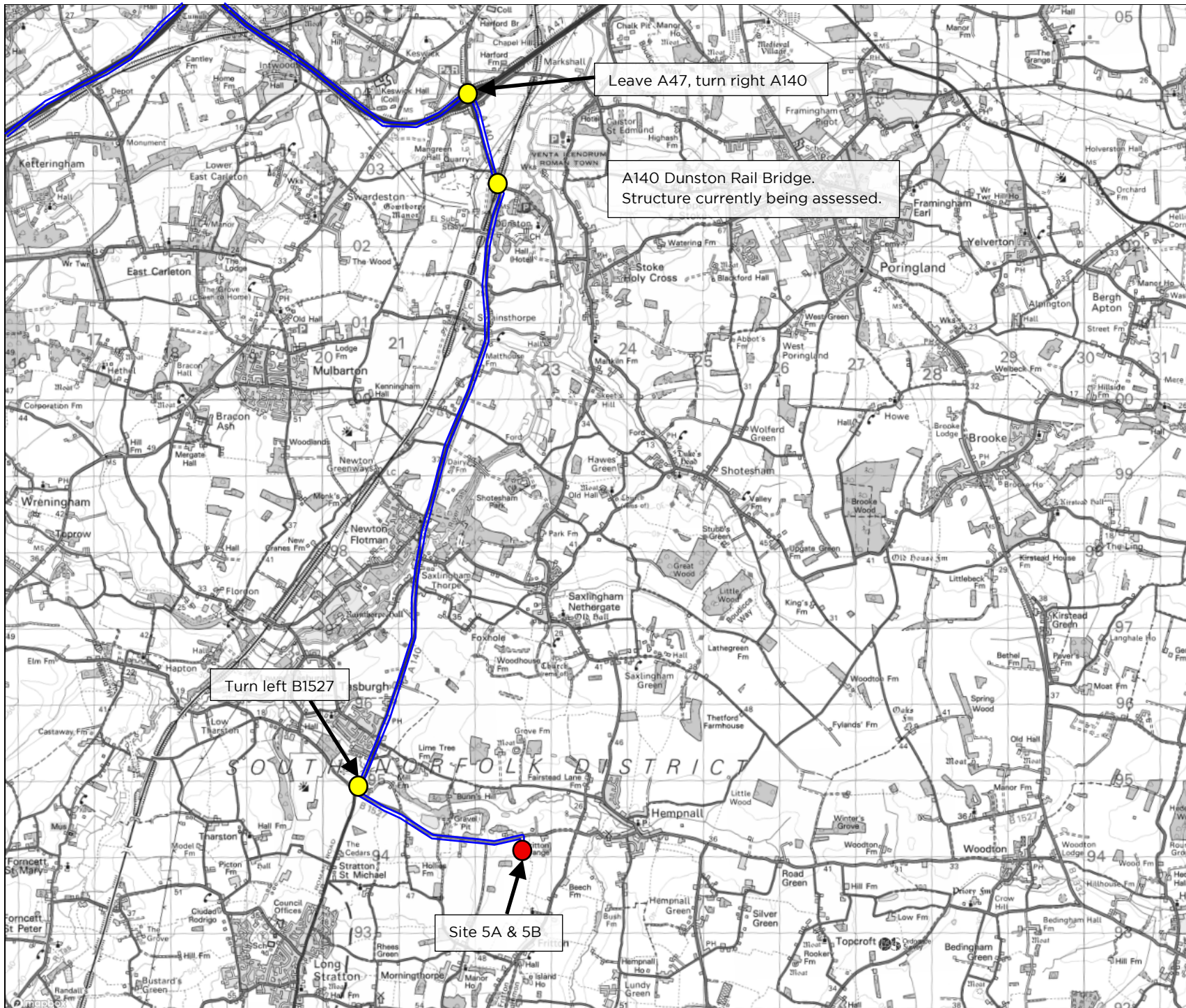


Site	East Pye Solar Site 5A & 5B
	<ul style="list-style-type: none"> • National Highways Area 7 • National Highways East Region • National Highways Yorkshire & North East Region • Network Rail LC & Rail over Road • Norfolk County Council Abnormal Load Service • Norfolk and Suffolk Constabulary • North Lincolnshire Council • Northamptonshire Police • Nottinghamshire Police • PRAX Lindsey Oil Refinery • Phillips 66 Oil Refinery
Any Known Problems for AIL Access in terms of Negotiability and other Route Comments?	No
Any Known Problems for AIL Access in terms of Onsite issues?	N/A - Proposal Stage
Do routing issues currently present a serious risk that access to the site may be restricted?	N/A
Any other Relevant Information and Notes: N/A	



Attachment 4

Map



Key		
	Proposed Routes	
	Points of Interest	
	East Pye Substation Location	
B		
A		
O	20.02.26	First Issue
Rev	Date	Amendments:
Revisions		
		Wynns Ltd. Independent Transportation Engineers
Shaftesbury House, 2 High Street, Eccleshall, Stafford, ST21 6BZ. Tel: (01785) 850411		
Client:		
Project:	East Pye Solar	
Title:	Site 5A & 5B Map	
Drawing Status:	Summary Report	
Scale (A4):	Drawn by:	Checked by:
NTS	SMB	ARP
Ref No.:	Sheet:	Rev.:
24-1275-Map1	1 of 1	0
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3.3. East Pye Site 7D, 7E & 7F

Site	East Pye Solar Site 7D, 7E & 7F
Route Inspection and AIL Access Report Recently undertaken by Wynns?	Yes
Has Agreement in Principle (AIP) been provided by National Highways in line with the Water Preferred Policy	Not applicable as 95te nett transformer will be moved within STGO Category 3 and as such will not require Special Order permissions from National Highways.
National Highways AIP Reference Number	N/A
Proposed port/Marine access point of Delivery	Port of Immingham The port of Immingham is well established for heavy project cargo and no issues are expected in respect to marine access. It should be noted that as the load is STGO it will not be specifically limited to Immingham as the closest port but Immingham does provide suitable facilities.
Transformer Transport Weight considered during the most recent report in line with future project requirements	Weight - 95te nett transformer Length - 7.90m Width - 4.86m Height - 4.50m
Typical trailer used in Route Clearance works	95te Tx - A 5 bed 5 trailer at 146te gross weight as shown in drawing number 24-1275.TC03
Expected delivery date of next planned transformer if known	To be confirmed
Last Recorded Special Order Movement (according to available records)	No movements to this site as is a new development
Suggested route based on historical information	Take A47 exit onto A146 (OS Grid Ref: TG 24912 06173) Turn right Loddon Rd Turn right B1332 Bungay Rd Turn right Stoke Rd Turn left Norwich Rd Continue to site (OS Grid Ref: TM 23784 95492)
Is a map available of the proposed route(s)?	Yes - See Attachment 5
Any Known Problems for AIL Access in terms of structures?	No - As the gross vehicle weight of the drawbar trailer is less than 150Te the vehicle will be considered as a STGO CAT3 movement. This requires that the movement is notified through the National Highways ESDAL platform with 5 working days notice. If no rejections are received within this window, the movement has permission to travel. Notification WYNL/305 was transmitted on 10/12/24 via

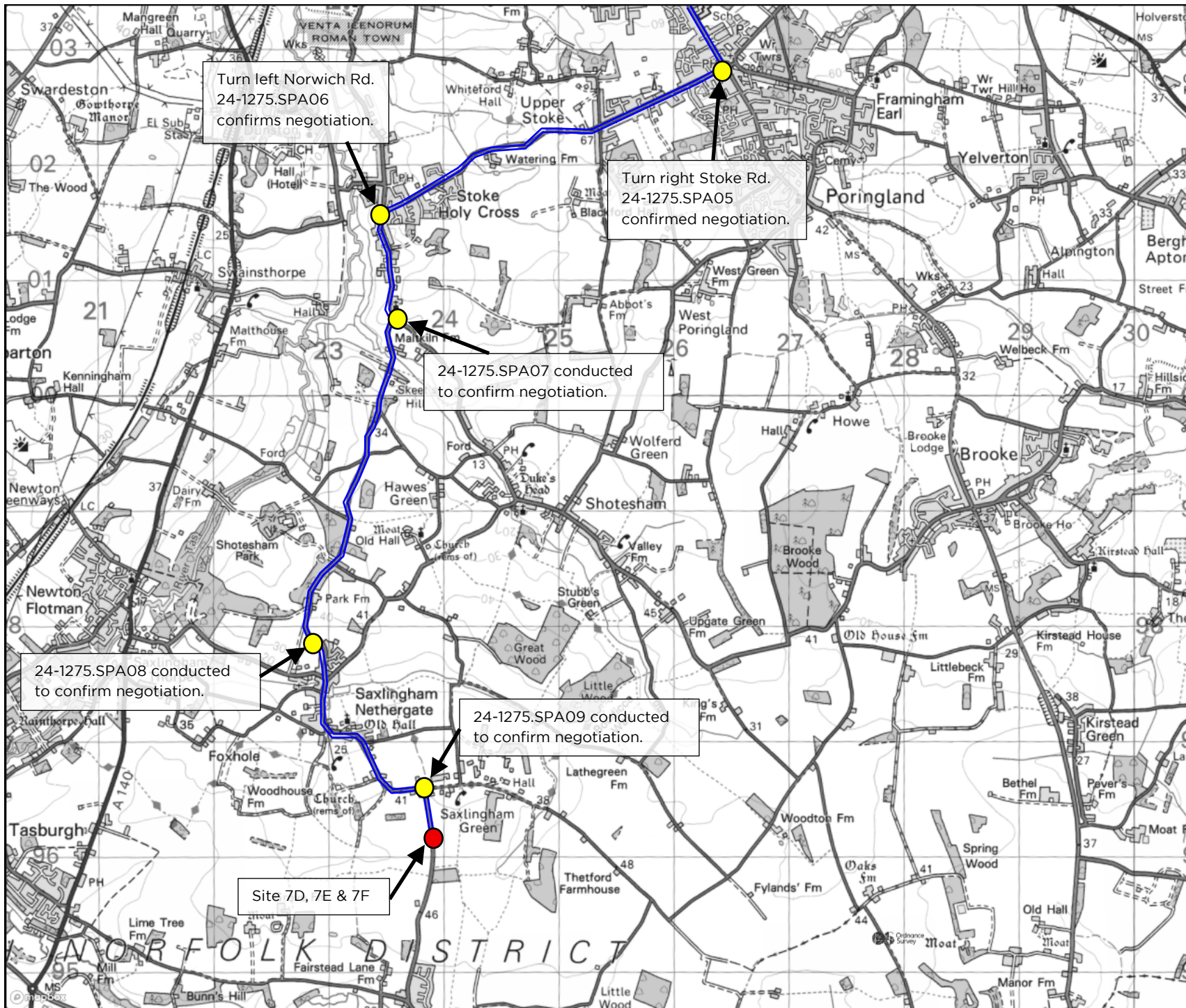


Site	East Pye Solar Site 7D, 7E & 7F
	<p>the ESDAL platform to which no rejections were received. Had a haulier made this same application, they would have therefore received the required permission to travel.</p>
<p>Authorities consulted in respect to AIL Access</p>	<ul style="list-style-type: none"> • A1(M) Alconbury to Peterborough DBFO • Bedfordshire/Hertfordshire/Cambridgeshire Constabulary • Humberside Police • Leicestershire Police • Lincolnshire County Council • Lincolnshire Police • National Highways Area 7 • National Highways East Region • National Highways Yorkshire & North East Region • Network Rail LC & Rail over Road • Norfolk County Council Abnormal Load Service • Norfolk and Suffolk Constabulary • North Lincolnshire Council • Northamptonshire Police • Nottinghamshire Police • PRAX Lindsey Oil Refinery • Phillips 66 Oil Refinery
<p>Any Known Problems for AIL Access in terms of Negotiability and other Route Comments?</p>	<p>Swept Path assessments were conducted to confirm the negotiation of route. See 24-1275.SPA05 to 24-1275.SPA09 in Attachment 6.</p> <p>The right hand turn from B1332 to Stoke Rd is to be negotiated by traveling in contraflow around roundabout.</p> <p>Oversail along the route to site is anticipated to remain within the public highway boundary.</p>
<p>Any Known Problems for AIL Access in terms of Onsite issues?</p>	<p>N/A - Proposal Stage</p>
<p>Do routing issues currently present a serious risk that access to the site may be restricted?</p>	<p>No</p>
<p>Any other Relevant Information and Notes: N/A</p>	



Attachment 5

Maps



Turn left Norwich Rd.
24-1275.SPA06
confirms negotiation.

Turn right Stoke Rd.
24-1275.SPA05
confirmed negotiation.

24-1275.SPA07 conducted
to confirm negotiation.

24-1275.SPA08 conducted
to confirm negotiation.

24-1275.SPA09 conducted
to confirm negotiation.

Site 7D, 7E & 7F

Key		
	Proposed Routes	
	Points of Interest	
	East Pye Substation Location	
B		
A		
O	20.02.26	First Issue
Rev	Date	Amendments:
Revisions		
Wynns Ltd. Independent Transportation Engineers Shaftesbury House, 2 High Street, Eccleshall, Stafford, ST21 6BZ. Tel: (01785) 850411		
Client:		
Project:	East Pye Solar	
Title:	Site 5A & 5B Map	
Drawing Status:	Summary Report	
Scale (A4):	Drawn by:	Checked by:
NTS	SMB	ARP
Ref No.:	Sheet:	Rev.:
24-1275-Map1	1 of 1	0
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Attachment 6

Swept Path Assessment



Swept Path Assessment
Indicative of 5-Axle bed 5-Axle Trailer
Constructed from Client Supplied Data and OS Mastermap Data
Scale 1:500

(Leader 1) Oversail and Overrun to Inside, Oversail to Outside of Turn, Expected to Remain Within Highway Boundary. Overrun Requires Plating/Packing to any Kerbs/Pavements.

(Leader 2) Street Furniture Positioned Along Turn, Includes Signage and Electrified Street Lamps, Conflict Not Expected, Positions Should be Cautioned.

(Leader 3) Depending on Growth at Time of Movement, Possible Trimming to Overhanging Branches May be Required.



The delivery vehicle can be seen turning right in contraflow from Norwich Road onto Stoke Road, at approximate OS grid reference: TG 26441 02838. The configuration is recommended to have full occupation of the available road way to aid in reducing any oversail and overrun that may occur, appropriate planning should be implemented to ensure full occupation via escort is carried out. The configuration is expected to oversail/overrun on the inside of the turn, and minimal oversail to the outside of the turn. Overrun would require plating/packing to any present kerbs/pavements; it should be noted that the rear 5 axles would be recommended to be hand steered and tracked towards the island as the front axles join Stoke Road, this is expected to reduce/mitigate the possibility of overrun on the inside of the turn (Leader 1). It should be noted that street furniture is positioned on the inside and outside of the turn, this includes signage and an electrified street lamp. Although conflicts are not anticipated, street furniture positions are approximate and should be cautioned (Leader 2). It should be noted that a tree is positioned on the inside of the turn, depending on growth at the time of movement trimming/pruning to branches that may overhang into the carriageway may be required, though no conflicts are anticipated (Leader 3). Subject to implementing the recommended remedial works, and observing the stated cautions, this section is considered to be negotiable.

Swept Path Assessment
Indicative of 5-Axle bed 5-Axle Trailer
Constructed from Client Supplied Data and OS Mastermap Data
Scale 1:250

Oversail of Approximately 30.77m²

Oversail and Overrun of Approximately 6.65m²

28.00m

2.02m

Oversail of Approximately 3.46m²

13.62m

0.39m

Direction of Travel



Legend:

- 5-axle bed 5-axle Trailer minimum turning arrangements Drawing ref. 24-1275.TC03
- Extent of tractor track
- Extent of trailer track
- Extent of oversail
- Extent of road boundary
- Extent of property boundary
- Overrun and oversail beyond kerb
- Overrun beyond kerb
- Oversail beyond kerb

1		
0	02.09.25	Issued for comment
Rev.	Date	Amendments

Revisions

Prepared by:

Shaftesbury House, 2 High Street,
Eccleshall, Stafford, ST21 6BZ
Tel: (01785) 850411

Independent Transportation Engineers

Client:

Project: East Pye

Title: Swept Path Assessments
Showing the right turn in contraflow from Norwich Road onto Stoke Road, at approximate OS grid reference: TG 26441 02838, on indicative 5-axle bed 5-axle trailer transporting a 100te transformer.

Drawing status: Final Report

Scale (A3): As shown	Drawn by: MTO	Checked by: PW
Dwg. no: 24-1221.SPA05	Sheet: 1 of 2	Rev: 0

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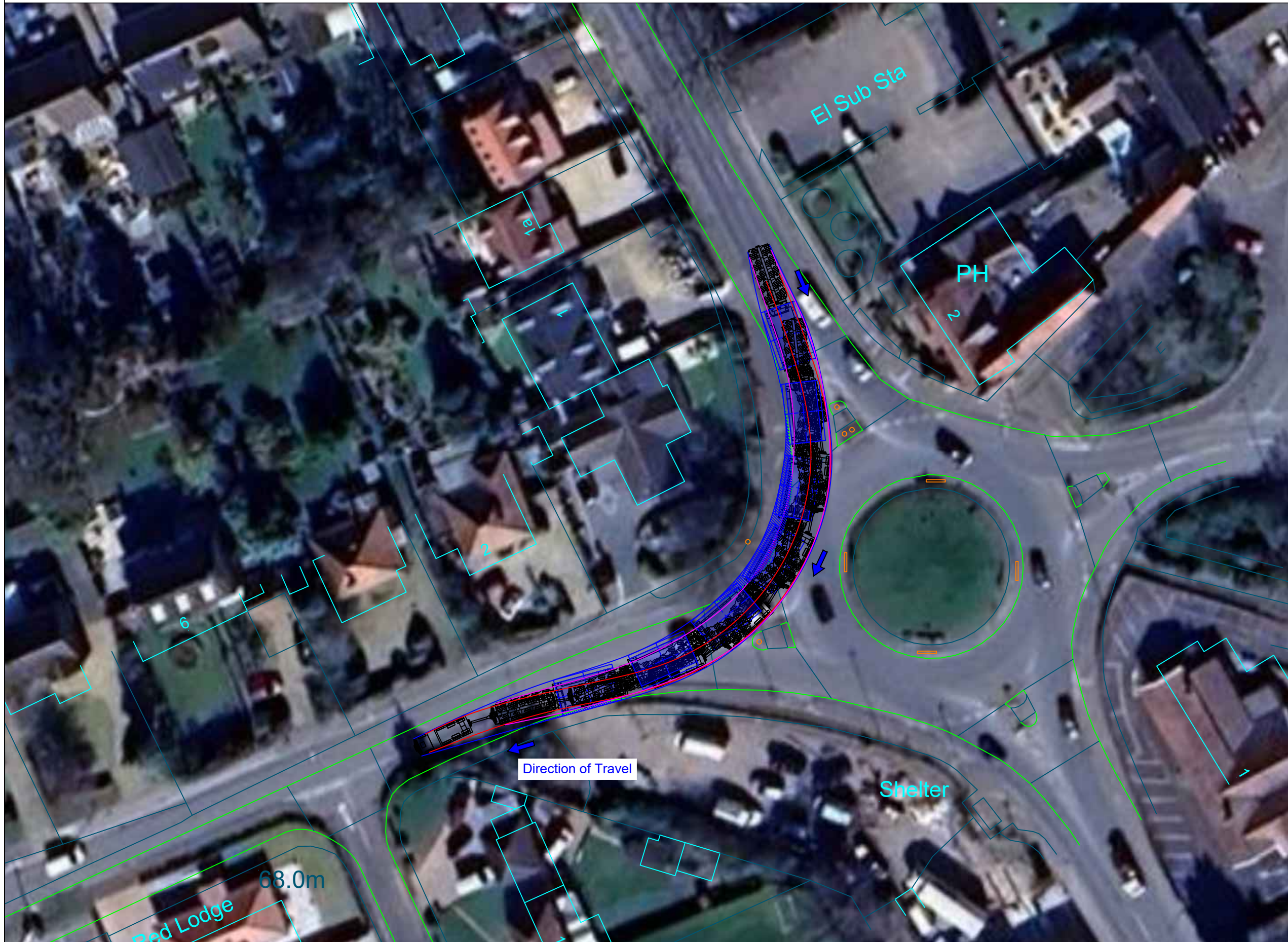
Swept Path Assessment
 Indicative of 5-Axle bed 5-Axle Trailer
 Constructed from Client Supplied Data and OS Mastermap Data
 Scale 1:500

NOTE: Overlay onto aerial image is not representative of the configuration relative to the environment. This is for illustrative purposes only, and should only be taken as such.



Legend:

-  5-axle bed 5-axle Trailer
minimum turning arrangements
Drawing ref. 24-1275.TC03
-  Extent of tractor track
-  Extent of trailer track
-  Extent of oversail
-  Extent of road boundary
-  Extent of property boundary
-  Overrun and oversail beyond kerb
-  Overrun beyond kerb
-  Oversail beyond kerb



1		
0	02.09.25	Issued for comment
Rev.	Date	Amendments

Prepared by:



Shaftesbury House, 2 High Street,
 Eccleshall, Stafford, ST21 6BZ
 Tel: (01785) 850411

Independent Transportation Engineers

Client:



Project: East Pye

Title: Swept Path Assessments
 Showing the right turn in contraflow from Norwich Road onto Stoke Road, at approximate OS grid reference: TG 26441 02838, on indicative 5-axle bed 5-axle trailer transporting a 100te transformer.

Drawing status: Final Report

Scale (A3): As shown	Drawn by: MTO	Checked by: PW
Dwg. no: 24-1221.SPA05	Sheet: 2 of 2	Rev: 0

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Swept Path Assessment
Indicative of 5-Axle bed 5-Axle Trailer
Constructed from Client Supplied Data and OS Mastermap Data
Scale 1:250

GP

Oversail of Approximately
11.09m²

Oversail of Approximately
8.69m²

17.70m

0.98m

0.91m

14.41m

The delivery vehicle can be seen turning left onto Norwich Road from Long Lane, at approximate OS grid reference: TG 23476 01587. The configuration is recommended to have full occupation of the available road way to aid in reducing any oversail and overrun that may occur, appropriate planning should be implemented to ensure full occupation via escort is carried out. The configuration is expected to oversail on the inside and outside of the turn (Leader 1). It should be noted that street furniture is positioned on the inside and outside of the turn, this includes signage and telecom poles. Although conflicts are not anticipated, street furniture positions are approximate and should be cautioned (Leader 2). It should be noted that overhead lines are present, confirmation of overhead line height is recommended, configuration anticipates a running height of circa 4.80m, however, this may be reduced using trailer hydraulics (Leader 3). Subject to implementing the recommended remedial works, and observing the stated cautions, this section is considered to be negotiable.

Swept Path Assessment
Indicative of 5-Axle bed 5-Axle Trailer
Constructed from Client Supplied Data and OS Mastermap Data
Scale 1:500

(Leader 1) Oversail to the
Inside and Outside of Turn,
Expected to Remain Within
Highway Boundary.

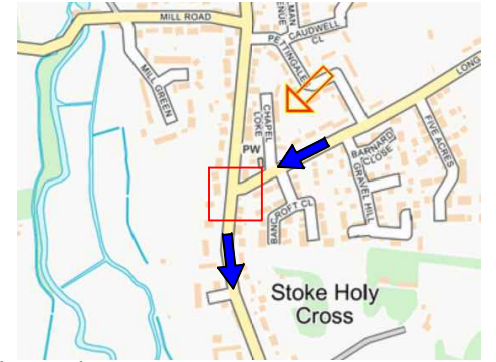
(Leader 2) Street Furniture
Positioned Along Turn, Includes
Signage and Telecom Poles,
Conflict Not Expected, Positions
Should be Cautioned.

(Leader 3) Telecom Overhead Lines Present
Along Route, Caution Advised, Confirmation
of Line Heights Recommended. Configuration
Running Height Anticipated to be Circa 4.80m.

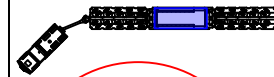





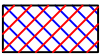
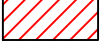

Direction of Travel



Location Plan



Legend:

-  5-axle bed 5-axle Trailer minimum turning arrangements Drawing ref. 24-1275.TC03
-  Extent of tractor track
-  Extent of trailer track
-  Extent of oversail
-  Extent of road boundary
-  Extent of property boundary
-  Overrun and oversail beyond kerb
-  Overrun beyond kerb
-  Oversail beyond kerb

1		
0	02.09.25	Issued for comment
Rev.	Date	Amendments

Revisions

Prepared by:



Shaftesbury House, 2 High Street,
Eccleshall, Stafford, ST21 6BZ
Tel: (01785) 850411

Independent Transportation Engineers

Client:



Project: East Pye

Title: Swept Path Assessments
Showing the left turn from Long Lane onto Norwich Road, at approximate OS grid reference: TG 23476 01587, on indicative 5-axle bed 5-axle trailer transporting a 100te transformer.

Drawing status: Final Report

Scale (A3): As shown	Drawn by: MTO	Checked by: PW
Dwg. no: 24-1221.SPA06	Sheet: 1 of 2	Rev: 0

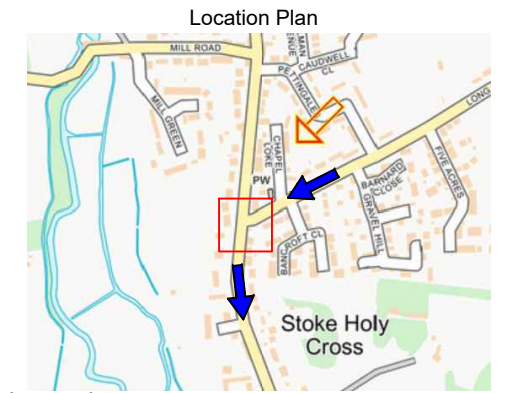
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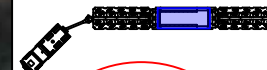










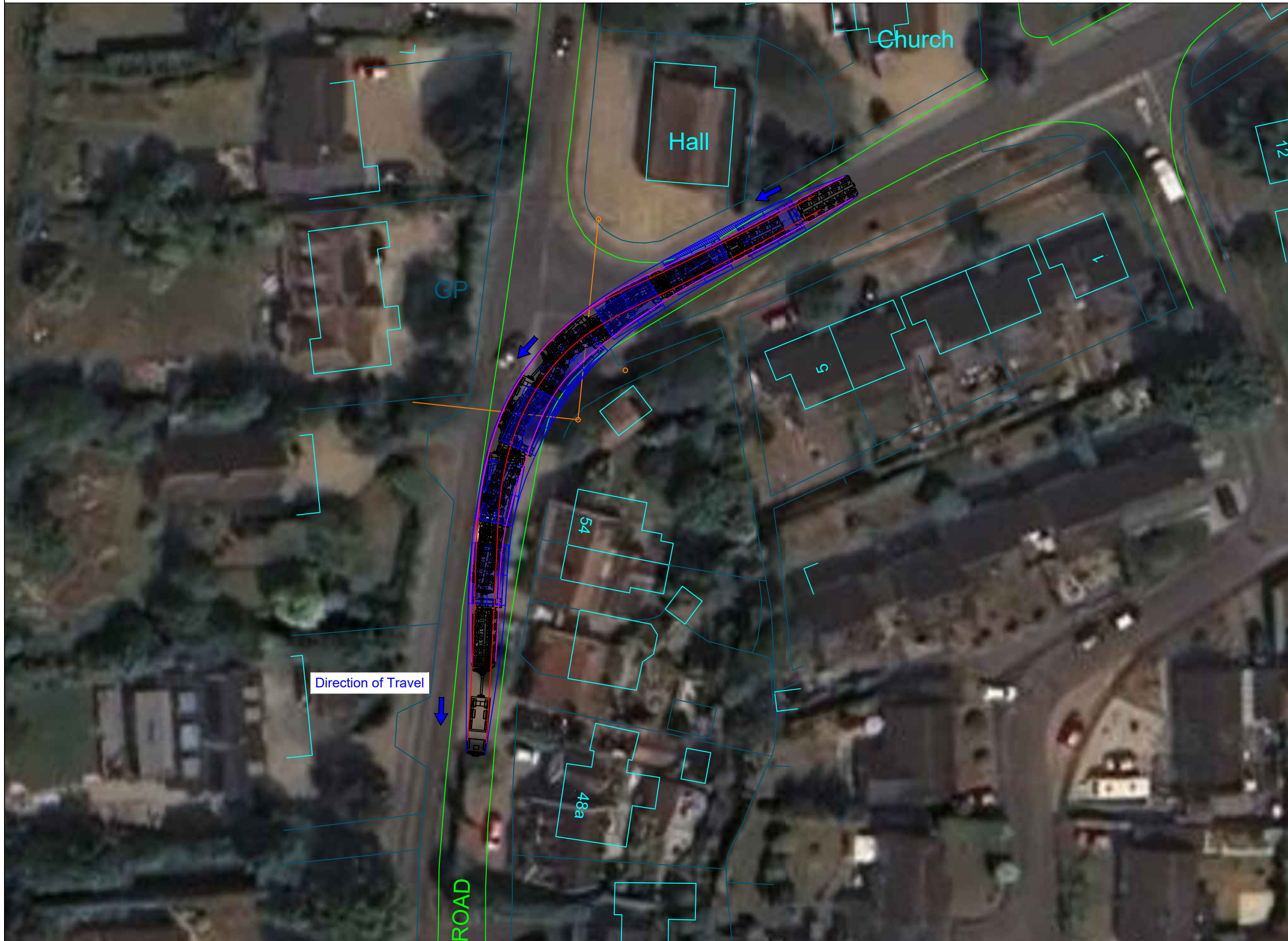
Swept Path Assessment
 Indicative of 5-Axle bed 5-Axle Trailer
 Constructed from Client Supplied Data and OS Mastermap Data
 Scale 1:500

NOTE: Overlay onto aerial image is not representative of the configuration relative to the environment. This is for illustrative purposes only, and should only be taken as such.



Legend:

-  5-axle bed 5-axle Trailer
minimum turning arrangements
Drawing ref. 24-1275.TC03
-  Extent of tractor track
-  Extent of trailer track
-  Extent of oversail
-  Extent of road boundary
-  Extent of property boundary
-  Overrun and oversail beyond kerb
-  Overrun beyond kerb
-  Oversail beyond kerb



1		
0	02.09.25	Issued for comment
Rev.	Date	Amendments

Revisions

Prepared by:



Shaftesbury House, 2 High Street,
 Eccleshall, Stafford, ST21 6BZ
 Tel: (01785) 850411

Independent Transportation Engineers

Client:



Project: East Pye

Title: Swept Path Assessments
 Showing the left turn from Long Lane onto Norwich Road,
 at approximate OS grid reference: TG 23476 01587, on
 indicative 5-axle bed 5-axle trailer transporting a 100te
 transformer.

Drawing status: Final Report

Scale (A3): As shown	Drawn by: MTO	Checked by: PW
Dwg. no: 24-1221.SPA06	Sheet: 2 of 2	Rev: 0

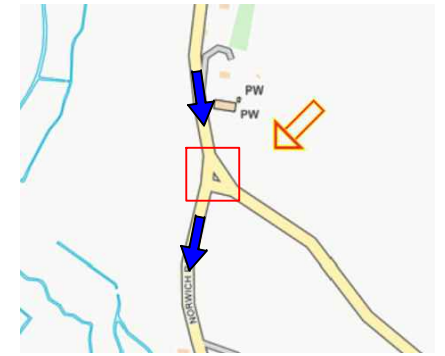
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Swept Path Assessment
 Indicative of 5-Axle bed 5-Axle Trailer
 Constructed from Client Supplied Data and OS Mastermap Data
 Scale 1:500

Location Plan



Legend:

-  5-axle bed 5-axle Trailer minimum turning arrangements Drawing ref. 24-1275.TC03
-  Extent of tractor track
-  Extent of trailer track
-  Extent of oversail
-  Extent of road boundary
-  Extent of property boundary
-  Overrun and oversail beyond kerb
-  Overrun beyond kerb
-  Oversail beyond kerb

Oversail of Approximately 7.69m²

36.02m

0.55m

The delivery vehicle can be seen bearing right along Norwich Road, at approximate OS grid reference: TG 23552 00688.

The configuration is recommended to have full occupation of the available road way to aid in reducing any oversail and overrun that may occur, appropriate planning should be implemented to ensure full occupation via escort is carried out. The configuration is expected to oversail on the inside of the turn (Leader 1).

It should be noted that street furniture is positioned on the inside and outside of the turn, this includes signage and telecom poles. Although conflicts are not anticipated, street furniture positions are approximate and should be cautioned (Leader 2). Depending on growth at the time of movement, trimming/pruning to overhanging branches may be required to facilitate. It should be noted that overhead lines are present, confirmation of overhead line height is recommended, configuration anticipates a running height of circa 4.80m, however, this may be reduced using trailer hydraulics (Leader 4). Subject to implementing the recommended remedial works, and observing the stated cautions, this section is considered to be negotiable.

Swept Path Assessment
 Indicative of 5-Axle bed 5-Axle Trailer
 Constructed from Client Supplied Data and OS Mastermap Data
 Scale 1:500

(Leader 1) Oversail to the Inside of the Turn, Expected to Remain Within Highway Boundary.

(Leader 2) Caution Street Furniture Positioned Along Turn, Includes Signage and Telecom Poles, Conflict Not Expected, Positions Though Should be Cautioned.

(Leader 3) Depending on Growth at Time of Movement, Possible Trimming to Overhanging Branches May be Required Along Route.

(Leader 4) Telecom Overhead Lines Present Along Route, Caution Advised, Confirmation of Line Heights Recommended. Configuration Running Height Anticipated to be Circa 4.80m.

Direction of Travel

1		
0	03.09.25	Issued for comment
Rev.	Date	Amendments

Revisions

Prepared by:



Shaftesbury House, 2 High Street,
 Eccleshall, Stafford, ST21 6BZ
 Tel: (01785) 850411

Independent Transportation Engineers

Client:



Project:

East Pye

Title:

Swept Path Assessments
 Showing the configuration bearing right along Norwich Road, at approximate OS grid reference: TG 23552 00688, on indicative 5-axle bed 5-axle trailer transporting a 100te transformer.

Drawing status:

Final Report

Scale (A3):	Drawn by:	Checked by:
As shown	MTO	PW
Dwg. no:	Sheet:	Rev:
24-1221.SPA07	1 of 2	0

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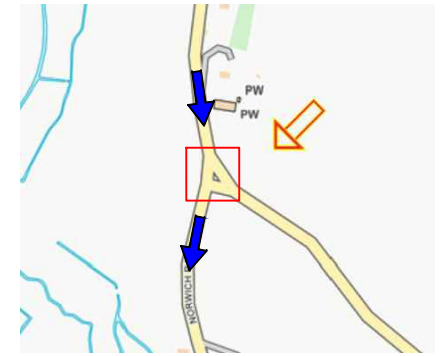
P:\Clients\Existing Clients\Island Green Power\24-1275 East Pye BESS Norfolk\Swept Path Assessments



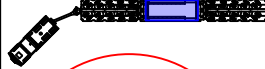








Swept Path Assessment
 Indicative of 5-Axle bed 5-Axle Trailer
 Constructed from Client Supplied Data and OS Mastermap Data
 Scale 1:1000

NOTE: Overlay onto aerial image is not representative of the configuration relative to the environment. This is for illustrative purposes only, and should only be taken as such.

Location Plan



Legend:

-  5-axle bed 5-axle Trailer
minimum turning arrangements
Drawing ref. 24-1275.TC03
-  Extent of tractor track
-  Extent of trailer track
-  Extent of oversail
-  Extent of road boundary
-  Extent of property boundary
-  Overrun and oversail beyond kerb
-  Overrun beyond kerb
-  Oversail beyond kerb



Direction of Travel

1		
0	03.09.25	Issued for comment
Rev.	Date	Amendments

Revisions

Prepared by:



Shaftesbury House, 2 High Street,
 Eccleshall, Stafford, ST21 6BZ
 Tel: (01785) 850411

Independent Transportation Engineers

Client:



Project:

East Pye

Title:

Swept Path Assessments
 Showing the configuration bearing right along Norwich Road, at approximate OS grid reference: TG 23552 00688, on indicative 5-axle bed 5-axle trailer transporting a 100te transformer.

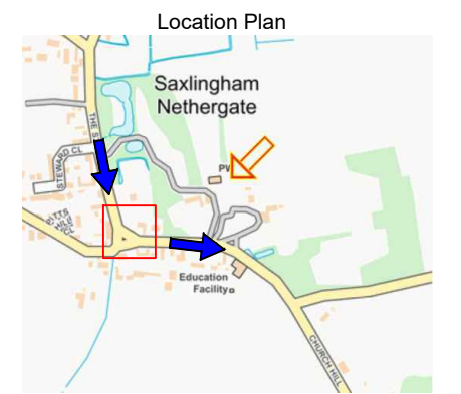
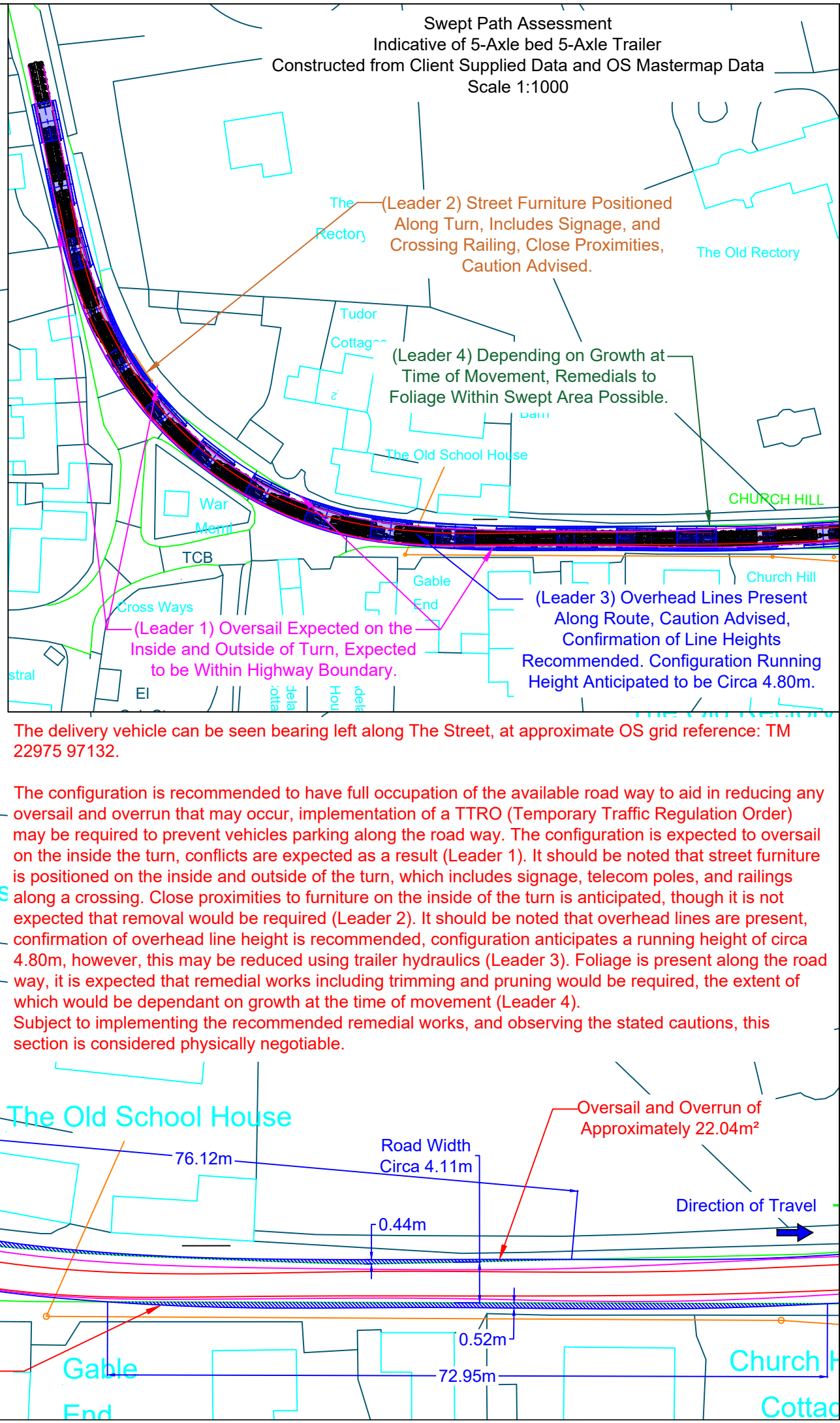
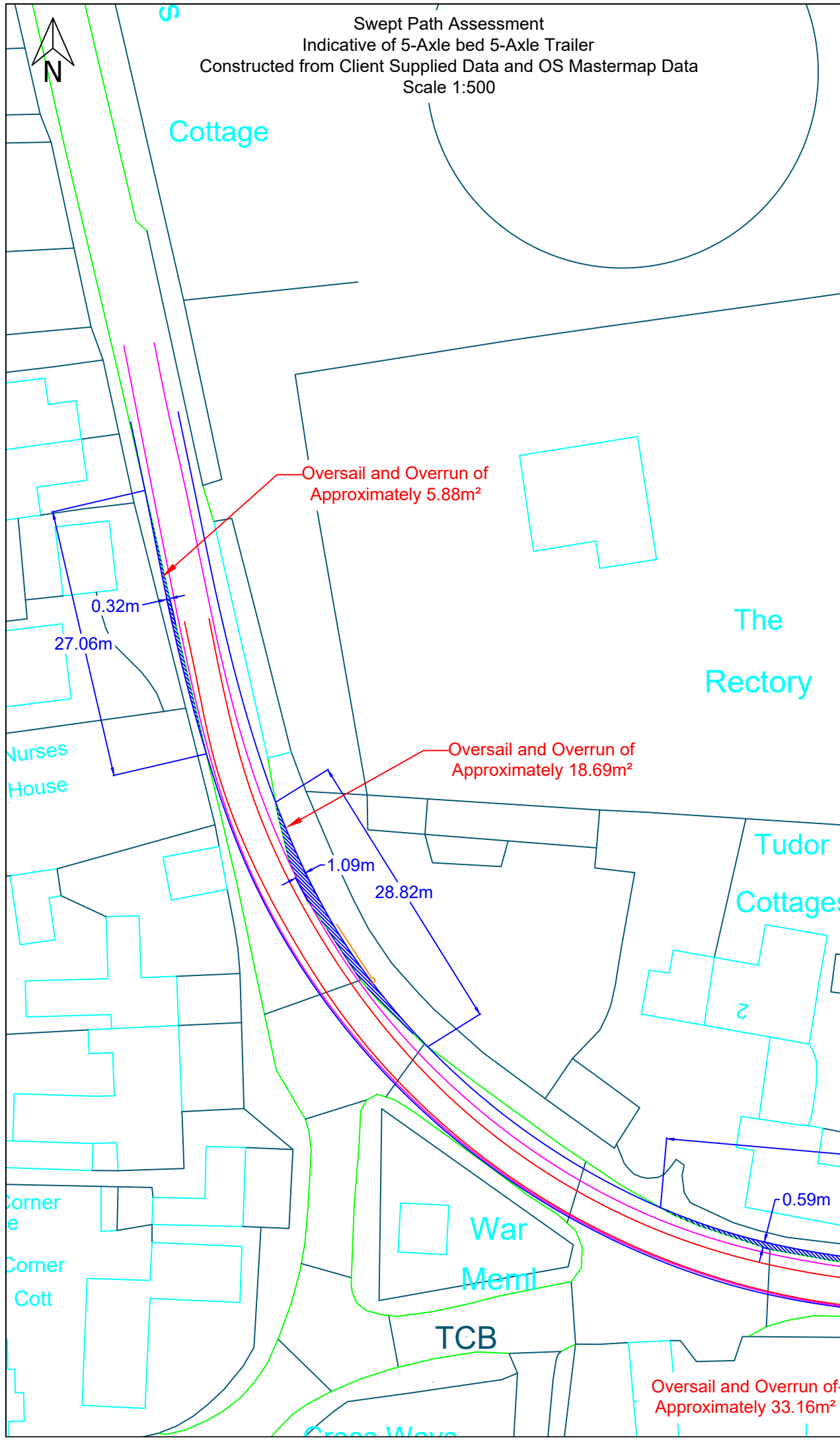
Drawing status:

Final Report

Scale (A3): As shown	Drawn by: MTO	Checked by: PW
Dwg. no: 24-1221.SPA07	Sheet: 2 of 2	Rev: 0

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P:\Clients\Existing Clients\Island Green Power\24-1275 East Pye BESS Norfolk\Swept Path Assessments



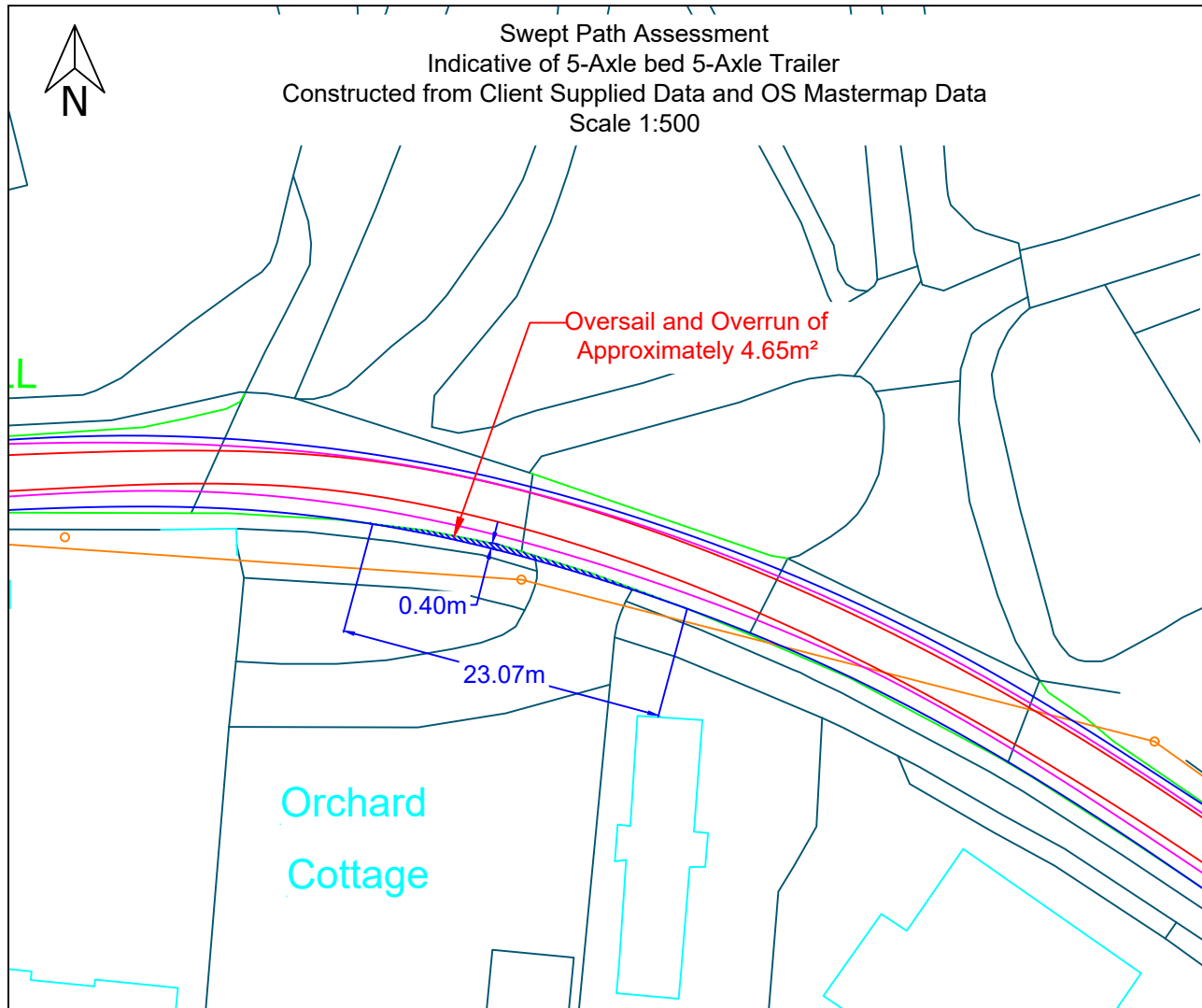
Legend:

- 5-axle bed 5-axle Trailer minimum turning arrangements Drawing ref. 24-1275.TC03
- Extent of tractor track
- Extent of trailer track
- Extent of oversail
- Extent of road boundary
- Extent of property boundary
- Oversail and overrun beyond kerb
- Oversail beyond kerb
- Oversail beyond kerb

The delivery vehicle can be seen bearing left along The Street, at approximate OS grid reference: TM 22975 97132.

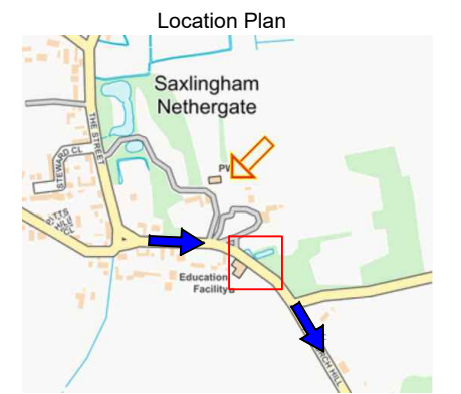
The configuration is recommended to have full occupation of the available road way to aid in reducing any oversail and overrun that may occur, implementation of a TTRO (Temporary Traffic Regulation Order) may be required to prevent vehicles parking along the road way. The configuration is expected to oversail on the inside the turn, conflicts are expected as a result (Leader 1). It should be noted that street furniture is positioned on the inside and outside of the turn, which includes signage, telecom poles, and railings along a crossing. Close proximities to furniture on the inside of the turn is anticipated, though it is not expected that removal would be required (Leader 2). It should be noted that overhead lines are present, confirmation of overhead line height is recommended, configuration anticipates a running height of circa 4.80m, however, this may be reduced using trailer hydraulics (Leader 3). Foliage is present along the road way, it is expected that remedial works including trimming and pruning would be required, the extent of which would be dependant on growth at the time of movement (Leader 4). Subject to implementing the recommended remedial works, and observing the stated cautions, this section is considered physically negotiable.

1		
0	04.09.25	Issued for comment
Rev.	Date	Amendments
Revisions		
Prepared by:		
Shaftesbury House, 2 High Street, Eccleshall, Stafford, ST21 6BZ Tel: (01785) 850411 Independent Transportation Engineers		
Client:		
Project: East Pye		
Title: Swept Path Assessments Showing the configuration bearing left along The Street, at approximate OS grid reference: TM 22975 97132, on indicative 5-axle bed 5-axle trailer transporting a 100t transformer.		
Drawing status: Final Report		
Scale (A3): As shown	Drawn by: MTO	Checked by: PW
Dwg. no: 24-1221.SPA08	Sheet: 1 of 3	Rev: 0
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P:\Clients\Existing Clients\Island Green Power\24-1275 East Pye BESS Norfolk\Swept Path Assessments		



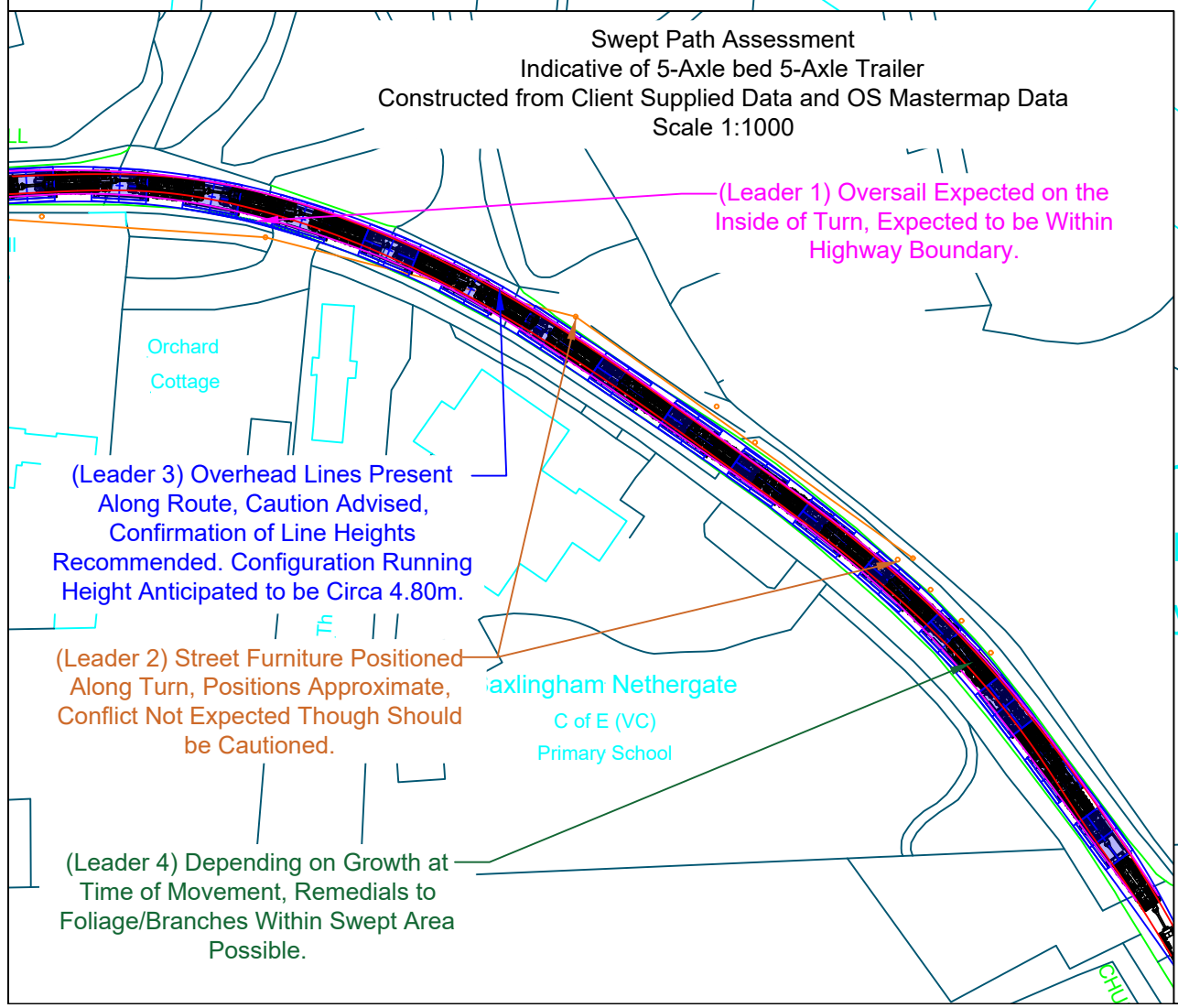
The delivery vehicle can be seen bearing left along The Street, at approximate OS grid reference: TM 22975 97132.

The configuration is recommended to have full occupation of the available road way to aid in reducing any oversail and overrun that may occur, implementation of a TTRO (Temporary Traffic Regulation Order) may be required to prevent vehicles parking along the road way. The configuration is expected to oversail on the inside the turn, though no conflicts are expected as a result (Leader 1). It should be noted that street furniture is positioned on the inside and outside of the turn, which includes signage, telecom poles, and railings, however no conflicts are expected to occur. It should be noted that positions are approximate and should be cautioned (Leader 2). It should be noted that overhead lines are present, confirmation of overhead line height is recommended, configuration anticipates a running height of circa 4.80m, however, this may be reduced using trailer hydraulics (Leader 3). Foliage is present along the road way, it is expected that remedial works including trimming and pruning would be required, the extent of which would be dependant on growth at the time of movement (Leader 4).
Subject to implementing the recommended remedial works, and observing the stated cautions, this section is considered physically negotiable.



Legend:

- 5-axle bed 5-axle Trailer minimum turning arrangements Drawing ref. 24-1275.TC03
- Extent of tractor track
- Extent of trailer track
- Extent of oversail
- Extent of road boundary
- Extent of property boundary
- Overrun and oversail beyond kerb
- Overrun beyond kerb
- Oversail beyond kerb



1		
0	04.09.25	Issued for comment
Rev.	Date	Amendments

Revisions

Prepared by:

Shaftesbury House, 2 High Street,
Eccleshall, Stafford, ST21 6BZ
Tel: (01785) 850411

Independent Transportation Engineers

Client:

Project: East Pye

Title: Swept Path Assessments
Showing the configuration bearing right along The Street, at approximate OS grid reference: TM 23204 97067, on indicative 5-axle bed 5-axle trailer transporting a 100te transformer.

Drawing status: Final Report

Scale (A3): As shown	Drawn by: MTO	Checked by: PW
Dwg. no: 24-1221.SPA08	Sheet: 2 of 3	Rev: 0

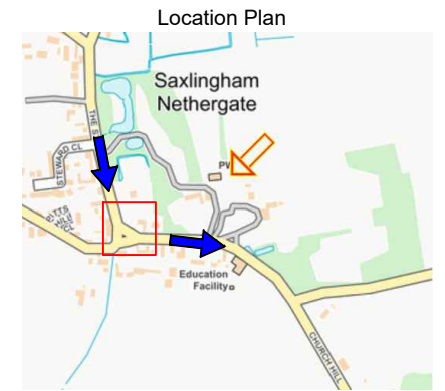
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Swept Path Assessment
 Indicative of 5-Axle bed 5-Axle Trailer
 Constructed from Client Supplied Data and OS Mastermap Data
 Scale 1:1500

NOTE: Overlay onto aerial image is not representative of the configuration relative to the environment. This is for illustrative purposes only, and should only be taken as such.



Legend:


-  5-axle bed 5-axle Trailer minimum turning arrangements Drawing ref. 24-1275.TC03
-  Extent of tractor track
-  Extent of trailer track
-  Extent of oversail
-  Extent of road boundary
-  Extent of property boundary
-  Overrun and oversail beyond kerb
-  Overrun beyond kerb
-  Oversail beyond kerb



1		
0	04.09.25	Issued for comment
Rev.	Date	Amendments

Revisions

Prepared by:
 **WYNNS** ENGINEERS
 Shaftesbury House, 2 High Street,
 Eccleshall, Stafford, ST21 6BZ
 Tel: (01785) 850411
 Independent Transportation Engineers

Client:
 **Island GREEN POWER**

Project: **East Pye**

Title: **Swept Path Assessments**
 Showing the configuration from approximate OS grid reference: TM 22975 97132, to approximate OS grid reference: TM 23204 97067, on indicative 5-axle bed 5-axle trailer transporting a 100te transformer.

Drawing status: **Final Report**

Scale (A3): As shown	Drawn by: MTO	Checked by: PW
Dwg. no: 24-1221.SPA08	Sheet: 3 of 3	Rev: 0

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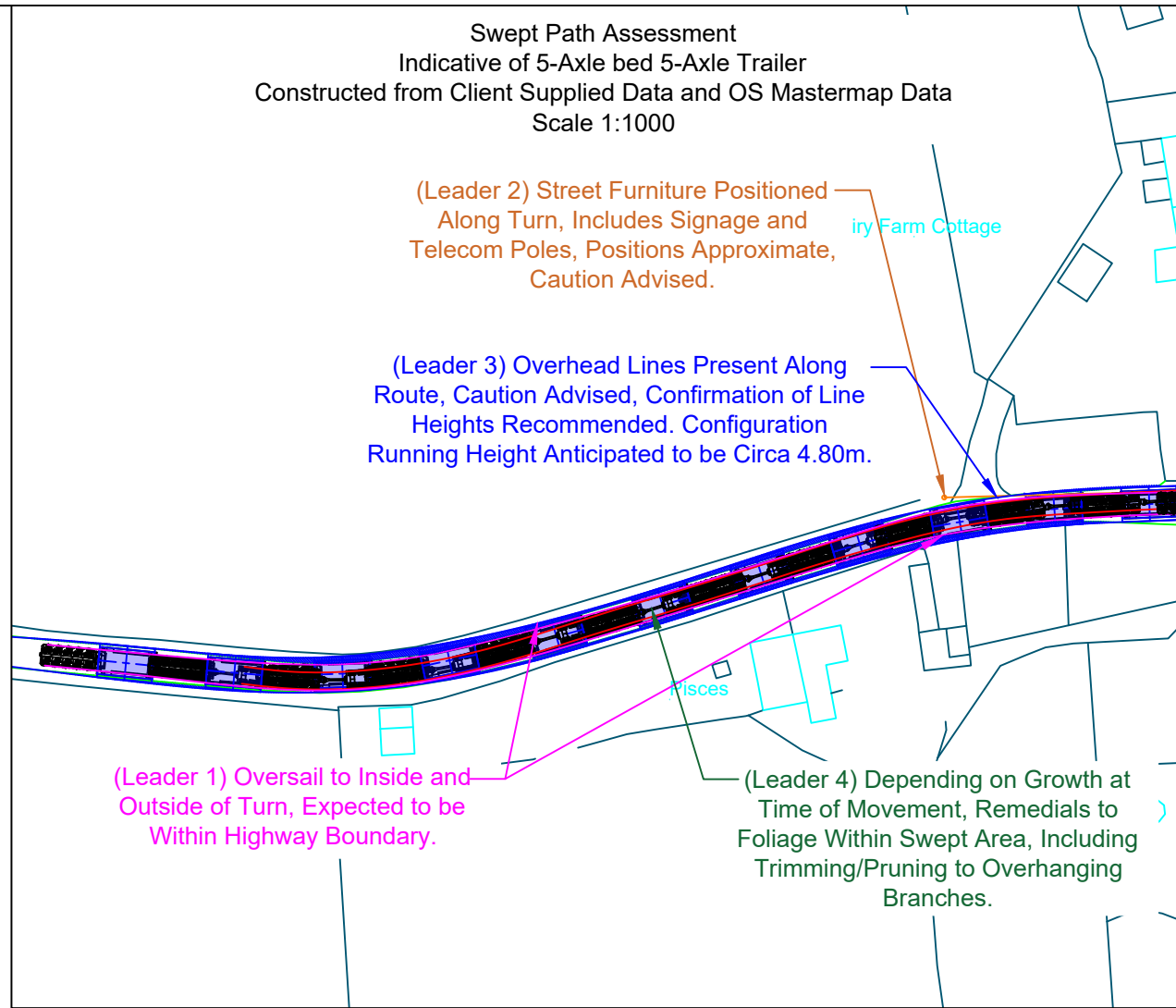


Swept Path Assessment
Indicative of 5-Axle bed 5-Axle Trailer
Constructed from Client Supplied Data and OS Mastermap Data
Scale 1:500

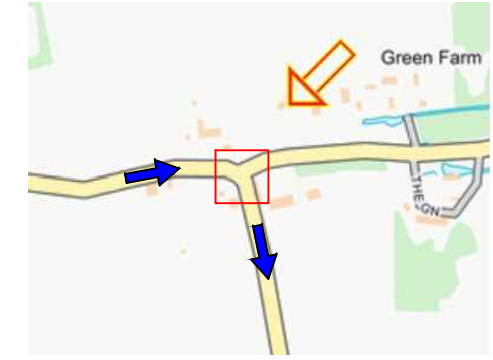
The delivery vehicle can be seen turning right along The Green/Broaden Lane, at approximate OS grid reference: TM 23787 96603.

The configuration is recommended to have full occupation of the available road way to aid in reducing any oversail and overrun that may occur. The configuration is expected to oversail on the inside and outside of the turn, oversail is expected to remain within the highway boundary (Leader 1). It should be noted that street furniture is positioned on the inside and outside of the turn, this includes signage and telecom poles, conflict is not expected along this section, though positions are approximate and caution is advised (Leader 2). It should be noted that overhead lines are present, confirmation of overhead line height is recommended, configuration anticipates a running height of circa 4.80m, however, this may be reduced using trailer hydraulics (Leader 3). Foliage is present along the road way, it is expected that remedial works including trimming and pruning would be required to hedges/branches, the extent of which would be dependant on growth at the time of movement (Leader 4). Subject to the implementation of the recommended remedial works, and observing the stated cautions, this section is considered to be physically negotiable.

Swept Path Assessment
Indicative of 5-Axle bed 5-Axle Trailer
Constructed from Client Supplied Data and OS Mastermap Data
Scale 1:1000



Location Plan



Legend:

- 5-axle bed 5-axle Trailer minimum turning arrangements Drawing ref. 24-1275.TC03
- Extent of tractor track
- Extent of trailer track
- Extent of oversail
- Extent of road boundary
- Extent of property boundary
- Overrun and oversail beyond kerb
- Overrun beyond kerb
- Oversail beyond kerb

1		
0	05.09.25	Issued for comment
Rev.	Date	Amendments

Prepared by:
 Shaftesbury House, 2 High Street, Eccleshall, Stafford, ST21 6BZ Tel: (01785) 850411
Independent Transportation Engineers

Client:

Project: East Pye

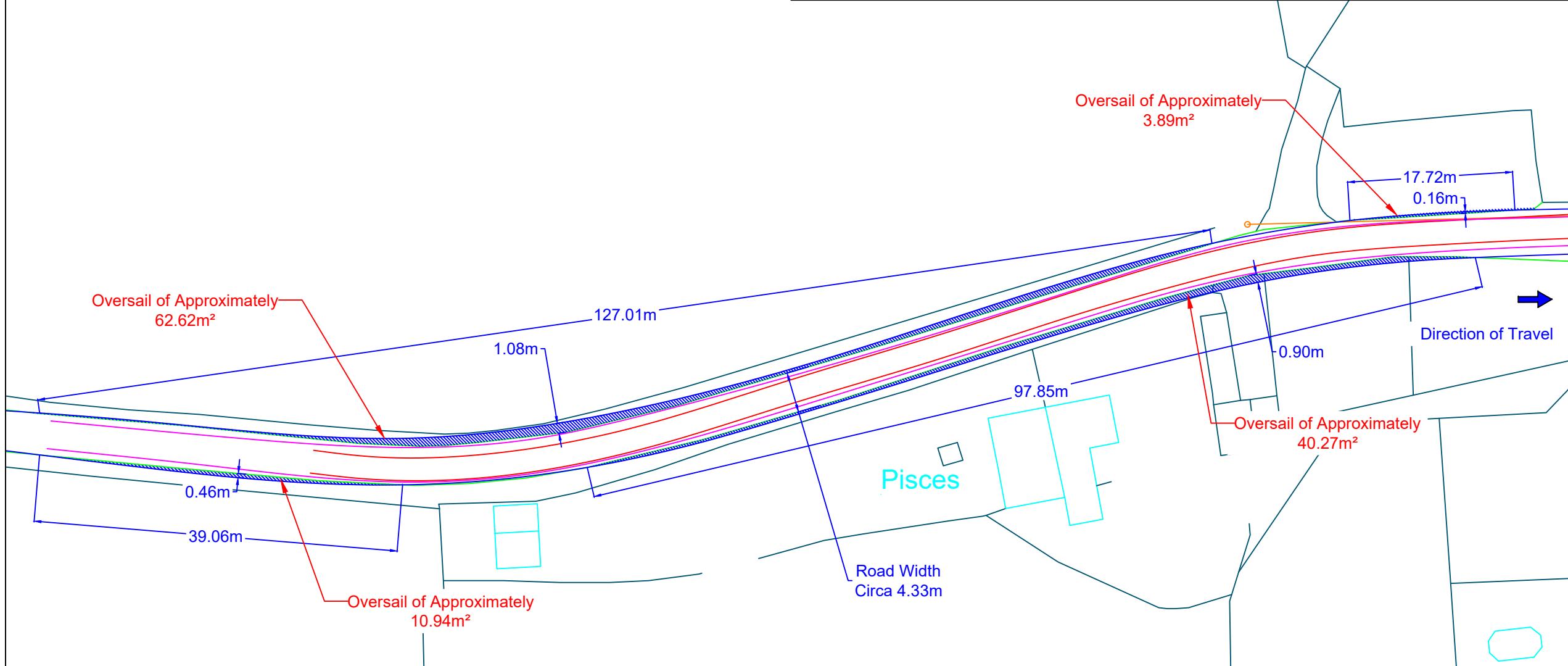
Title: Swept Path Assessments
Showing the right turn along The Green/Broaden Lane, at approximate OS grid reference: TM 23787 96603, on indicative 5-axle bed 5-axle trailer transporting a 100t transformer.

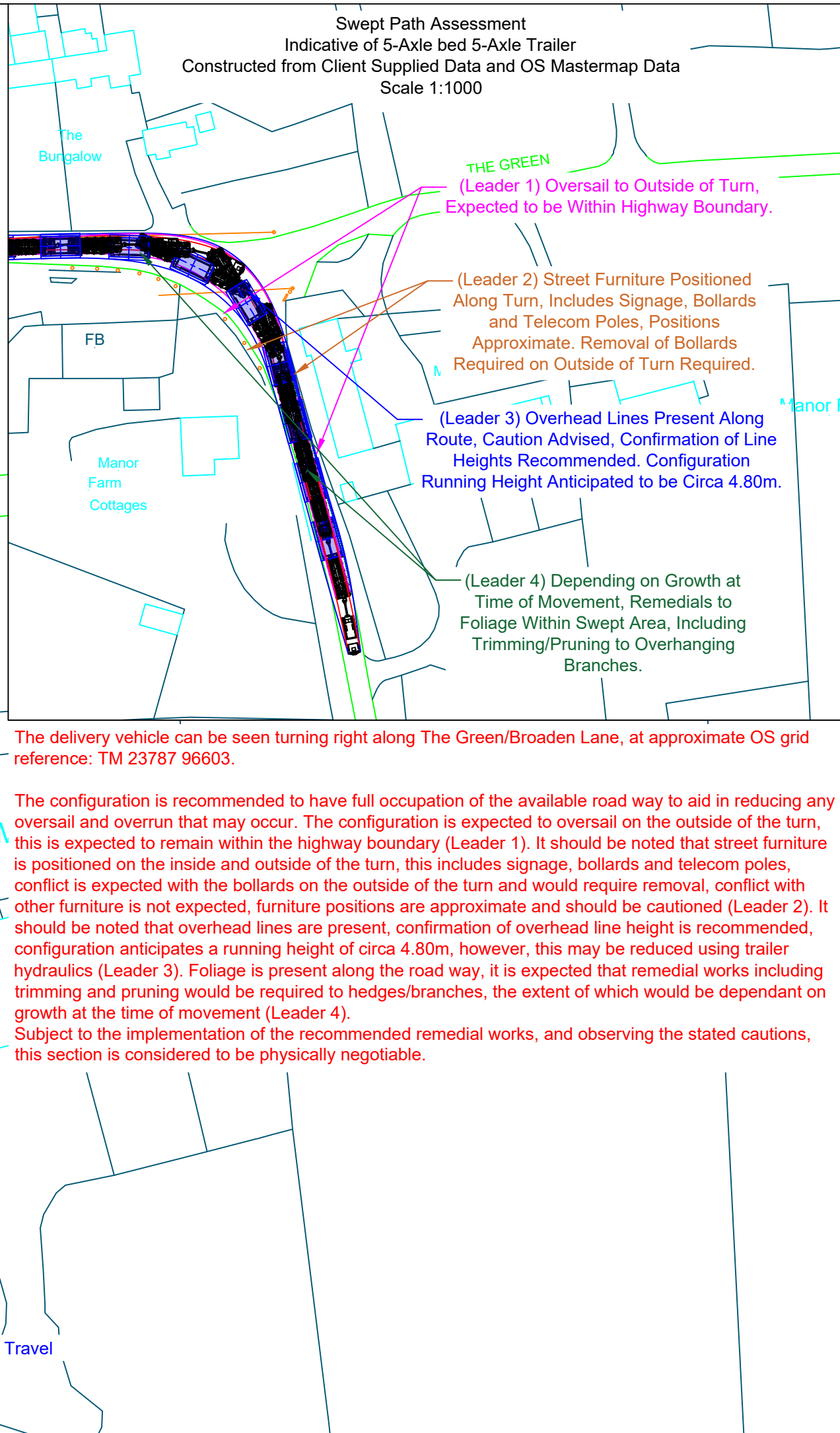
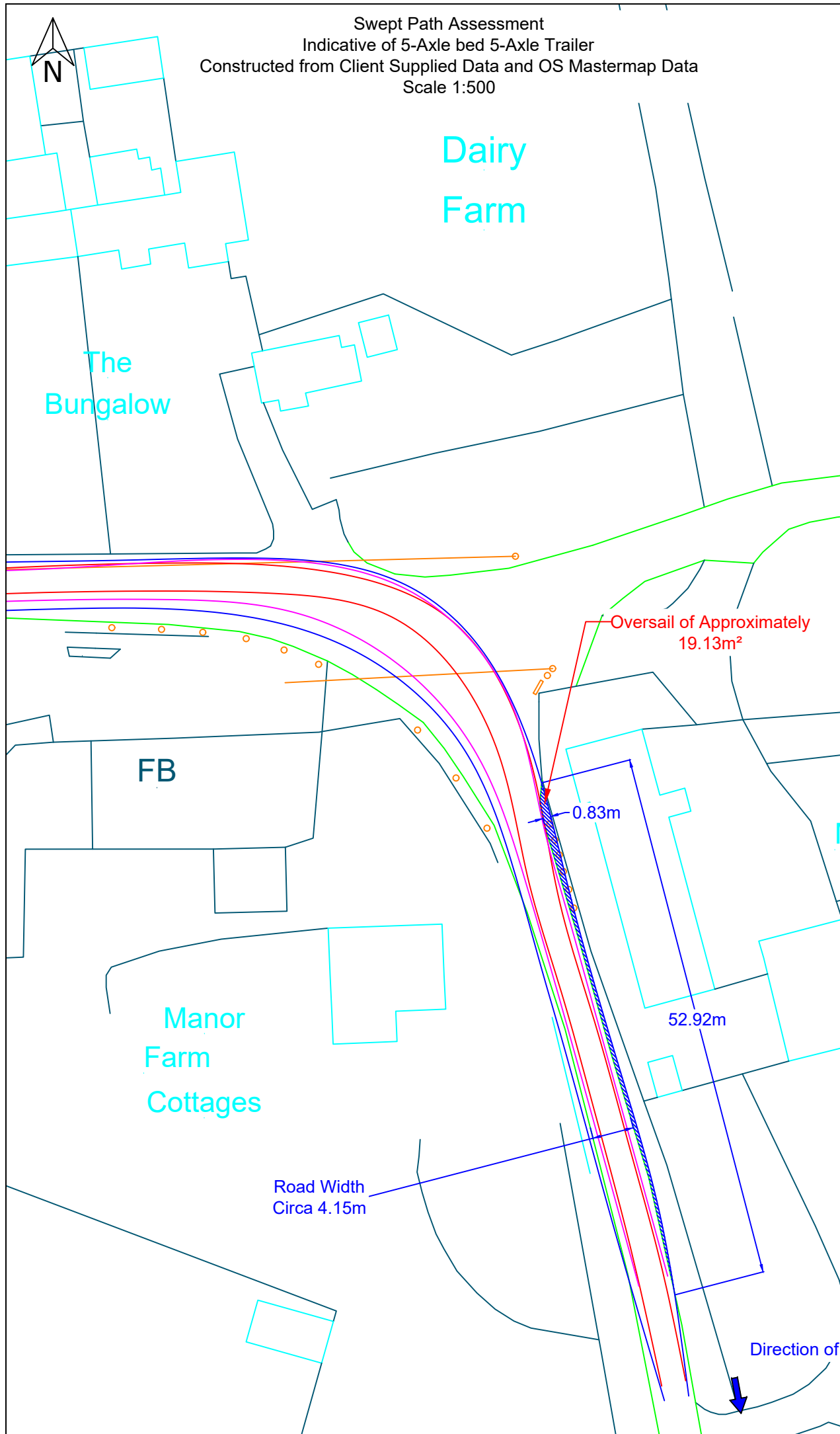
Drawing status: Final Report

Scale (A3): As shown	Drawn by: MTO	Checked by: PW
Dwg. no: 24-1221.SPA09	Sheet: 1 of 3	Rev: 0

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Location Plan

Legend:

- 5-axle bed 5-axle Trailer minimum turning arrangements Drawing ref. 24-1275.TC03
- Extent of tractor track
- Extent of trailer track
- Extent of oversail
- Extent of road boundary
- Extent of property boundary
- Overrun and oversail beyond kerb
- Overrun beyond kerb
- Oversail beyond kerb

The delivery vehicle can be seen turning right along The Green/Broaden Lane, at approximate OS grid reference: TM 23787 96603.

The configuration is recommended to have full occupation of the available road way to aid in reducing any oversail and overrun that may occur. The configuration is expected to oversail on the outside of the turn, this is expected to remain within the highway boundary (Leader 1). It should be noted that street furniture is positioned on the inside and outside of the turn, this includes signage, bollards and telecom poles, conflict is expected with the bollards on the outside of the turn and would require removal, conflict with other furniture is not expected, furniture positions are approximate and should be cautioned (Leader 2). It should be noted that overhead lines are present, confirmation of overhead line height is recommended, configuration anticipates a running height of circa 4.80m, however, this may be reduced using trailer hydraulics (Leader 3). Foliage is present along the road way, it is expected that remedial works including trimming and pruning would be required to hedges/branches, the extent of which would be dependant on growth at the time of movement (Leader 4). Subject to the implementation of the recommended remedial works, and observing the stated cautions, this section is considered to be physically negotiable.

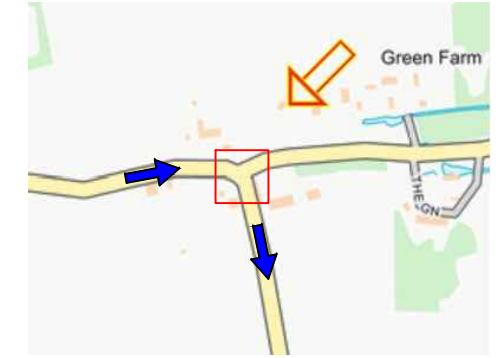
1		
0	05.09.25	Issued for comment
Rev.	Date	Amendments
Revisions		
Prepared by:		
Shaftesbury House, 2 High Street, Eccleshall, Stafford, ST21 6BZ Tel: (01785) 850411 Independent Transportation Engineers		
Client:		
Project: East Pye		
Title: Swept Path Assessments Showing the right turn along The Green/Broaden Lane, at approximate OS grid reference: TM 23787 96603, on indicative 5-axle bed 5-axle trailer transporting a 100t transformer.		
Drawing status: Final Report		
Scale (A3): As shown	Drawn by: MTO	Checked by: PW
Dwg. no: 24-1221.SPA09	Sheet: 2 of 3	Rev: 0
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P:\Clients\Existing Clients\Island Green Power\24-1275 East Pye BESS Norfolk\Swept Path Assessments		






Swept Path Assessment
 Indicative of 5-Axle bed 5-Axle Trailer
 Constructed from Client Supplied Data and OS Mastermap Data
 Scale 1:1000

NOTE: Overlay onto aerial image is not representative of the configuration relative to the environment. This is for illustrative purposes only, and should only be taken as such.

Location Plan



Legend:

-  5-axle bed 5-axle Trailer
minimum turning arrangements
Drawing ref. 24-1275.TC03
-  Extent of tractor track
-  Extent of trailer track
-  Extent of oversail
-  Extent of road boundary
-  Extent of property boundary
-  Overrun and oversail beyond kerb
-  Overrun beyond kerb
-  Oversail beyond kerb



1		
0	05.09.25	Issued for comment
Rev.	Date	Amendments

Revisions

Prepared by:

 Independent Transportation Engineers
 Shaftesbury House, 2 High Street,
 Eccleshall, Stafford, ST21 6BZ
 Tel: (01785) 850411

Client:


Project: East Pye

Title: Swept Path Assessments
 Showing the right turn along The Green/Broaden Lane, at
 approximate OS grid reference: TM 23787 96603, on
 indicative 5-axle bed 5-axle trailer transporting a 100te
 transformer.

Drawing status: Final Report

Scale (A3): As shown	Drawn by: MTO	Checked by: PW
Dwg. no: 24-1221.SPA09	Sheet: 3 of 3	Rev: 0

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3.4. East Pye Site 10B & 10C

Site	East Pye Solar Site 10B & 10C
Route Inspection and AIL Access Report Recently undertaken by Wynns?	Yes
Has Agreement in Principle (AIP) been provided by National Highways in line with the Water Preferred Policy	Not applicable as 95te nett transformer will be moved within STGO Category 3 and as such will not require Special Order permissions from National Highways.
National Highways AIP Reference Number	N/A
Proposed port/Marine access point of Delivery	Port of Immingham The port of Immingham is well established for heavy project cargo and no issues are expected in respect to marine access. It should be noted that as the load is STGO it will not be specifically limited to Immingham as the closest port but Immingham does provide suitable facilities.
Transformer Transport Weight considered during the most recent report in line with future project requirements	Weight - 95te nett transformer Length - 7.90m Width - 4.86m Height - 4.50m
Typical trailer used in Route Clearance works	A 5 bed 5 trailer at 146te gross weight as shown in drawing number 24-1275.TC03
Expected delivery date of next planned transformer if known	To be confirmed
Last Recorded Special Order Movement (according to available records)	No movements to this site as is a new development
Suggested route based on historical information	Take A47 exit onto A146 (OS Grid Ref: TG 24912 06173) Turn right Loddon Rd Turn right B1332 Bungay Rd Turn left Harvey's Ln Continue to site (OS Grid Ref: TM 29839 95957)
Is a map available of the proposed route(s)?	Yes - See Attachment 7
Any Known Problems for AIL Access in terms of structures?	No - As the gross vehicle weight of the drawbar trailer is less than 150Te the vehicle will be considered as a STGO CAT3 movement. This requires that the movement is notified through the National Highways ESDAL platform with 5 working days notice. If no rejections are received within this window, the movement has permission to travel. Notification WYNL/187 was transmitted on 10/12/24 via

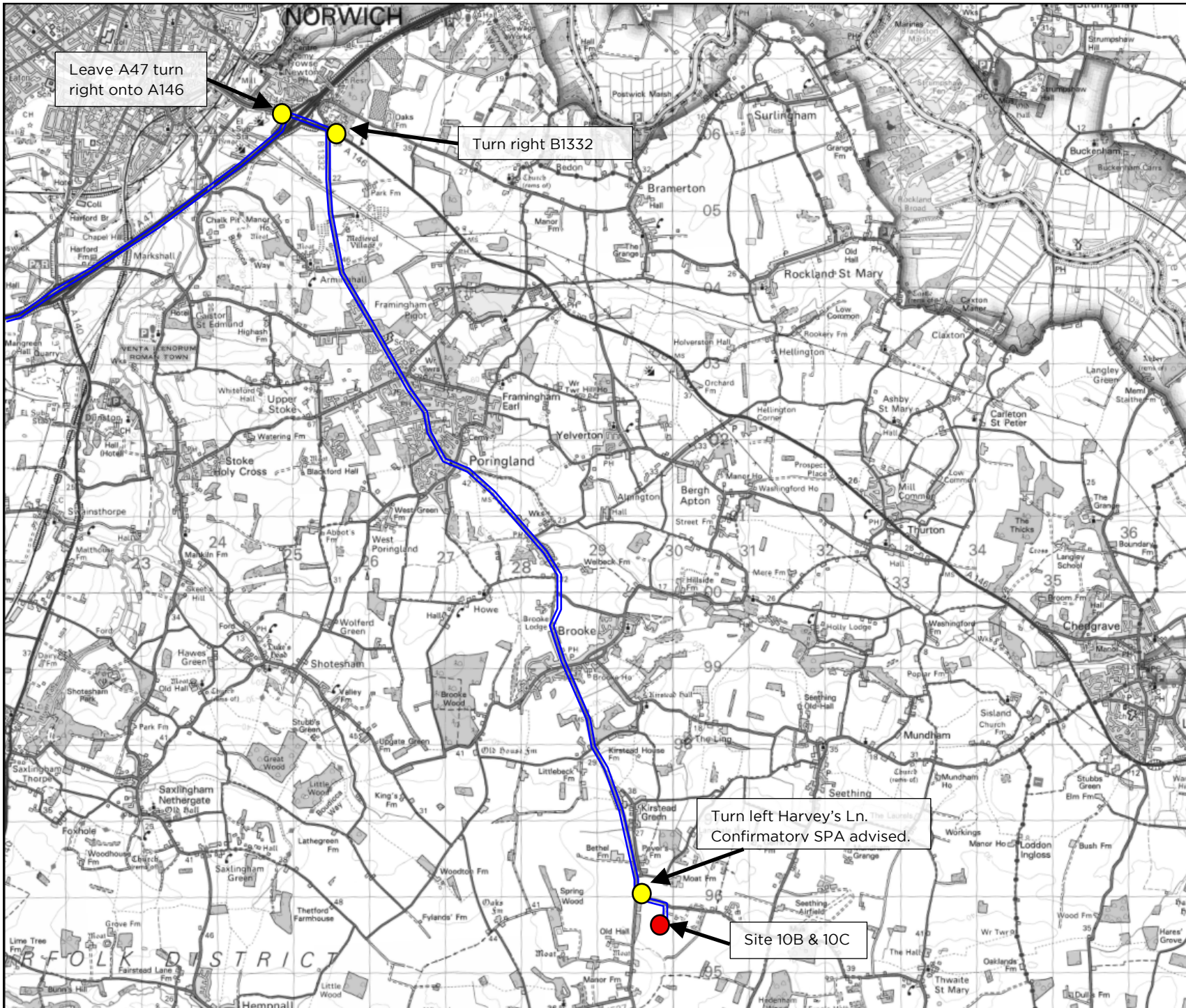


Site	East Pye Solar Site 10B & 10C
	the ESDAL platform to which no rejections were received. Had a haulier made this same application, they would have therefore received the required permission to travel.
Authorities consulted in respect to AIL Access	<ul style="list-style-type: none"> • A1(M) Alconbury to Peterborough DBFO • Bedfordshire/Hertfordshire/Cambridgeshire Constabulary • Humberside Police • Leicestershire Police • Lincolnshire County Council • Lincolnshire Police • National Highways Area 7 • National Highways East Region • National Highways Yorkshire & North East Region • Network Rail LC & Rail over Road • Norfolk County Council Abnormal Load Service • Norfolk and Suffolk Constabulary • North Lincolnshire Council • Northamptonshire Police • Nottinghamshire Police • PRAX Lindsey Oil Refinery • Phillips 66 Oil Refinery
Any Known Problems for AIL Access in terms of Negotiability and other Route Comments?	<p>Confirmatory swept path assessment advised for LH turn from B1332 onto to Harvey's Ln.</p> <p>Full road possession is anticipated for the negotiation of Harvey's Ln. Police escort can halt oncoming traffic to ensure safe passage.</p>
Any Known Problems for AIL Access in terms of Onsite issues?	N/A - Proposal Stage
Do routing issues currently present a serious risk that access to the site may be restricted?	No
Any other Relevant Information and Notes: N/A	



Attachment 7

Map



Key		
	Proposed Routes	
	Points of Interest	
	East Pye Substation Location	
B		
A		
O	20.02.26	First Issue
Rev	Date	Amendments:
Revisions		
		Wynns Ltd. Independent Transportation Engineers
Shaftesbury House, 2 High Street, Eccleshall, Stafford, ST21 6BZ. Tel: (01785) 850411		
Client:		
Project:	East Pye Solar	
Title:	Site 10B & 10C Map	
Drawing Status:	Summary Report	
Scale (A4):	Drawn by:	Checked by:
NTS	SMB	ARP
Ref No.:	Sheet:	Rev.:
24-1275-Map1	1 of 1	0
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3.5. East Pye BESS Site

Site	East Pye Solar BESS Site
Route Inspection and AIL Access Report Recently undertaken by Wynns?	Yes
Has Agreement in Principle (AIP) been provided by National Highways in line with the Water Preferred Policy	<p>Not applicable for 95te nett as transformer will be moved within STGO Category 3 and as such will not require Special Order permissions from National Highways.</p> <p>For 155te nett transformer an AIP application will be made when route clearance works are complete. This is anticipated to be either Port of Kings Lynn or Port of Ipswich.</p>
National Highways AIP Reference Number	N/A
Proposed port/Marine access point of Delivery	<p>Port of Immingham (For 95te Transformer Only) The port of Immingham is well established for heavy project cargo and no issues are expected in respect to marine access. It should be noted that as the load is STGO it will not be specifically limited to Immingham as the closest port but Immingham does provide suitable facilities.</p> <p>Port of Kings Lynn The port of Kings Lynn is also a well established port and has been used recently for access to National Grid Norwich Main Substation AIL project cargo. In early 2026 Port of Kings Lynn received heavy project cargo for the Hornsea 3 project.</p> <p>Port of Ipswich The port of Ipswich East Quay is a well established for AIL project cargo and has most recently seen the delivery of large synchronous condenser to the National Grid Yaxley project. The onward route requires the traversal of Orwell Bridge which is possible for movements of this size under a full road closure whilst the transport traverses.</p> <p>ABP Heavy Lift Policy Both Port of Kings Lynn and Port of Ipswich are owned and operated by Associated British Ports. Therefore, the ABP Heavy Lift Policy must be adhered to.</p>
Transformer Transport Weight considered during the most recent report in line with future project requirements	<p>Weight - 95te nett transformer Length - 7.90m Width - 4.86m Height - 4.50m</p> <p>Weight - 155te nett transformer</p>



Site	East Pye Solar BESS Site
	Length - 7.24m Width - 5.00m Height - 4.77m
Typical trailer used in Route Clearance works	95te Tx - A 5 bed 5 trailer at 146te gross weight as shown in drawing number 24-1275.TC03 A 16 axle girder frame trailer at 249te gross weight as shown in drawing number 24-1275.TC04
Expected delivery date of next planned transformer if known	To be confirmed
Last Recorded Special Order Movement (according to available records)	No movements to this site as is a new development
Suggested route based on historical information	Take A47 exit onto A146 (OS Grid Ref: TG 24912 06173) Turn right Loddon Rd Turn right B1332 Bungay Rd Turn right B1527 Church Rd Turn left Alburgh Rd Continue to site (OS Grid Ref: TM 25018 92230)
Is a map available of the proposed route(s)?	Yes - See Attachment 8
Any Known Problems for AIL Access in terms of structures?	Yes - A140 Dunston Railway Bridge (OS Grid Ref: TG 22321 02853) is currently under assessment to confirm structural AIL loading capacity. Dunston Railway Bridge is responsibility of Norfolk County Council. Should A140 Dunston Rail Bridge assessment provide a negative result, further investigation will be required in order to gain access from direction of Ipswich. There is however the need for the overbridging of A140 Brockford Street Bridge. Detailed discussions with Suffolk County Council and Norfolk and Suffolk Constabulary resulted in objection of overbridging due to the associated disruption caused.
Authorities consulted in respect to AIL Access	<ul style="list-style-type: none"> • A1(M) Alconbury to Peterborough DBFO • Bedfordshire/Hertfordshire/Cambridgeshire Constabulary • Humberside Police • Leicestershire Police • Lincolnshire County Council • Lincolnshire Police • National Highways Area 7 • National Highways East Region

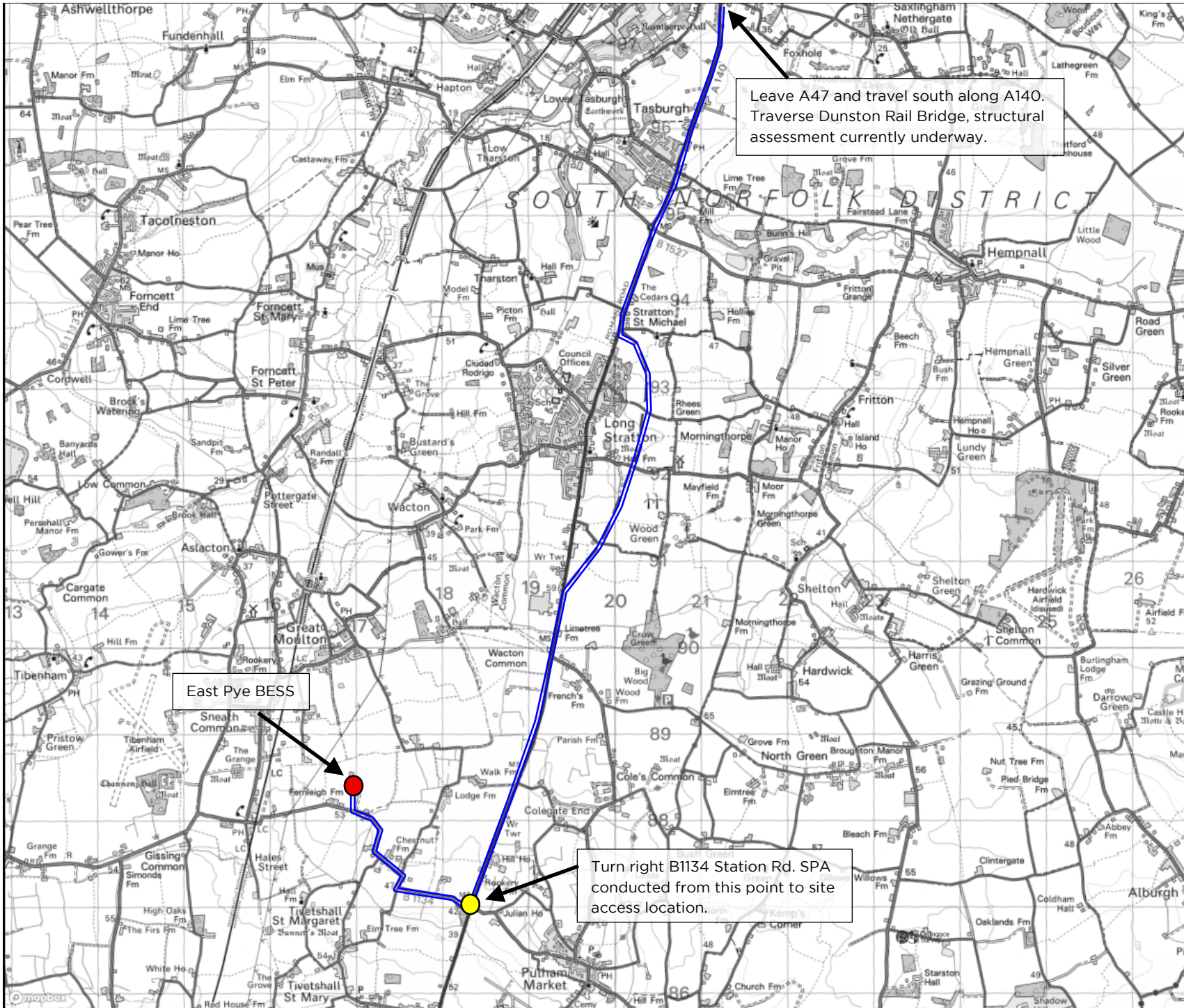


Site	East Pye Solar BESS Site
	<ul style="list-style-type: none"> • National Highways Yorkshire & North East Region • Network Rail LC & Rail over Road • Norfolk County Council Abnormal Load Service • Norfolk and Suffolk Constabulary • North Lincolnshire Council • Northamptonshire Police • Nottinghamshire Police • PRAX Lindsey Oil Refinery • Phillips 66 Oil Refinery
<p>Any Known Problems for AIL Access in terms of Negotiability and other Route Comments?</p>	<p>Swept path assessments have been conducted to confirm negotiability. See swept path assessment 24-1275.SP03 appended as Attachment 9. Vehicle is expected to travel outside of the public highway boundary but remain within the red line boundary.</p> <p>Full road possession is anticipated for the negotiation of B1134 Station Rd to site location. Police escort can halt oncoming traffic to ensure safe passage.</p>
<p>Any Known Problems for AIL Access in terms of Onsite issues?</p>	<p>N/A - Proposal Stage</p>
<p>Do routing issues currently present a serious risk that access to the site may be restricted?</p>	<p>No</p>
<p>Any other Relevant Information and Notes: Physical route survey was conducted before the opening to traffic of Long Stratton Bypass. Design information received and understood to be negotiable by transport configurations considered within this report. It is advisable for the new bypass road to be surveyed for confirmatory purposes before movement of AIL vehicle takes place.</p> <p>Swept path assessment number 24-1275.SPA03 was produced before the route from Ipswich was objected by Suffolk County Council and Constabulary. Page 1 shows route traveling northbound and negotiating LH turn onto Station Rd. There are no negotiation concerns should the vehicle approach from the North and turn right onto Station Rd by traveling around roundabout in contraflow.</p>	






Attachment 8

Map



Key

	Proposed Routes
	Points of Interest
	East Pye Substation Site

B		
A		
O	20.02.26	First Issue
Rev	Date	Amendments:

Revisions



Wynns Ltd.
Independent
Transportation
Engineers

Shaftesbury House, 2 High Street, Eccleshall,
Stafford, ST21 6BZ. Tel: (01785) 850411

Client:	
---------	---

Project:	East Pye Solar
----------	----------------

Title:	BESS Site Map
--------	---------------

Drawing Status:	Summary Report
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Scale (A4):	Drawn by:	Checked by:
NTS	SMB	ARP

Ref No.:	Sheet:	Rev.:
24-1275-Map1	1 of 1	0

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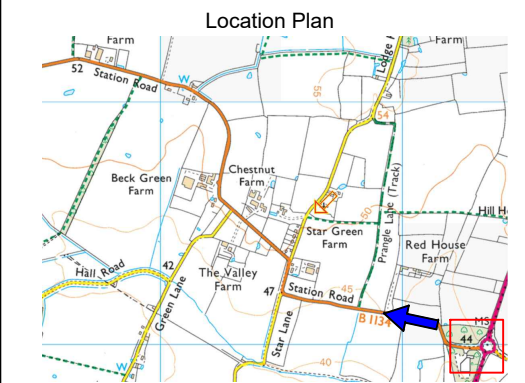


Attachment 9

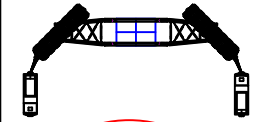







Swept Path Assessment



Swept Path Assessment
 Indicative of 16 Axle Girder Frame Trailer
 Constructed from Client Supplied Data and OS Mastermap Data
 Scale 1:500



Legend:

-  16-axle Girder Frame Trailer
minimum turning arrangements
Drawing ref. 24-1275.TC04
-  Extent of vehicle track
-  Extent of oversail
-  Extent of road boundary
-  Extent of property boundary
-  Overrun and oversail beyond kerb
-  Overrun beyond kerb
-  Oversail beyond kerb

Direction of Travel



STATION ROAD

Oversail of Approximately
7.83m²

Oversail of Approximately
98.23m²

0.38m
29.70m

2.60m

41.52m

Swept Path Assessment
 Indicative of 16 Axle Girder Frame Trailer
 Constructed from Client Supplied Data and OS Mastermap Data
 Scale 1:1000

(Leader 1) Oversail to Inside of
Turn, Remedials Required,
Within Highway Boundary.

STATION ROAD

(Leader 2) Street Furniture
Positioned Along Turn, Includes
Signage, Removal Required to
Facilitate.

(Leader 3) Depending on
Growth at Time of Movement,
Remedials to Foliage Within
Swept Area Possible.


The delivery vehicle can be seen turning left at the island from Long Stratton Bypass/Norwich Road onto B1134 Station Road, at approximate OS grid reference: TM 18309 86988.

The configuration is recommended to occupy the full available road way to aid in reducing oversail/overrun where possible. The configuration anticipates oversail on the inside of the turn, this is expected to remain within the highway boundary (Leader 1). It should be noted that street furniture is positioned on the inside of the turn, this includes signage, this would require removal to facilitate (Leader 2). Additionally, depending on growth at the time of movement, trimming to foliage within the swept area may be required to facilitate delivery (Leader 3). Subject to observing the stated cautions and implementing the required remedial works, this section is considered to be physically negotiable.

1		
0	11.08.25	Issued for comment
Rev.	Date	Amendments

Revisions

Prepared by:



Shaftesbury House, 2 High Street,
Eccleshall, Stafford, ST21 6BZ
Tel: (01785) 850411

Independent Transportation Engineers

Client:



Project: East Pye

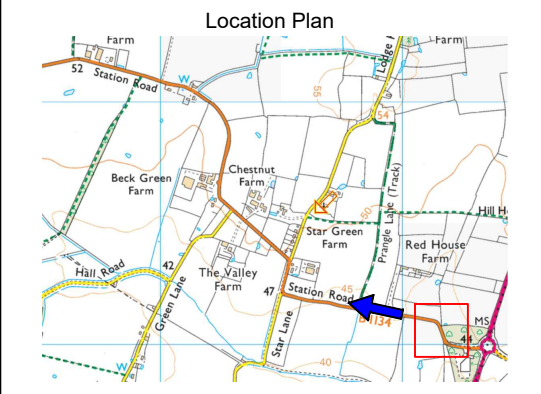
Title: Swept Path Assessments
 Showing the left turn from Long Stratton Bypass/Norwich Road onto B1134 Station Road, at approximate OS grid reference: TM 18309 86988, on indicative 16 axle girder trailer transporting a 157t transformer.

Drawing status: Final Report

Scale (A3): As shown	Drawn by: MTO	Checked by: PW
Dwg. no: 24-1221.SPA03	Sheet: 1 of 12	Rev: 0

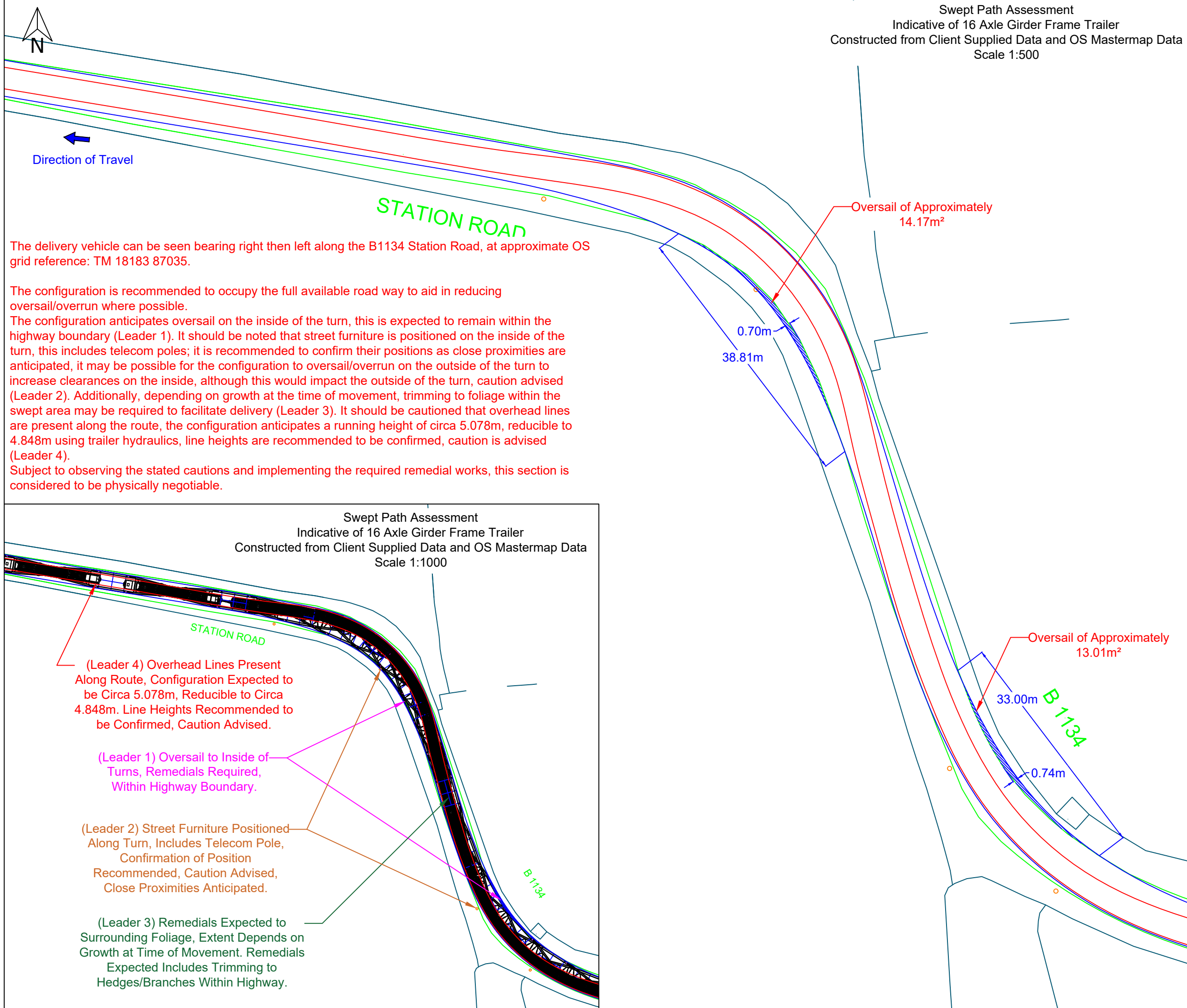
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 P:\Clients\Existing Clients\Island Green Power\24-1275 East Pye BESS Norfolk\Swept Path Assessments

Swept Path Assessment
 Indicative of 16 Axle Girder Frame Trailer
 Constructed from Client Supplied Data and OS Mastermap Data
 Scale 1:500



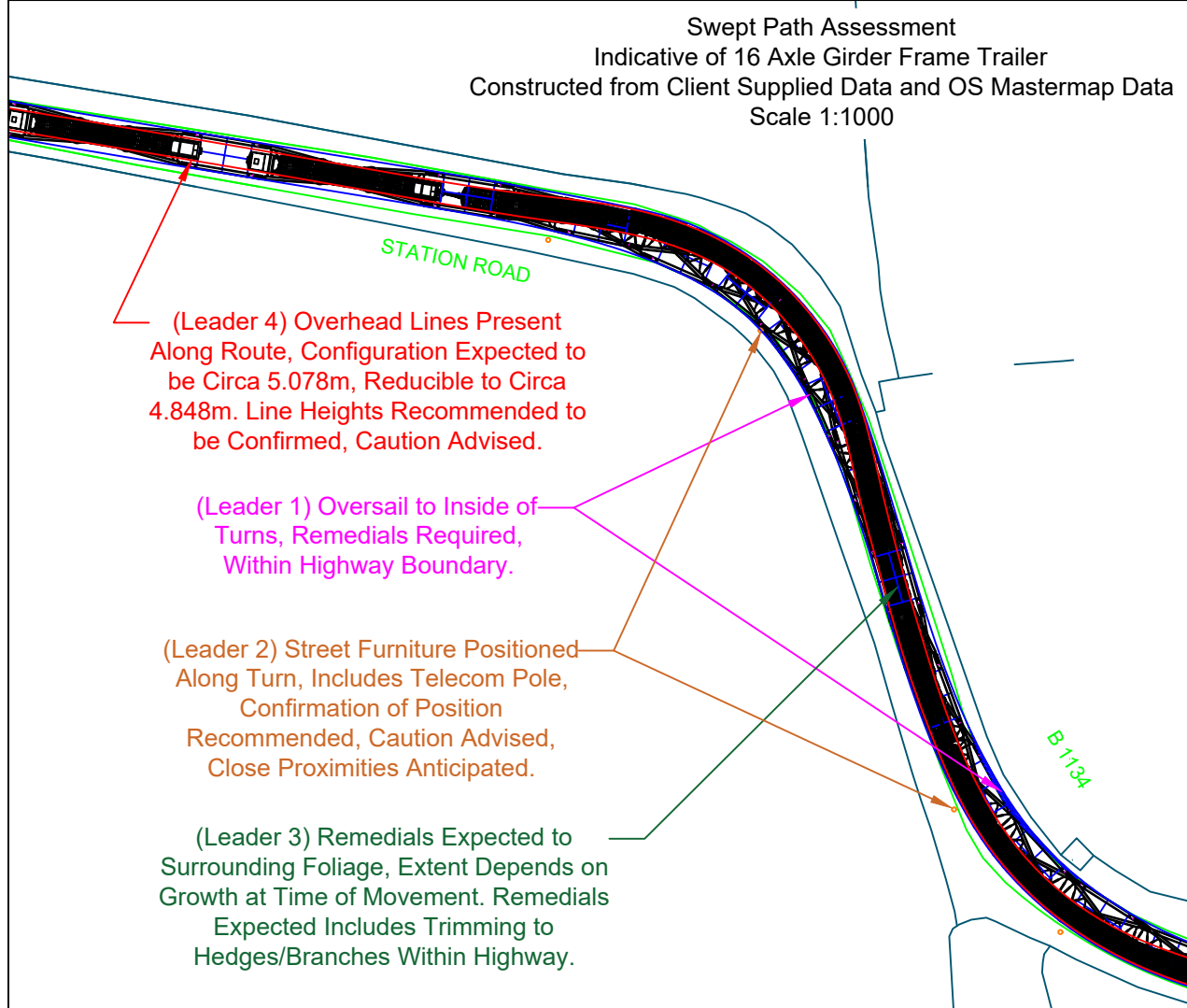
Legend:

- 16-axle Girder Frame Trailer minimum turning arrangements Drawing ref. 24-1275.TC04
- Extent of vehicle track
- Extent of oversail
- Extent of road boundary
- Extent of property boundary
- Overrun and oversail beyond kerb
- Overrun beyond kerb
- Oversail beyond kerb



The delivery vehicle can be seen bearing right then left along the B1134 Station Road, at approximate OS grid reference: TM 18183 87035.

The configuration is recommended to occupy the full available road way to aid in reducing oversail/overrun where possible.
 The configuration anticipates oversail on the inside of the turn, this is expected to remain within the highway boundary (Leader 1). It should be noted that street furniture is positioned on the inside of the turn, this includes telecom poles; it is recommended to confirm their positions as close proximities are anticipated, it may be possible for the configuration to oversail/overrun on the outside of the turn to increase clearances on the inside, although this would impact the outside of the turn, caution advised (Leader 2). Additionally, depending on growth at the time of movement, trimming to foliage within the swept area may be required to facilitate delivery (Leader 3). It should be cautioned that overhead lines are present along the route, the configuration anticipates a running height of circa 5.078m, reducible to 4.848m using trailer hydraulics, line heights are recommended to be confirmed, caution is advised (Leader 4).
 Subject to observing the stated cautions and implementing the required remedial works, this section is considered to be physically negotiable.



Swept Path Assessment
 Indicative of 16 Axle Girder Frame Trailer
 Constructed from Client Supplied Data and OS Mastermap Data
 Scale 1:1000

(Leader 4) Overhead Lines Present Along Route, Configuration Expected to be Circa 5.078m, Reducible to Circa 4.848m. Line Heights Recommended to be Confirmed, Caution Advised.

(Leader 1) Oversail to Inside of Turns, Remedials Required, Within Highway Boundary.

(Leader 2) Street Furniture Positioned Along Turn, Includes Telecom Pole, Confirmation of Position Recommended, Caution Advised, Close Proximities Anticipated.

(Leader 3) Remedials Expected to Surrounding Foliage, Extent Depends on Growth at Time of Movement. Remedials Expected Includes Trimming to Hedges/Branches Within Highway.

1		
0	11.08.25	Issued for comment
Rev.	Date	Amendments

Revisions

Prepared by:

Shaftesbury House, 2 High Street,
 Eccleshall, Stafford, ST21 6BZ
 Tel: (01785) 850411

Independent Transportation Engineers

Client:

Project: East Pye

Title: Swept Path Assessments
 Shown continuing along B1134 Station Road, at approximate OS grid reference: TM 18183 87035, on indicative 16 axle girder trailer transporting a 157te transformer.

Drawing status: Final Report

Scale (A3): As shown	Drawn by: MTO	Checked by: PW
Dwg. no: 24-1221.SPA03	Sheet: 2 of 12	Rev: 0

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 P:\Clients\Existing Clients\Island Green Power\24-1275 East Pye BESS Norfolk\Swept Path Assessments



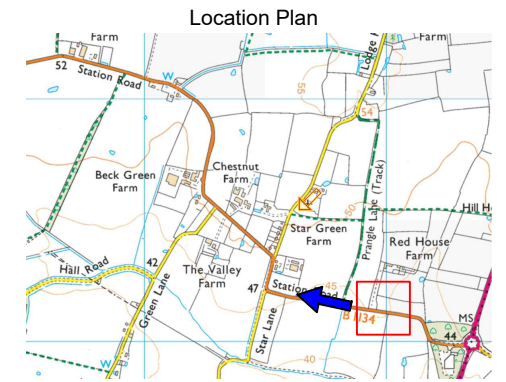
Direction of Travel



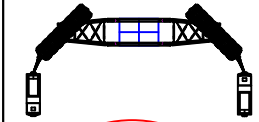







Swept Path Assessment
Indicative of 16 Axle Girder Frame Trailer
Constructed from Client Supplied Data and OS Mastermap Data
Scale 1:500

Road Width Offers
Circa 5.21m Clear Width.
Configuration Anticipates
Circa 5.0m Width.

Road Width Varies, Configuration Anticipates
Minimal Oversail Along Road Way When
Navigate Road Bends, Remedials May be
Required to Foliage/Street Furniture
Positioned Along the Road Way. .



Legend:

-  16-axle Girder Frame Trailer
minimum turning arrangements
Drawing ref. 24-1275.TC04
-  Extent of vehicle track
-  Extent of oversail
-  Extent of road boundary
-  Extent of property boundary
-  Overrun and oversail beyond kerb
-  Overrun beyond kerb
-  Oversail beyond kerb

Swept Path Assessment
Indicative of 16 Axle Girder Frame Trailer
Constructed from Client Supplied Data and OS Mastermap Data
Scale 1:1000

(Leader 1) Oversail May Occur
as Configuration Navigates
Bends/Road Widths Vary,
Expected to Remain Within
Highway Boundary.

B 1134

(Leader 2) Street Furniture
Positioned Along Turn, Includes
Signage, Removal Required to
Facilitate.

(Leader 3) Remedials Expected to
Surrounding Foliage, Extent Depends on
Growth at Time of Movement. Remedials
Expected Includes Trimming to
Hedges/Branches Within Highway.

The delivery vehicle can be seen continuing along the B1134 Station Road, at approximate OS grid reference: TM 18309 86988.

The configuration is recommended to occupy the full available road way to aid in reducing oversail/overrun where possible. The configuration is expected to remain within the proposed road way, however, as the road width varies and the configuration negotiates bends along the road, minimal oversail would be expected at points. The road way offering circa 5.21m clear width, and the configuration anticipated a running width of circa 5.0m no conflicts are anticipated along this section (Leader 1). It should be noted that street furniture is positioned along the roadway, this includes signage, conflict not anticipated but caution is advised (Leader 2). Additionally, depending on growth at the time of movement, trimming to foliage within the swept area may be required to facilitate delivery (Leader 3). Subject to observing the stated cautions and implementing the required remedial works, this section is considered to be physically negotiable.

1		
0	11.08.25	Issued for comment
Rev.	Date	Amendments

Revisions

Prepared by:



Shaftesbury House, 2 High Street,
Eccleshall, Stafford, ST21 6BZ
Tel: (01785) 850411

Independent Transportation Engineers

Client:



Project: East Pye

Title: Swept Path Assessments
Shown continuing along B1134 Station Road, at
approximate OS grid reference: TM 17931 87134, on
indicative 16 axle girder trailer transporting a 157te
transformer.

Drawing status: Final Report

Scale (A3): As shown	Drawn by: MTO	Checked by: PW
Dwg. no: 24-1221.SPA03	Sheet: 3 of 12	Rev: 0

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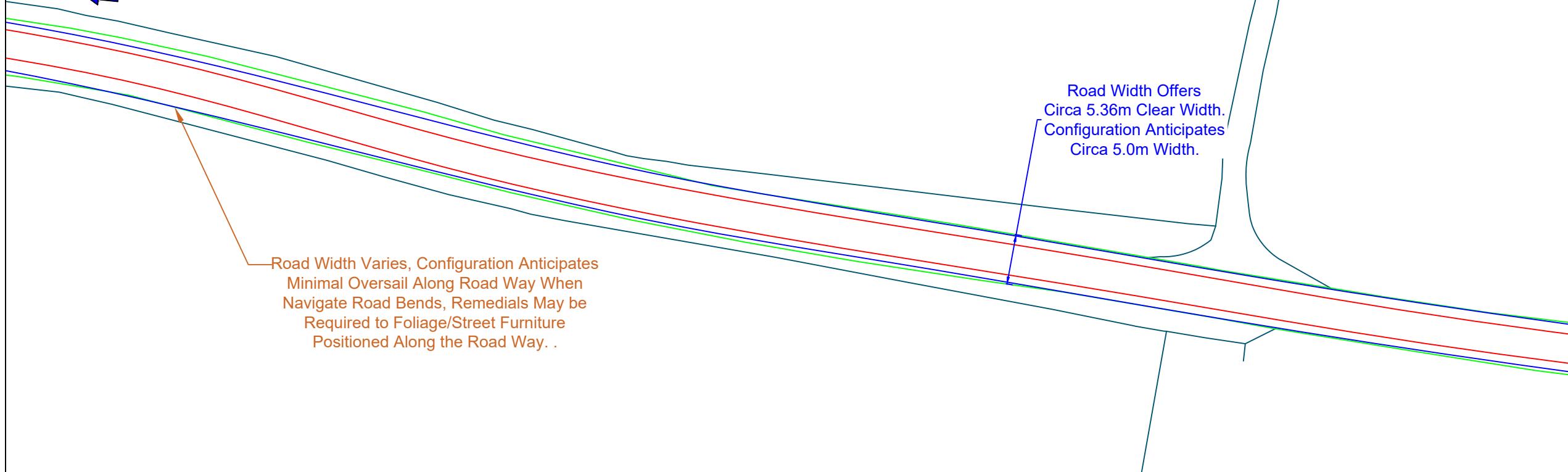
P:\Clients\Existing Clients\Island Green Power\24-1275 East Pye BESS Norfolk\Swept Path Assessments



Direction of Travel



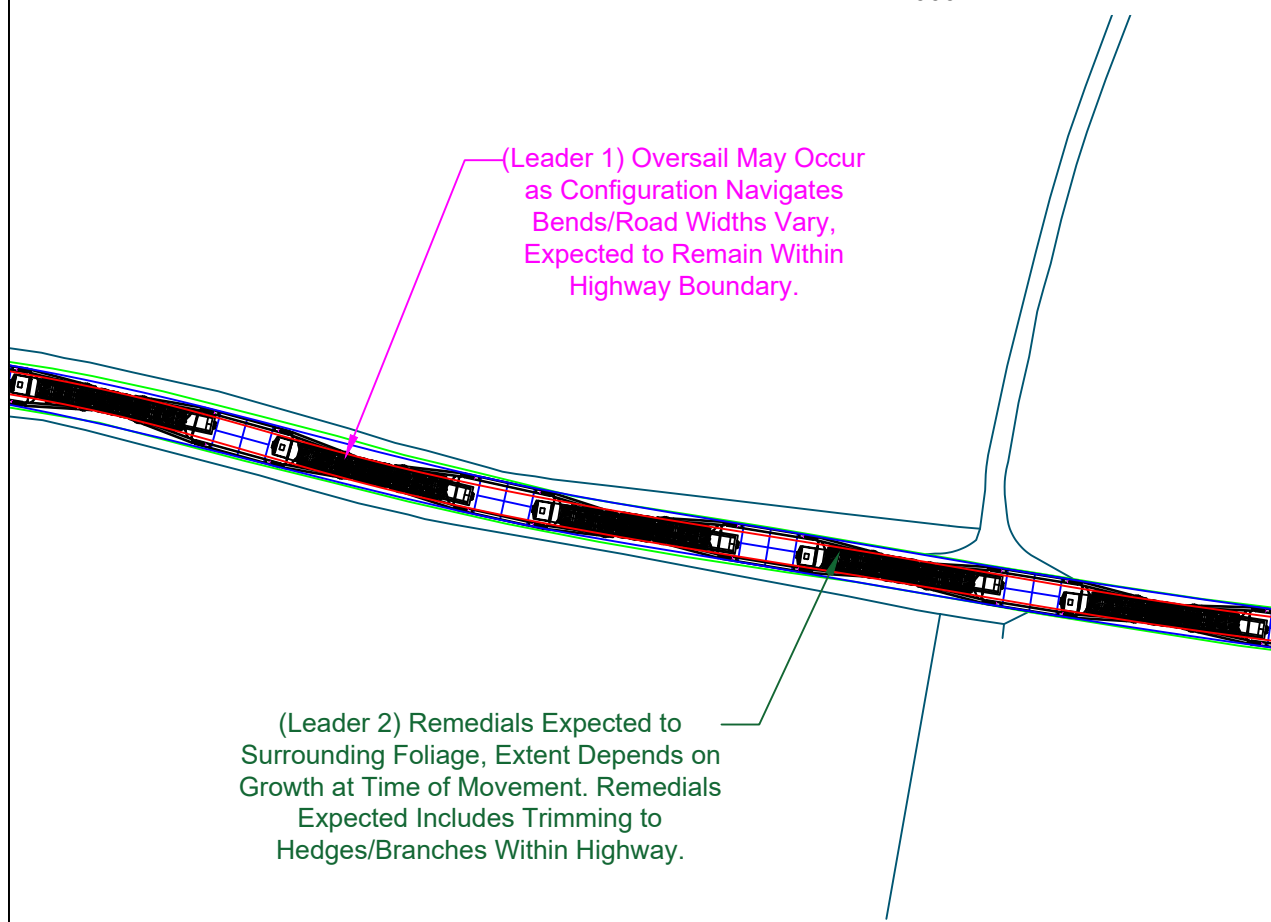
Swept Path Assessment
Indicative of 16 Axle Girder Frame Trailer
Constructed from Client Supplied Data and OS Mastermap Data
Scale 1:500



Road Width Varies, Configuration Anticipates Minimal Oversail Along Road Way When Navigate Road Bends, Remedials May be Required to Foliage/Street Furniture Positioned Along the Road Way. .

Road Width Offers Circa 5.36m Clear Width. Configuration Anticipates Circa 5.0m Width.

Swept Path Assessment
Indicative of 16 Axle Girder Frame Trailer
Constructed from Client Supplied Data and OS Mastermap Data
Scale 1:1000



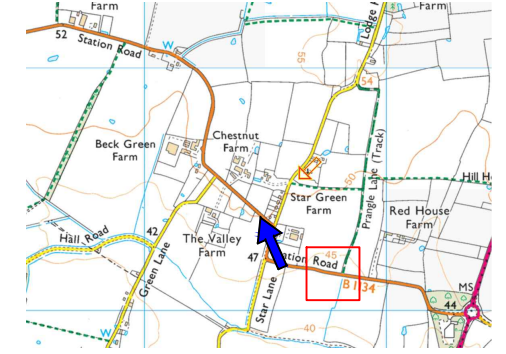
(Leader 1) Oversail May Occur as Configuration Navigates Bends/Road Widths Vary, Expected to Remain Within Highway Boundary.

(Leader 2) Remedials Expected to Surrounding Foliage, Extent Depends on Growth at Time of Movement. Remedials Expected Includes Trimming to Hedges/Branches Within Highway.

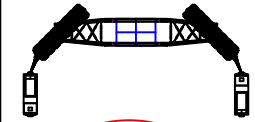







The delivery vehicle can be seen continuing along the B1134 Station Road, at approximate OS grid reference: TM 17751 87165.

The configuration is recommended to occupy the full available road way to aid in reducing oversail/overrun where possible. The configuration is expected to remain within the proposed road way, however, as the road width varies and the configuration negotiates bends along the road, minimal oversail would be expected at points. The road way offering circa 5.36m clear width, and the configuration anticipated a running width of circa 5.0m no conflicts are anticipated along this section (Leader 1). Additionally, depending on growth at the time of movement, trimming to foliage within the swept area may be required to facilitate delivery (Leader 2). Subject to observing the stated cautions and implementing the required remedial works, this section is considered to be physically negotiable.

Location Plan




Legend:

-  16-axle Girder Frame Trailer minimum turning arrangements Drawing ref. 24-1275.TC04
-  Extent of vehicle track
-  Extent of oversail
-  Extent of road boundary
-  Extent of property boundary
-  Overrun and oversail beyond kerb
-  Overrun beyond kerb
-  Oversail beyond kerb

1		
0	11.08.25	Issued for comment
Rev.	Date	Amendments

Revisions

Prepared by:



Shaftesbury House, 2 High Street,
Eccleshall, Stafford, ST21 6BZ
Tel: (01785) 850411

Independent Transportation Engineers

Client:



Project: East Pye

Title: Swept Path Assessments
Shown continuing along B1134 Station Road, at approximate OS grid reference: TM 17751 87165, on indicative 16 axle girder trailer transporting a 157te transformer.

Drawing status: Final Report

Scale (A3): As shown	Drawn by: MTO	Checked by: PW
Dwg. no: 24-1221.SPA03	Sheet: 4 of 12	Rev: 0

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Direction of Travel



Swept Path Assessment
Indicative of 16 Axle Girder Frame Trailer
Constructed from Client Supplied Data and
OS Mastermap Data
Scale 1:500

Glenhaven

Swept Path Assessment
Indicative of 16 Axle Girder Frame Trailer
Constructed from Client Supplied Data and OS Mastermap Data
Scale 1:1000

(Leader 3) Trimming to Surrounding
Foliage, Extent Depends on Growth at
Time of Movement.

(Leader 2) Street Furniture Positioned
Along Turn, Includes Telecom Pole,
Confirmation of Position
Recommended, Caution Advised,
Close Proximities Anticipated.

(Leader 1) Oversail to Inside of Turn,
Outside Highway Boundary but
Understood to be Within Client Red
Line Boundary, Confirmation of Land
Ownerships Recommended.

(Leader 4) Overhead Lines Present
Along Route, Configuration Expected to
be Circa 5.078m, Reducible to Circa
4.848m. Line Heights Recommended to
be Confirmed, Caution Advised.

Oversail of Approximately
101.62m²

43.79m

4.30m

STATION ROAD

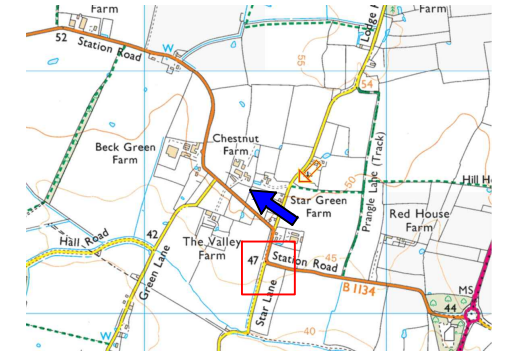
The delivery vehicle can be seen turning right along the B1134 Station Road, at approximate OS grid reference: TM 17509 87216.

The configuration is recommended to occupy the full available road way to aid in reducing oversail/overrun where possible.

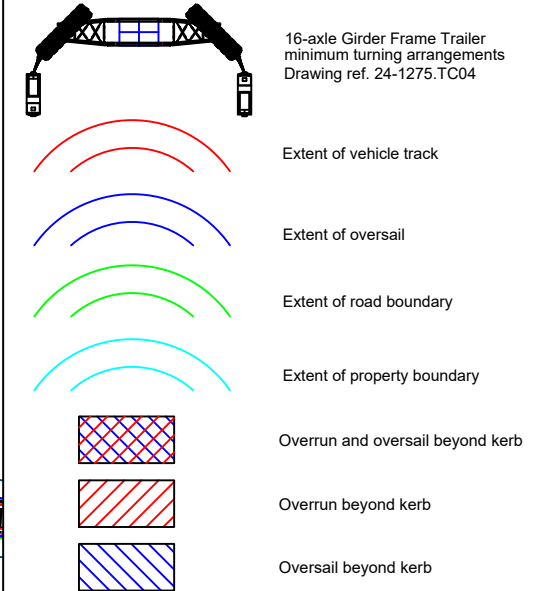
The configuration anticipates oversail on the inside of the turn, this is expected to be into third-party land, although this is understood to be within the clients red line boundary, confirmation of land ownerships are recommended (Leader 1). It should be noted that street furniture is positioned along the outside of the turn, this includes telecom poles; it is recommended to confirm their positions as close proximities are anticipated, caution advised (Leader 2). Additionally, depending on growth at the time of movement, trimming to foliage within the swept area may be required to facilitate delivery (Leader 3). It should be cautioned that overhead lines are present along the route, the configuration anticipates a running height of circa 5.078m, reducible to 4.848m using trailer hydraulics, line heights are recommended to be confirmed, caution is advised (Leader 4).

Subject to observing the stated cautions and implementing the required remedial works, this section is considered to be physically negotiable.

Location Plan



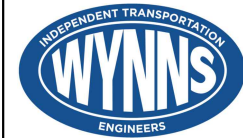
Legend:



1		
0	11.08.25	Issued for comment
Rev.	Date	Amendments

Revisions

Prepared by:



Independent Transportation Engineers

Shaftesbury House, 2 High Street,
Eccleshall, Stafford, ST21 6BZ
Tel: (01785) 850411

Client:



Project:

East Pye

Title:

Swept Path Assessments
Shown continuing along B1134 Station Road, at
approximate OS grid reference: TM 17509 87216, on
indicative 16 axle girder trailer transporting a 157te
transformer.

Drawing status:

Final Report

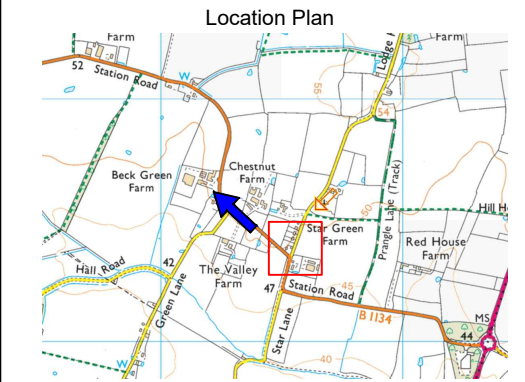
Scale (A3):	Drawn by:	Checked by:
As shown	MTO	PW
Dwg. no:	Sheet:	Rev:
24-1221.SPA03	5 of 12	0

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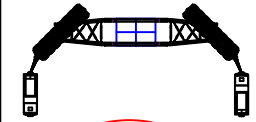







P:\Clients\Existing Clients\Island Green Power\24-1275 East Pye BESS Norfolk\Swept Path Assessments



Swept Path Assessment
 Indicative of 16 Axle Girder Frame Trailer
 Constructed from Client Supplied Data and OS Mastermap Data
 Scale 1:500

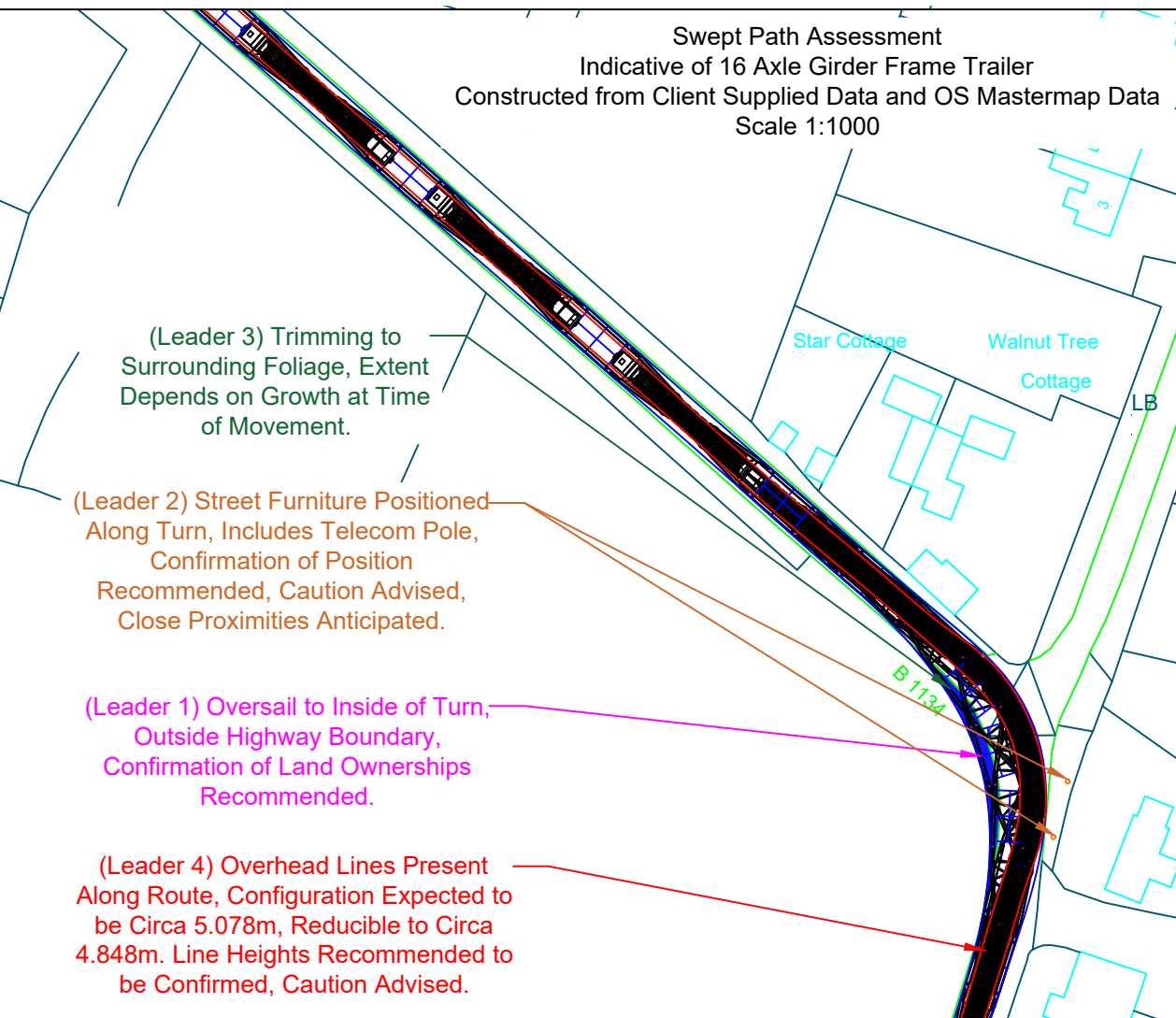


Legend:

-  16-axle Girder Frame Trailer
minimum turning arrangements
Drawing ref. 24-1275.TC04
-  Extent of vehicle track
-  Extent of oversail
-  Extent of road boundary
-  Extent of property boundary
-  Overrun and oversail beyond kerb
-  Overrun beyond kerb
-  Oversail beyond kerb

The delivery vehicle can be seen turning left along the B1134 Station Road, at approximate OS grid reference: TM 17537 87340.

The configuration is recommended to occupy the full available road way to aid in reducing oversail/overrun where possible.
 The configuration anticipates oversail on the inside of the turn, this is expected to be into third-party land, confirmation of land ownerships are recommended (Leader 1). It should be noted that street furniture is positioned along the outside of the turn, this includes telecom poles; it is recommended to confirm their positions as close proximities are anticipated, caution advised (Leader 2). Additionally, depending on growth at the time of movement, trimming to foliage within the swept area may be required to facilitate delivery (Leader 3). It should be cautioned that overhead lines are present along the route, the configuration anticipates a running height of circa 5.078m, reducible to 4.848m using trailer hydraulics, line heights are recommended to be confirmed, caution is advised (Leader 4).
 Subject to observing the stated cautions and implementing the required remedial works, this section is considered to be physically negotiable.




Oversail of Approximately
 55.28m²

B 1134
 40.86m
 1.94m

1		
0	11.08.25	Issued for comment
Rev.	Date	Amendments

Revisions

Prepared by:



Shaftesbury House, 2 High Street,
 Eccleshall, Stafford, ST21 6BZ
 Tel: (01785) 850411

Independent Transportation Engineers

Client:

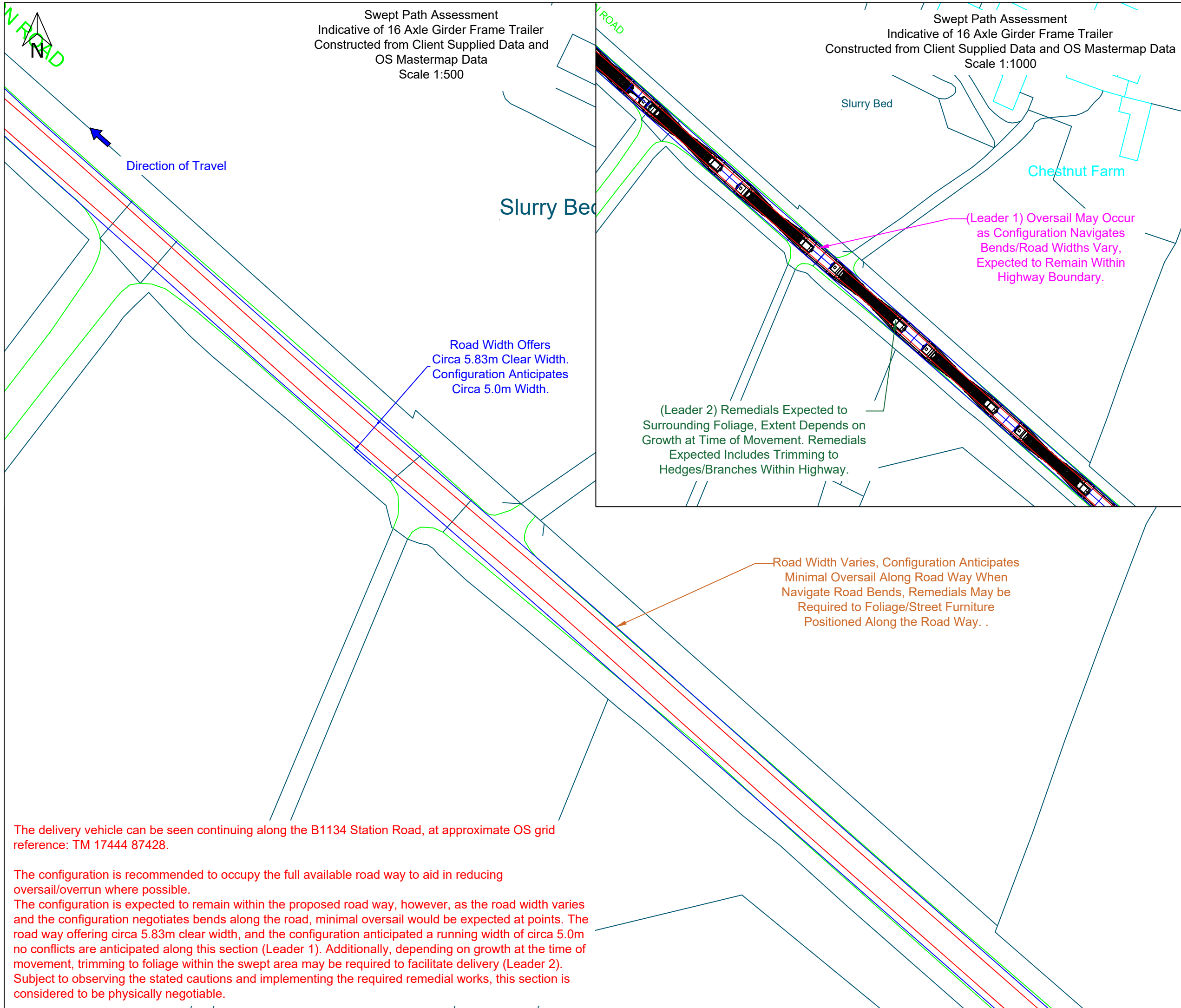


Project: East Pye

Title: Swept Path Assessments
 Shown continuing along B1134 Station Road, at approximate OS grid reference: TM 17537 87340, on indicative 16 axle girder trailer transporting a 157te transformer.

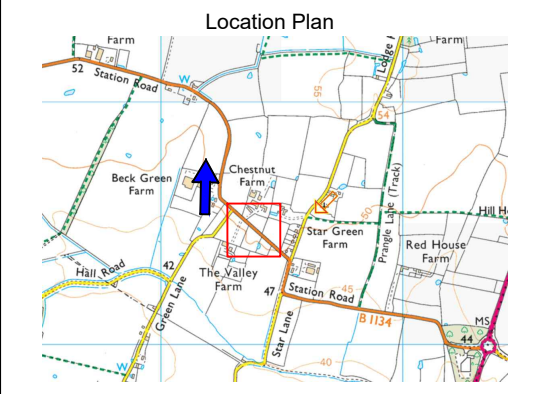
Drawing status: Final Report

Scale (A3): As shown	Drawn by: MTO	Checked by: PW
Dwg. no: 24-1221.SPA03	Sheet: 6 of 12	Rev: 0

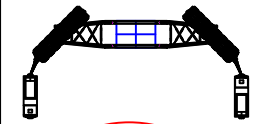




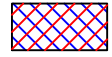




Swept Path Assessment
 Indicative of 16 Axle Girder Frame Trailer
 Constructed from Client Supplied Data and
 OS Mastermap Data
 Scale 1:500

Swept Path Assessment
 Indicative of 16 Axle Girder Frame Trailer
 Constructed from Client Supplied Data and OS Mastermap Data
 Scale 1:1000




Legend:

-  16-axle Girder Frame Trailer minimum turning arrangements Drawing ref. 24-1275.TC04
-  Extent of vehicle track
-  Extent of oversail
-  Extent of road boundary
-  Extent of property boundary
-  Overrun and oversail beyond kerb
-  Overrun beyond kerb
-  Oversail beyond kerb

1		
0	11.08.25	Issued for comment
Rev.	Date	Amendments

Revisions

Prepared by:



Shaftesbury House, 2 High Street,
 Eccleshall, Stafford, ST21 6BZ
 Tel: (01785) 850411

Independent Transportation Engineers

Client:



Project: East Pye

Title: Swept Path Assessments
 Shown continuing along B1134 Station Road, at approximate OS grid reference: TM 17444 87428, on indicative 16 axle girder trailer transporting a 157te transformer.

Drawing status: Final Report

Scale (A3): As shown	Drawn by: MTO	Checked by: PW
Dwg. no: 24-1221.SPA03	Sheet: 7 of 12	Rev: 0

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The delivery vehicle can be seen continuing along the B1134 Station Road, at approximate OS grid reference: TM 17444 87428.

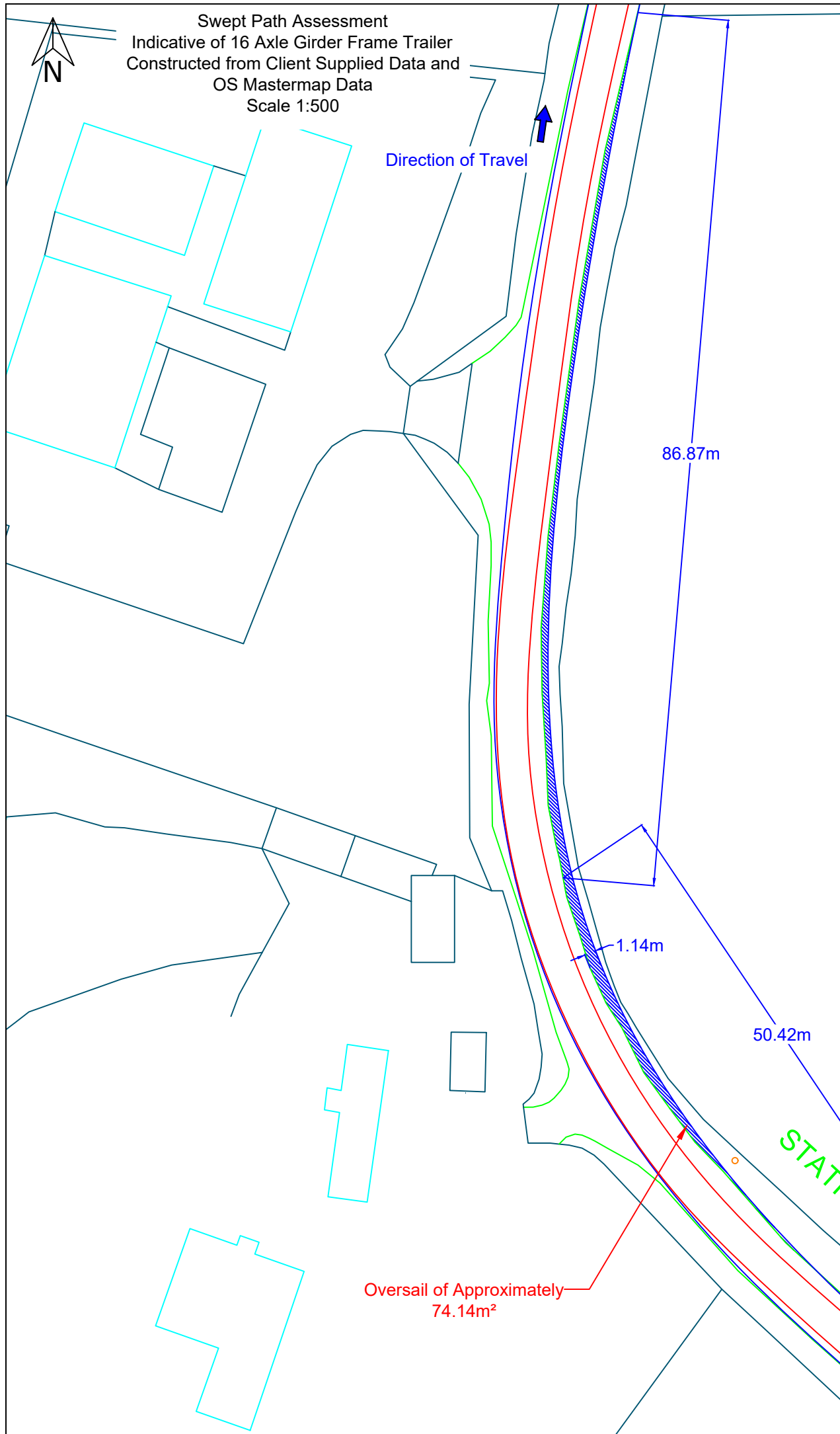
The configuration is recommended to occupy the full available road way to aid in reducing oversail/overrun where possible. The configuration is expected to remain within the proposed road way, however, as the road width varies and the configuration negotiates bends along the road, minimal oversail would be expected at points. The road way offering circa 5.83m clear width, and the configuration anticipated a running width of circa 5.0m no conflicts are anticipated along this section (Leader 1). Additionally, depending on growth at the time of movement, trimming to foliage within the swept area may be required to facilitate delivery (Leader 2). Subject to observing the stated cautions and implementing the required remedial works, this section is considered to be physically negotiable.

(Leader 2) Remedials Expected to Surrounding Foliage, Extent Depends on Growth at Time of Movement. Remedials Expected Includes Trimming to Hedges/Branches Within Highway.

(Leader 1) Oversail May Occur as Configuration Navigates Bends/Road Widths Vary, Expected to Remain Within Highway Boundary.

Road Width Varies, Configuration Anticipates Minimal Oversail Along Road Way When Navigate Road Bends, Remedials May be Required to Foliage/Street Furniture Positioned Along the Road Way. .

Road Width Offers Circa 5.83m Clear Width. Configuration Anticipates Circa 5.0m Width.



Swept Path Assessment
 Indicative of 16 Axle Girder Frame Trailer
 Constructed from Client Supplied Data and
 OS Mastermap Data
 Scale 1:500

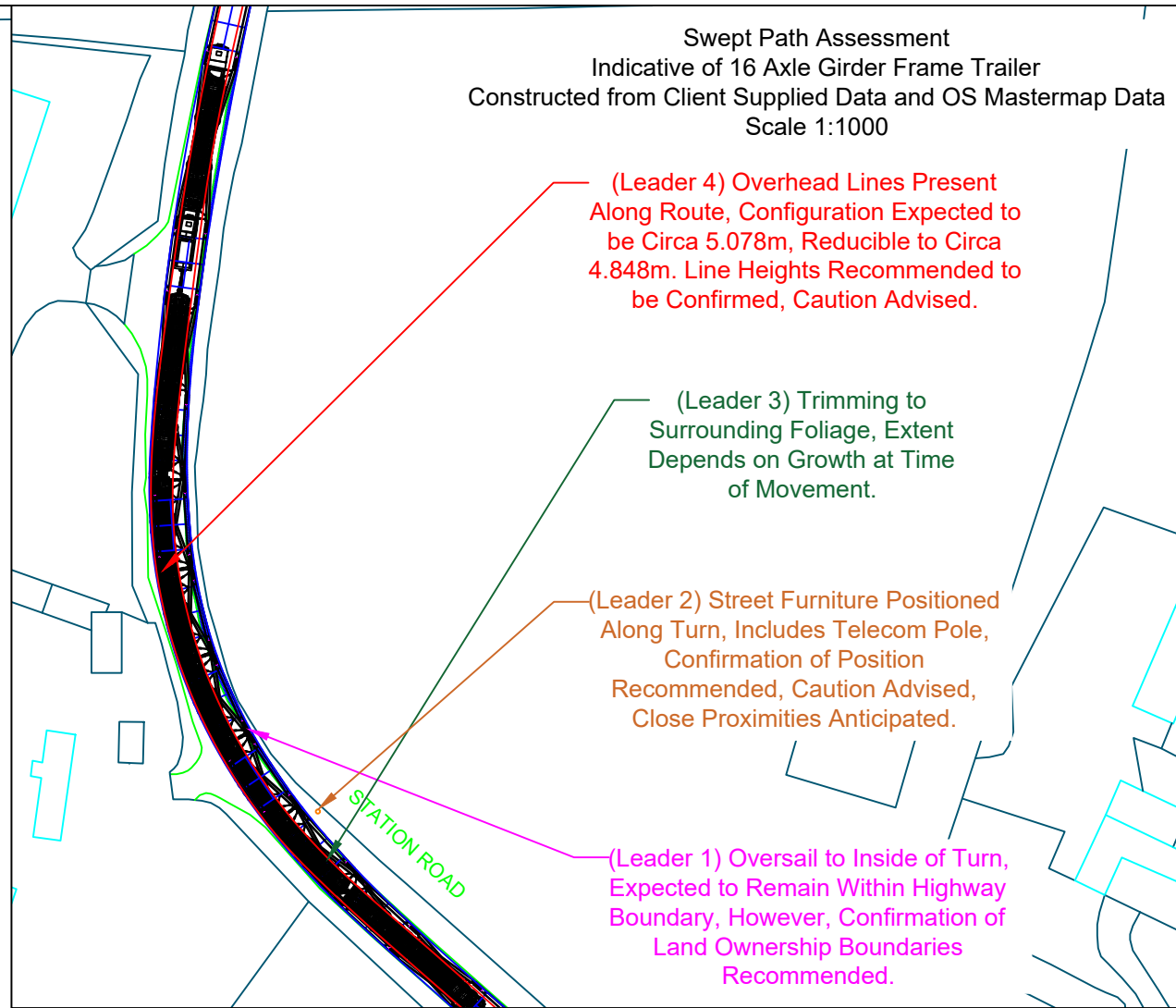
Direction of Travel

86.87m

1.14m

50.42m

Oversail of Approximately
 74.14m²



Swept Path Assessment
 Indicative of 16 Axle Girder Frame Trailer
 Constructed from Client Supplied Data and OS Mastermap Data
 Scale 1:1000

(Leader 4) Overhead Lines Present
 Along Route, Configuration Expected to
 be Circa 5.078m, Reducible to Circa
 4.848m. Line Heights Recommended to
 be Confirmed, Caution Advised.

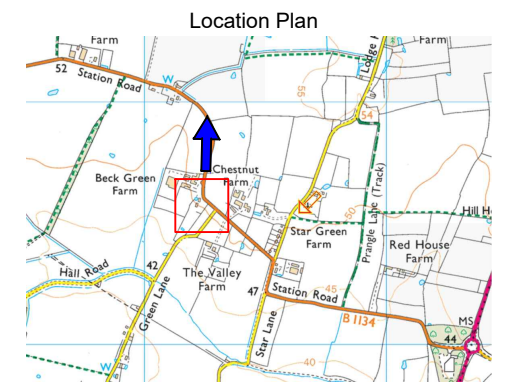
(Leader 3) Trimming to
 Surrounding Foliage, Extent
 Depends on Growth at Time
 of Movement.

(Leader 2) Street Furniture Positioned
 Along Turn, Includes Telecom Pole,
 Confirmation of Position
 Recommended, Caution Advised,
 Close Proximities Anticipated.

(Leader 1) Oversail to Inside of Turn,
 Expected to Remain Within Highway
 Boundary, However, Confirmation of
 Land Ownership Boundaries
 Recommended.

The delivery vehicle can be seen bearing right along the B1134 Station Road, at approximate OS grid reference: TM 17264 87594.

The configuration is recommended to occupy the full available road way to aid in reducing oversail/overrun where possible. The configuration anticipates oversail on the inside of the turn, although this is not expected to be into third-party land, confirmation of land ownerships are recommended, and if required, topographical surveys may be completed to confirm oversail extents (Leader 1). It should be noted that street furniture is positioned along the inside of the turn, this includes telecom poles; it is recommended to confirm their positions as close proximities are anticipated, caution advised (Leader 2). Additionally, depending on growth at the time of movement, trimming to foliage within the swept area may be required to facilitate delivery (Leader 3). It should be cautioned that overhead lines are present along the route, the configuration anticipates a running height of circa 5.078m, reducible to 4.848m using trailer hydraulics, line heights are recommended to be confirmed, caution is advised (Leader 4). Subject to observing the stated cautions and implementing the required remedial works, this section is considered to be physically negotiable.



Location Plan

Legend:

- 16-axle Girder Frame Trailer minimum turning arrangements Drawing ref. 24-1275.TC04
- Extent of vehicle track
- Extent of oversail
- Extent of road boundary
- Extent of property boundary
- Overrun and oversail beyond kerb
- Overrun beyond kerb
- Oversail beyond kerb

1		
0	11.08.25	Issued for comment
Rev.	Date	Amendments

Revisions

Prepared by:

Shaftesbury House, 2 High Street,
 Eccleshall, Stafford, ST21 6BZ
 Tel: (01785) 850411

Independent Transportation Engineers

Client:

Project: East Pye

Title: Swept Path Assessments
 Shown continuing along B1134 Station Road, at
 approximate OS grid reference: TM 17264 87594, on
 indicative 16 axle girder trailer transporting a 157te
 transformer.

Drawing status: Final Report

Scale (A3): As shown	Drawn by: MTO	Checked by: PW
Dwg. no: 24-1221.SPA03	Sheet: 8 of 12	Rev: 0

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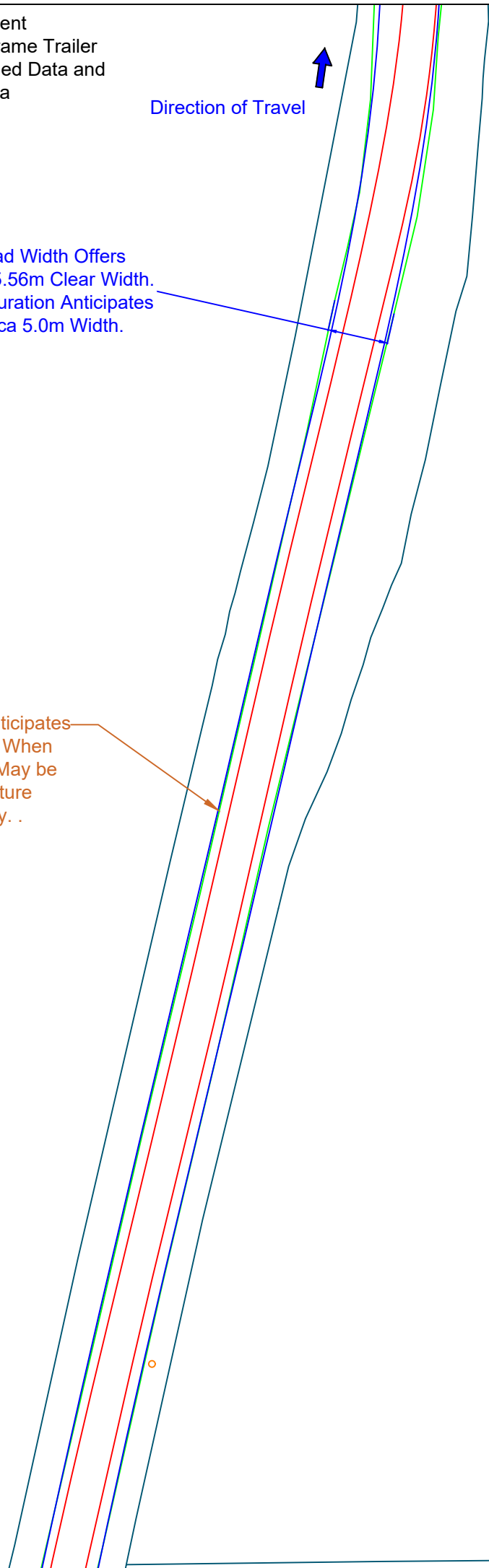


Swept Path Assessment
 Indicative of 16 Axle Girder Frame Trailer
 Constructed from Client Supplied Data and
 OS Mastermap Data
 Scale 1:500

Direction of Travel

Road Width Offers
 Circa 5.56m Clear Width.
 Configuration Anticipates
 Circa 5.0m Width.

Road Width Varies, Configuration Anticipates
 Minimal Oversail Along Road Way When
 Navigate Road Bends, Remedials May be
 Required to Foliage/Street Furniture
 Positioned Along the Road Way. .

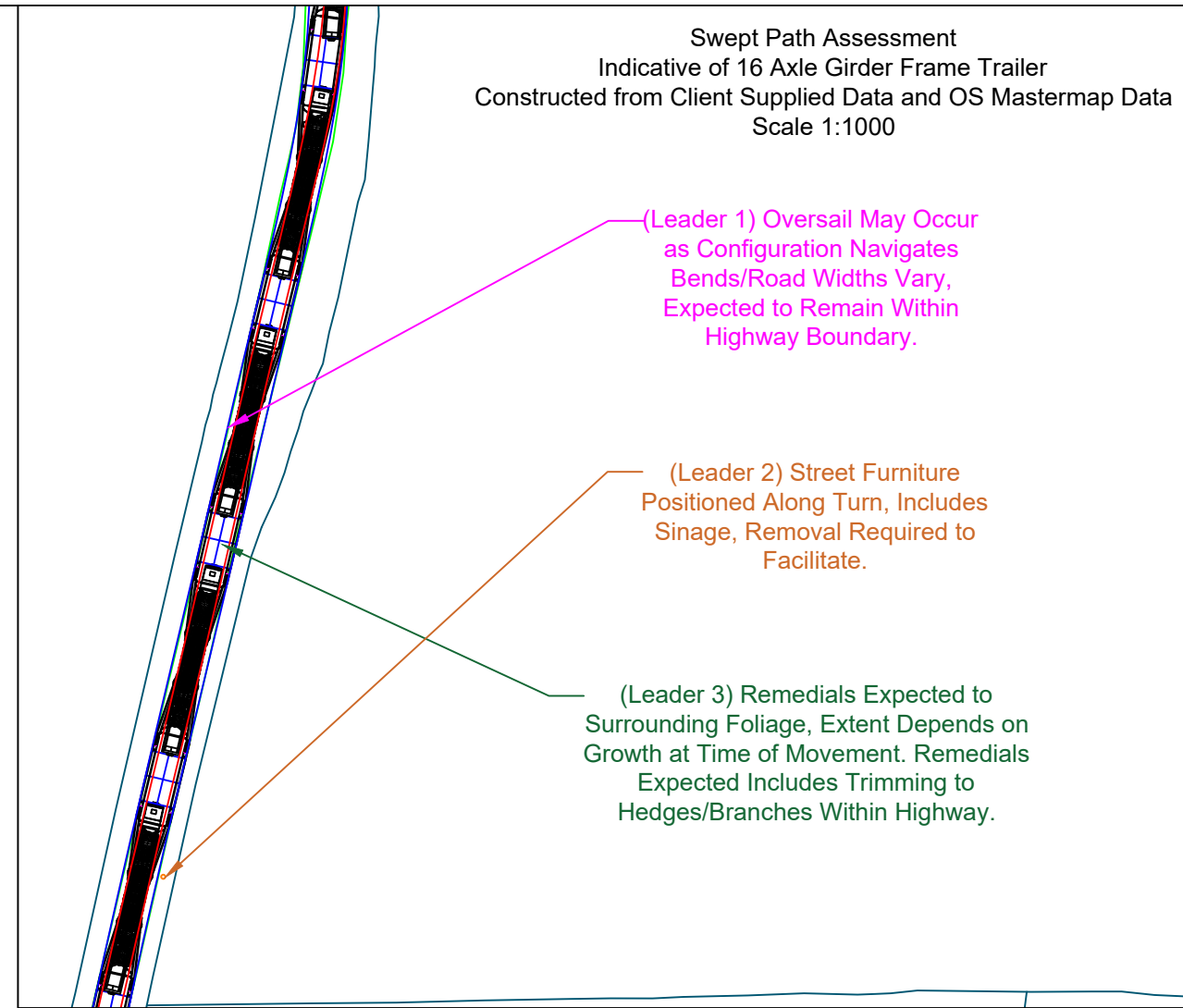


Swept Path Assessment
 Indicative of 16 Axle Girder Frame Trailer
 Constructed from Client Supplied Data and OS Mastermap Data
 Scale 1:1000

(Leader 1) Oversail May Occur
 as Configuration Navigates
 Bends/Road Widths Vary,
 Expected to Remain Within
 Highway Boundary.

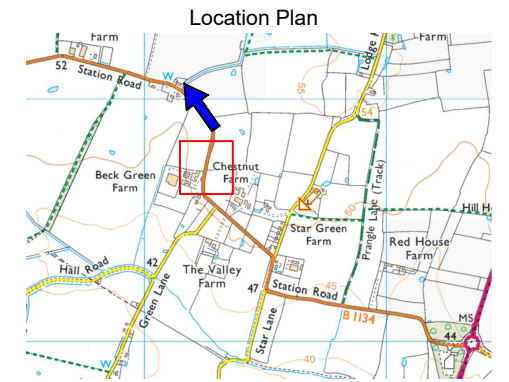
(Leader 2) Street Furniture
 Positioned Along Turn, Includes
 Sinage, Removal Required to
 Facilitate.

(Leader 3) Remedials Expected to
 Surrounding Foliage, Extent Depends on
 Growth at Time of Movement. Remedials
 Expected Includes Trimming to
 Hedges/Branches Within Highway.



The delivery vehicle can be seen continuing along the B1134 Station Road, at approximate OS grid reference: TM 17273 87787.

The configuration is recommended to occupy the full available road way to aid in reducing oversail/overrun where possible. The configuration is expected to remain within the proposed road way, however, as the road width varies and the configuration negotiates bends along the road, minimal oversail would be expected at points. The road way offering circa 5.56m clear width, and the configuration anticipated a running width of circa 5.0m no conflicts are anticipated along this section (Leader 1). It should be noted that street furniture is positioned along the roadway, this includes signage, conflict not anticipated but caution is advised (Leader 2). Additionally, depending on growth at the time of movement, trimming to foliage within the swept area may be required to facilitate delivery (Leader 3). Subject to observing the stated cautions and implementing the required remedial works, this section is considered to be physically negotiable.



Legend:

- 16-axle Girder Frame Trailer minimum turning arrangements Drawing ref. 24-1275.TC04
- Extent of vehicle track
- Extent of oversail
- Extent of road boundary
- Extent of property boundary
- Overrun and oversail beyond kerb
- Overrun beyond kerb
- Oversail beyond kerb

1		
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Rev.	Date	Amendments

Revisions

Prepared by:

Shaftesbury House, 2 High Street,
 Eccleshall, Stafford, ST21 6BZ
 Tel: (01785) 850411

Independent Transportation Engineers

Client:

Project: East Pye

Title: Swept Path Assessments
 Shown continuing along B1134 Station Road, at
 approximate OS grid reference: TM 17273 87787, on
 indicative 16 axle girder trailer transporting a 157te
 transformer.

Drawing status: Final Report

Scale (A3): As shown	Drawn by: MTO	Checked by: PW
Dwg. no: 24-1221.SPA03	Sheet: 9 of 12	Rev: 0

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Swept Path Assessment
Indicative of 16 Axle Girder Frame Trailer
Constructed from Client Supplied Data and
OS Mastermap Data
Scale 1:500

Direction of Travel

Road Width Offers
Circa 5.64m Clear Width.
Configuration Anticipates
Circa 5.0m Width.

Road Width Varies, Configuration Anticipates
Minimal Oversail Along Road Way When
Navigate Road Bends, Remedials May be
Required to Foliage/Street Furniture
Positioned Along the Road Way. .

Swept Path Assessment
Indicative of 16 Axle Girder Frame Trailer
Constructed from Client Supplied Data and OS Mastermap Data
Scale 1:1000

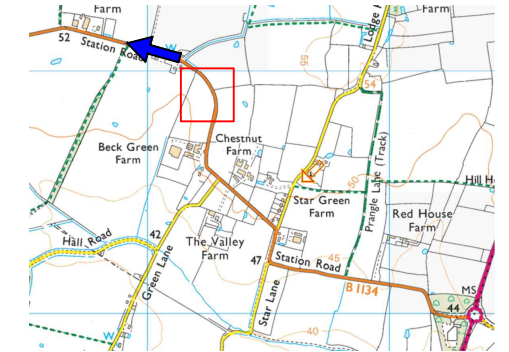
(Leader 1) Oversail May Occur
as Configuration Navigates
Bends/Road Widths Vary,
Expected to Remain Within
Highway Boundary.

(Leader 2) Remedials Expected to
Surrounding Foliage, Extent Depends on
Growth at Time of Movement. Remedials
Expected Includes Trimming to
Hedges/Branches Within Highway.

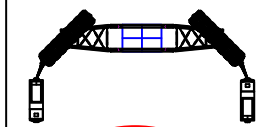







The delivery vehicle can be seen bearing left along the B1134 Station Road, at approximate OS grid reference: TM 17273 87916.

The configuration is recommended to occupy the full available road way to aid in reducing oversail/overrun where possible. The configuration is expected to remain within the proposed road way, however, as the road width varies and the configuration negotiates bends along the road, minimal oversail would be expected at points. The road way offering circa 5.64m clear width, and the configuration anticipated a running width of circa 5.0m no conflicts are anticipated along this section (Leader 1). Additionally, depending on growth at the time of movement, trimming to foliage within the swept area may be required to facilitate delivery (Leader 2). Subject to observing the stated cautions and implementing the required remedial works, this section is considered to be physically negotiable.

Location Plan



Legend:

-  16-axle Girder Frame Trailer
minimum turning arrangements
Drawing ref. 24-1275.TC04
-  Extent of vehicle track
-  Extent of oversail
-  Extent of road boundary
-  Extent of property boundary
-  Overrun and oversail beyond kerb
-  Overrun beyond kerb
-  Oversail beyond kerb

1		
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Rev.	Date	Amendments

Revisions

Prepared by:



Shaftesbury House, 2 High Street,
Eccleshall, Stafford, ST21 6BZ
Tel: (01785) 850411

Independent Transportation Engineers

Client:



Project: East Pye

Title: Swept Path Assessments
Shown continuing along B1134 Station Road, at
approximate OS grid reference: TM 17273 87916, on
indicative 16 axle girder trailer transporting a 157te
transformer.

Drawing status: Final Report

Scale (A3): As shown	Drawn by: MTO	Checked by: PW
Dwg. no: 24-1221.SPA03	Sheet: 10 of 12	Rev: 0

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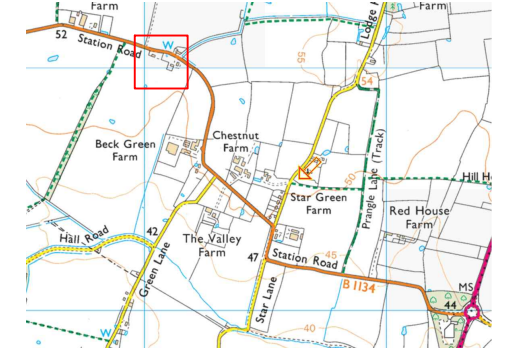
P:\Clients\Existing Clients\Island Green Power\24-1275 East Pye BESS Norfolk\Swept Path Assessments



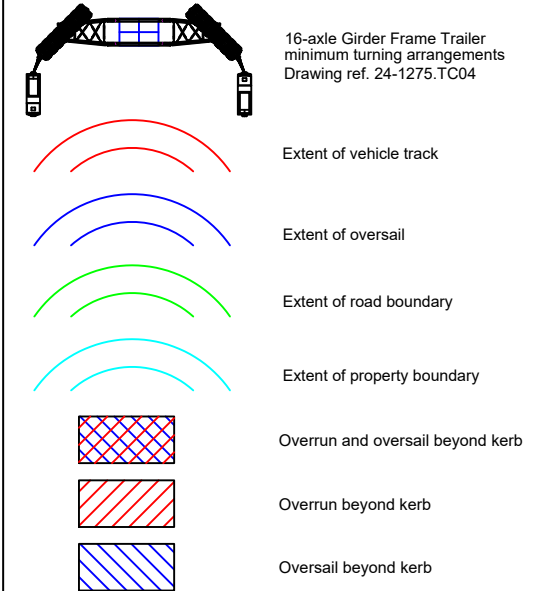
Swept Path Assessment
 Indicative of 16 Axle Girder Frame Trailer
 Constructed from Client Supplied Data and
 OS Mastermap Data
 Scale 1:500

Swept Path Assessment
 Indicative of 16 Axle Girder Frame Trailer
 Constructed from Client Supplied Data and OS Mastermap Data
 Scale 1:1000

Location Plan



Legend:



gh Cottages

h Cottages

ges

ION'
 Direction of Travel
 ROAD

tages

h Cottages

ges

The delivery vehicle can be seen continuing along the B1134 Station Road, at approximate OS grid reference: TM 17116 88060.

The configuration is recommended to occupy the full available road way to aid in reducing oversail/overrun where possible. The configuration is expected to remain within the proposed road way, however, as the road width varies and the configuration negotiates bends along the road, minimal oversail would be expected at points. The road way offering circa 5.36m clear width, and the configuration anticipated a running width of circa 5.0m no conflicts are anticipated along this section (Leader 1). Additionally, depending on growth at the time of movement, trimming to foliage within the swept area may be required to facilitate delivery (Leader 2). Subject to observing the stated cautions and implementing the required remedial works, this section is considered to be physically negotiable.

Road Width Offers
 Circa 5.36m Clear Width.
 Configuration Anticipates
 Circa 5.0m Width.

Road Width Varies, Configuration Anticipates
 Minimal Oversail Along Road Way When
 Navigate Road Bends, Remedials May be
 Required to Foliage/Street Furniture
 Positioned Along the Road Way. .

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1		
0	11.08.25	Issued for comment
Rev.	Date	Amendments

Revisions

Prepared by:



Shaftesbury House, 2 High Street,
 Eccleshall, Stafford, ST21 6BZ
 Tel: (01785) 850411

Independent Transportation Engineers

Client:



Project: East Pye

Title: Swept Path Assessments
 Shown continuing along B1134 Station Road, at
 approximate OS grid reference: TM 17116 88060, on
 indicative 16 axle girder trailer transporting a 157te
 transformer.

Drawing status: Final Report

Scale (A3): As shown	Drawn by: MTO	Checked by: PW
Dwg. no: 24-1221.SPA03	Sheet: 11 of 12	Rev: 0

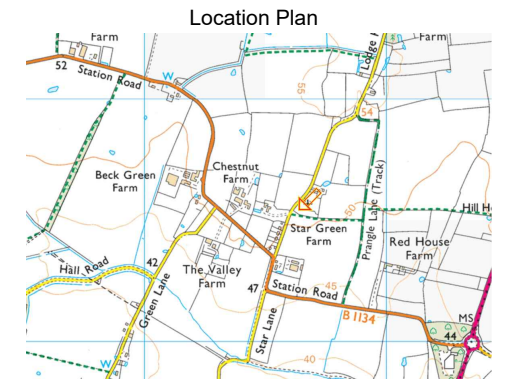
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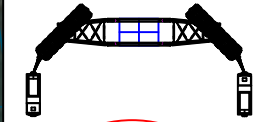









Swept Path Assessment
 Indicative of 16 Axle Girder Frame Trailer
 Constructed from Client Supplied Data and OS Mastermap Data
 Scale 1:5000

NOTE: Overlay onto aerial image is not representative of the configuration relative to the environment. This is for illustrative purposes only, and should only be taken as such.



Legend:

-  16-axle Girder Frame Trailer
minimum turning arrangements
Drawing ref. 24-1275.TC04
-  Extent of vehicle track
-  Extent of oversail
-  Extent of road boundary
-  Extent of property boundary
-  Overrun and oversail beyond kerb
-  Overrun beyond kerb
-  Oversail beyond kerb



1		
0	11.08.25	Issued for comment
Rev.	Date	Amendments

Revisions

Prepared by:
 Shaftesbury House, 2 High Street,
 Eccleshall, Stafford, ST21 6BZ
 Tel: (01785) 850411
 Independent Transportation Engineers

Client:


Project: East Pye

Title: Swept Path Assessments
 Showing tracking from approximate OS grid reference: TM 18309 86988 to approximate OS grid reference: TM 17116 88060, on indicative 16 axle girder trailer transporting a 157te transformer.

Drawing status: Final Report

Scale (A3): As shown	Drawn by: MTO	Checked by: PW
Dwg. no: 24-1221.SPA03	Sheet: 12 of 12	Rev: 0

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4. Cable Drum Highway Access Location Summary

- 4.1. Attachment 11 is a summary spreadsheet of access locations where the cable routes cross or travel near to the public highway. A red, amber, green colour coded system has been utilised to show the expected negotiability to each of the access points listed below. Green highlights very minimal issues expected, Amber is accessible with additional remedial works and Red is non-negotiable with the transport arrangements being considered. These transport arrangements for access is contained within this report as drawings numbered 24-1275.TC01 and 24-1275.TC02 in attachment 1.



Attachment 11

Cable Drum Highway Access Location Summary

Access point	Google Map Link	Pinch Points	Comment from Route Survey
2.1	https://maps.app.goo.gl/SFEaXTEbZIV1fK49	Tree trimming required within Brooke, along High Green and Woodton Road. Woodton Road is very narrow. Overhead wires.	Good access. Vehicle may need to reverse to site access from junction of Heath Road / Baxters Lane. Confirmatory SPA will be required at junction of Norwich Rd/High Green. Parking restrictions required along High Green. SPA will be required at junction of Honeypot Ln and Backwood Ln. SPA will be required at junction of Backwood Ln and Heath Rd. SPA will be required at Brooke Rd and Baxters Ln.
2.2	https://maps.app.goo.gl/mipc9rYpGnBfVp9i6	Tree trimming required within Brooke, along High Green and Woodton Road. Woodton Road is very narrow. Overhead wires.	Good access. Vehicle may need to reverse to site access from junction of Heath Road / Baxters Lane. Confirmatory SPA will be required at junction of Norwich Rd/High Green. Parking restrictions required along High Green.
2.3	https://maps.app.goo.gl/iameEI VQryh2icRZc6	Tree trimming required within Brooke, along High Green and Woodton Road. Woodton Road is very narrow. Overhead wires.	Left turn onto Mill Lane will require land take and removal of hedge. SPA required to confirm extent of works required.
3.1	https://maps.app.goo.gl/rJ5gux7MC16LN7M26	Overhead wires. Above highway tree trimming required.	Best route, B1332, Turn right Stoke Road, Turn left Norwich Road, Turn left The Green. Turn left Fylands Rd, Turn left Wash Lane. Narrow road widths along The Green restrict access.
3.2	https://maps.app.goo.gl/Q8nABC3giRwWDI2F9		Best route, B1332, Turn right Poringland Road, Turn left Norwich Road, Turn left The Green. Narrow road widths along The Green restrict access. Potential access point relocation before narrow section.
5.1	https://maps.app.goo.gl/QtVUIajkemvDq2iaA	Overhead wires	Confirmatory SPA advised for LH turn from Harvey's Ln onto Uppgate Rd.
5.2	https://maps.app.goo.gl/QtVUIajkemvDq2iaA		Confirmatory SPA advised for LH turn from Harvey's Ln onto Uppgate Rd.
6.1	https://maps.app.goo.gl/FcL2yVRSq9XGt3188		Good access.
6.2	https://maps.app.goo.gl/tJ8MgDCNucEi6kmw6	Very narrow carriageway. High volume of above highway tree trimming required.	Very narrow tree lined highway from B1527. Telegraph poles within wooded area will require relocation to allow access. Caution advised for RH turn from B1332 Norwich Rd to B1527.
6.3	https://maps.app.goo.gl/K3W55xrgUFitsTwS8		Good access.
6.4	https://maps.app.goo.gl/K3W55xrgUFitsTwS8		Good access.
9.1	https://maps.app.goo.gl/jskEemQRh9r2Lxcl6		Good access.
9.2	https://maps.app.goo.gl/jskEemQRh9r2Lxcl6	Unsure of access point.	Good access if using egress from B1527.
10.1	https://maps.app.goo.gl/JEZ4bBHtN54AUYaZ7		Good access.
10.2	https://maps.app.goo.gl/NKgdvpFFsrtp77Q6A	Unsure of access point. Good access to egress from B1527. If from Field Lane, then same issues arise as 11.1.	Good access if using egress from B1527. No access from Field Ln.
10.3	https://maps.app.goo.gl/PVR9A1c2L3YvhpAi6	Right hand turn from B1527 to Field Lane has property on inside, SPA advised. Overhead telecom wires and telecom poles near the carriageway along Field Lane. Small amount of tree trimming in places.	SPA required, roadway very narrow within residential zone (OS Grid Ref: TM 23940 93668). Left hand bend within Lundy Green has telecom pole near to highway on inside of bend. No access through Lundy Green due to residential properties in close proximity to highway.
11.1	https://maps.app.goo.gl/NKgdvpFFsrtp77Q6A	Right hand turn from B1527 to Field Lane has property on inside, SPA advised. Overhead telecom wires and telecom poles near the carriageway along Field Lane. Small amount of tree trimming in places.	Roadway very narrow within residential zone (OS Grid Ref: TM 23940 93668). None negotiable.
11.2	https://maps.app.goo.gl/H4rnNIPw3y4hAJaL9	Good access as far as 'The Street'	Good access if egress from The Street. No access via Field Ln.
11.3	https://maps.app.goo.gl/z2PmYiDAIPT4MVQL8	Good access as far as 'The Street'	Good access if egress from The Street. No access via Field Ln.
11.4	https://maps.app.goo.gl/z2PmYiDAIPT4MVQL8		Good access.
14.1	https://maps.app.goo.gl/kivxxF6Gag2JmssMA	Tree trimming required. Good access as far as B1527.	Access along trackway/footpath unknown. Will require haul road implementation.
14.2	https://maps.app.goo.gl/6W3M4QWah9JyGpY46	Above highway tree trimming. Overhead power lines. High volume of above highway tree trimming required.	Very narrow highway. Trees close to carriageway in places restrict access along Brick Kiln Ln.
14.3	Direct access from A140 Long Stratton Bypass, turn right Edge's Lane. Google map link not valid due to current road closure.	Above highway tree trimming. Overhead power lines. High volume of above highway tree trimming required.	Very narrow highway. Trees close to carriageway in places restrict access along Brick Kiln Ln. Road layout has altered since construction of Long Stratton Bypass. Recommended to revisit route survey once Edge's Lane is reopened to public traffic.

14.4	https://maps.app.goo.gl/VkHaMySrsb79ERCFA	Above highway tree trimming. Overhead power lines.	S' bend of road at Hall Ln / Mill Ln not negotiable due to properties near to carriageway. Hall Lane overbridge structure capacity unknown. Potentially explore access direct from new bypass.
14.5	https://maps.app.goo.gl/Z2mY5aJPFU3jnBkA8	Above highway tree trimming. Overhead power lines.	Unable to reach location at time of survey due to Long Stratton construction works. Not accessible from east of site. Hall Lane overbridge structure capacity unknown. Potentially explore access direct from new bypass.
14.6	https://maps.app.goo.gl/YEKtdhZZ1RFv5mLGA	Parkers Lane consists of very narrow carriageway. Trees close to carriageway. Right hand turn will require land take or roadway reconstruction to avoid.	Unable to reach location at time of survey due to Long Stratton construction works. Not accessible from east of site. Potentially explore access direct from new bypass.
14.7	https://maps.app.goo.gl/7YfYS6UJNmriZz5CA	Overhead wires, above highway tree trimming required. Telecom poles near to carriageway. High volume of above highway tree trimming on private lane.	Remedials required but good access. May be required to reverse along Wood Lane to private road in order to negotiable angle of turn. Private land not surveyed. Confirmatory SPA advised.
14.8	https://maps.app.goo.gl/7FHUsR7TxmLzsNJTA	Overhead wires, above highway tree trimming required. Telecom poles near to carriageway.	Remedials required but good access.
14.9	https://maps.app.goo.gl/SLD1vhdDRkSpAu669	Overhead wires, above highway tree trimming required. Telecom poles near to carriageway. Tree on inside of left hand turn from Common Road to Wood Lane. SPA required.	LH turn from Wood Ln continuing onto Wood Ln not negotiable due to large tree on inside of turn.
14.10	https://maps.app.goo.gl/y1V1AWctKfPHyEHL7	Overhead wires, above highway tree trimming required. Telecom poles near to carriageway.	Remedials required but good access. Same highway egress location as 14.11.
14.11	https://maps.app.goo.gl/y1V1AWctKfPHyEHL7	Overhead wires, above highway tree trimming required. Telecom poles near to carriageway.	Remedials required but good access. Same highway egress location as 14.10.
14.12	https://maps.app.goo.gl/XjYAXXDzfwSjDXe8		Good direct access from A140
15.1	https://maps.app.goo.gl/nwQy2NjKvhtFhln6A	Overhead wires, above highway tree trimming required. Telecom poles near to carriageway on Brick Lane.	Unable to gain access to location due to Long Stratton bypass construction works. Reassessment required.
16.1	https://maps.app.goo.gl/JK3UnTrFnRAdCJaG7	Telecom poles near to carriageway to South.	Access lined with telegraph poles on south of roadway. Road widening or relocation/undergrounding of overhead lines anticipated to negotiate.
17.1	https://maps.app.goo.gl/CPASxfQ4B1TYvW6A		Good direct access from A140
18.1	https://maps.app.goo.gl/1dfte9HwurCUyPH37	High volume of above highways tree trimming required.	Left hand bend (OS Grid Ref: TM 18236 88820) not negotiable as tree near to carriageway on inside. Vehicle will be required to gain access to field from southern entrance. Narrow road width on Lodge Lane, caution required.
18.2	https://maps.app.goo.gl/1dfte9HwurCUyPH37	High volume of above highways tree trimming required.	Left hand bend (OS Grid Ref: TM 18236 88820) not negotiable as tree near to carriageway on inside. Vehicle will be required to gain access to field from southern entrance. Narrow road width on Lodge Lane, caution required.
18.3	https://maps.app.goo.gl/1dfte9HwurCUyPH37	High volume of above highways tree trimming required.	Left hand bend (OS Grid Ref: TM 18236 88820) not negotiable as tree near to carriageway on inside. Vehicle will be required to gain access to field from southern entrance. Narrow road width on Lodge Lane, caution required.

Key		
Green		Good Access, small amount of remedials expected. Measures to be taken which are commonplace or have taken place recently which stakeholders are aware of.
Amber		Further investigations required to confirm negotiability. May not be negotiable in current form. High amount of remedial works may be required.
Red		Access none negotiable. Further remedial works will not allow passage. Alternative route must be found.

Appendix E Correspondence with NCC and NH